

STANISLAUS COUNTY PLANNING COMMISSION

May 4, 2017

STAFF REPORT

REZONE APPLICATION NO. PLN2016-0066 BRONCO WINE COMPANY

REQUEST: REQUEST TO REZONE A 117.93 ACRE PARCEL FROM EXISTING PLANNED DEVELOPMENT P-D (6) AND P-D (321) ZONES TO A NEW PLANNED DEVELOPMENT (P-D) ZONE TO ALLOW FOR THE EXPANSION OF AN EXISTING WINERY AND BOTTLING FACILITY DEVELOPED ON 82.15 ACRES OF THE PROJECT SITE. THE EXPANSION INCLUDES 14 PROPOSED BUILDINGS, TOTALING 1,462,186 SQUARE FEET, THE CONSTRUCTION OF TWO RAIL SPURS, AND THE ADDITION OF A FLEET OF 53 FOOT LONG TRUCKS AND TANKER TRUCKS.

APPLICATION INFORMATION

| | |
|------------------------------|--|
| Applicant/Property owner: | Bronco Wine Company - John Franzia |
| Agent: | CB Engineering |
| Location: | 800 E. Keyes Road and 6342 Bystrum Road, at the southeast corner of Bystrum and E. Keyes Roads, east of Crows Landing Road, west of State Highway 99, and south of Ceres. |
| Section, Township, Range: | 33-4-9 |
| Supervisorial District: | Two (Supervisor Chiesa) |
| Assessor's Parcel: | 041-046-021 |
| Referrals: | See Exhibit M Environmental Review Referrals |
| Area of Parcel(s): | 117.93 acres |
| Water Supply: | Private well |
| Sewage Disposal: | Private septic system |
| General Plan Designation: | AG (Agriculture) |
| Existing Zoning: | Planned Development (6) & Planned Development (321) |
| Sphere of Influence: | N/A |
| Community Plan Designation: | N/A |
| Williamson Act Contract No.: | N/A |
| Environmental Review: | Mitigated Negative Declaration |
| Present Land Use: | Bronco Wine Company, single-family dwelling, drainage basin, and a vineyard. |
| Surrounding Land Use: | Scattered single-family dwellings in all directions. To the north, orchards and row crops. To the east, a vineyard, orchards, and row crops. To the west orchards, row crops, and a dairy farm. To the south a chicken farm, orchard, row crops, and a dairy farm. |

RECOMMENDATION

Staff recommends the Planning Commission recommend that the Board of Supervisors approve this request based on the discussion below and on the whole of the record provided to the County. If the Planning Commission decides to recommend approval of this project, Exhibit A provides an overview of all of the findings required for project approval, which includes rezone findings and adoption of a Mitigated Negative Declaration.

SITE DESCRIPTION

The project site is located at 800 E. Keyes Road and 6342 Bystrum Road, east of Crows Landing Road, west of State Highway 99, and south of Ceres. The northern portion is improved with a single-family dwelling, drainage basin, and a vineyard. The southern portion includes the existing Bronco Wine Company facility. The Union Pacific Railroad abuts the western property line of the project site. The parcel includes a 20-foot-wide panhandle which extends from the eastern most portion of the project site, north to E. Keyes Road.

The surrounding area consists of agricultural uses, primarily orchards and vineyards, with scattered single-family dwellings.

BACKGROUND

According to County records, the southern 82.15 acres of the project site, which currently includes the Bronco Wine Company facility, located at 6342 Bystrum Road, was rezoned to allow operation of a winery and bottling facility in 1974. The Board of Supervisors approved the rezone from A-2-10 (General Agriculture) to P-D (6) (Planned Development) based on the following factors:

1. The proposed project should not be detrimental to the existing agricultural usage of the surrounding neighborhood if developed in compliance with the recommended performance standards.
2. The use is in compliance with the General Plan as a facility that is associated with agricultural production and complies with the provisions of the Planned Development zone.
3. The project is to be located near major or collector streets and a railroad facility that would provide the necessary transportation needs of the facility.
4. Many such winery facilities are located throughout the valley region in rural areas without apparent conflict with surrounding agricultural uses.

Since its approval in 1974, Bronco Wine Company has produced wine and sparkling wine and has a license to produce malt beverages. Grapes are trucked to the site and crushing operations take place during the grape harvest season, generally from July to November. After crushing, the grapes are fermented in large stainless steel tanks and grape skins and seeds are pressed and discarded with the pressed grape pomace to be sold for feed. After fermenting, the wine is transferred to storage tanks where it is cooled, filtered, blended, and bottled.

A Use Permit was completed in 1981 which allowed for additional tanks and a doubling in size of the crushing facilities. Since then, they have expanded through several Staff Approval permits which have allowed for numerous expansions to the operations' bottling, cooling, tank facilities, warehousing, and offices.

The northern 35.78 project site, which includes an existing single-family dwelling, drainage basin, and vineyard, was rezoned to P-D (321) in 2010, permitting conversion of the dwelling into an office for their shipping and receiving services. The rezone also allowed for the construction of two truck scales, a guard shack, the construction of employee and truck parking lots, and two 14,400 square-foot office buildings, new septic tanks, and landscaping.

A Time Extension processed for P-D (321) extended the Development Schedule to October 20, 2016. Although some grading occurred on the northern portion of the site prior to the date allowed by the Time Extension, the development schedule has not been met; and, as such, a new Rezone is required to develop the site.

The northern parcel (previously APN: 041-046-019) and the existing winery facility to the south (previously APN: 041-046-020) were merged into one parcel, and a new and expanded project description is now being proposed, requiring a new Rezone for the entire 117+ acre merged property (now APN: 041-046-021).

PROJECT DESCRIPTION

This is a request to rezone a 117.93 acre parcel from existing Planned Development P-D (6) and P-D (321) zones to a new Planned Development (P-D) zone to allow for expansion of an existing winery and bottling facility developed on 82.15 acres of the project site.

The expansion includes construction of the following 14 proposed buildings, totaling 1,462,186 square feet to be developed in phases. (See Buildings labeled N-BB on the site plan included in Exhibit B-5 through B-9):

- Four 120,000 square-foot warehouses (Buildings N, O, P, and Q), two with 10 additional truck docks each
- Three 44,483 square-foot warehouses (Buildings V, W, and X)
- A 13,000 square-foot office (Building T)
- A 38,000 square-foot office (Building U)
- A 10,300 square-foot employee center (commercial kitchen, cafeteria, and conference area, Building R)
- A 2,264 square-foot pavilion (roof only shade structure, Building S)
- A 20,000 square-foot employee center (lockers and restrooms, Building Y)
- A 30,000 square-foot administration building (Building Z)
- A 16,000 square-foot filter storage building (Building AA).
- Construction of two rail spurs to be utilized for deliveries
- A fleet of 53-foot-long trucks and tanker trucks

Phase 1 – Includes construction of a 120,000 square foot warehouse (Building Q) to be utilized for the storage of bottled wine stock, construction of two rail spurs to be utilized for deliveries, a fleet of 53 foot long trucks and tanker trucks, and fencing around the perimeter of the new warehouse. Development will occur within five years of project approval.

Future Phases – All other proposed uses will be included in future phases to be constructed according to market demand and will require a Use Permit be obtained. The parking lot expansion will occur as required for each building constructed. Fencing and landscaping around the entire project site to be completed with the first building permit to be issued for the next Phase.

(See Exhibit D – *Development Schedule* and Exhibit E – *Applicant's Project Description*.)

The hours of operation for the winery are Monday-Friday, 24 hours a day year-round and additionally, Sunday-Saturday 24 hours per day during seasonal months, which is from mid-July to mid-November. There are currently 396 employees year-round with an additional 90 employees during seasonal months, for a total of 486 employees maximum. At full build-out, there will be approximately 30 additional year-round employees, for a total of 426 employees year-round and 516 with employees seasonally. The addition of the employee center and pavilion will be utilized for educational seminars and meetings, to be held up to two times per year for up to 68 people, for Bronco's National sales force, and for Bronco's Wholesale Division's monthly meetings (Northern California sales force), which proposes to utilize the Ceres site up to four times per year for up to 50 managers.

All access associated with this project will occur along Bystrum Road. All entrances to the operation are fenced and include security gates. The expansion also includes railroad access to Union Pacific Railroad by constructing two rail spurs, which will minimize traffic impacts in surrounding areas.

As part of the rezone, a fleet of 53-foot-long trucks and tanker trucks will be added to the operation and stored on-site to allow both bulk and bottled wines to be picked up and delivered to partner wineries. On-site truck maintenance will be limited to minor maintenance activities. Any required major maintenance will be performed at off-site truck repair shops.

The project site currently includes six-foot high security chain-link fencing, and cypress trees along the eastern and western property borders and proposes to extend the fencing and cypress trees along the northern property line.

The project proposes to maintain their current operational ratio of approximately 88% of product produced and owned by Bronco, 8% produced by other California wineries, and 4% imported from other countries.

ISSUES

The following is a summary of those issues which have been identified as part of the review of the project:

Traffic

A Traffic Impact Analysis for the proposed project was prepared by KD Anderson & Associates, Inc., dated November 23, 2016. The analysis evaluated traffic impacts from the project based on the proposed new structures and based on the addition of a fleet of trucks and the utilization of rail, which will allow the current truck trip to inventory ratio to be decreased. Trucks currently arrive to the site empty or leave the site empty. The addition of their own truck fleet will allow truck trips to be full both on the way to the site and on the way to a delivery/pick-up destination. The use of rail will also offset truck trips as the equivalent of four fully stocked trucks can fit into one rail car. At full build-out there will be approximately 30 additional year-round employees and the proposed employee centers will host staff for trainings up to six times a year.

Stanislaus County Public Works and the Stanislaus County Environmental Review Committee both provided referral responses requesting that the Traffic Impact Analysis be amended to address safety concerns. The Traffic Impact Analysis was revised on March 15, 2017, to include improvements to the intersection of Keyes Road and Bystrum Road, including dedicated turn lanes

per the California Highway Design Manual, to address traffic safety concerns. This has been incorporated into the project as a Mitigation Measure. With mitigation applied, impacts to transportation and traffic are considered to be less than significant. (See Exhibit H - *Traffic Impact Analysis, prepared by KD Anderson & Associates, Inc. dated November 23, 2016, Revised March 15, 2017.*)

Air Quality

No referral response was received from the San Joaquin Valley Air Pollution Control District during the Early Consultation referral period. However, a referral response was received from the Air District during the 30-day Initial Study review period indicating that further review of the project's potential impacts to air quality should be conducted. Specifically, the response letter stated that the project's emissions of criteria pollutants, at full build-out, may exceed the District's thresholds of 10 tons/year NOX, 10 tons/year ROG, and 15 tons/year PM10. Further, the response letter stated that project related pollutant emissions should be identified and quantified, for both existing and post-project construction and operational emissions. The letter also indicated that a Health Impact Assessment may also be needed to evaluate the project's health related impacts.

The comments provided by the Air District are based on the project at full build-out. However, Phase 1 of the project, which includes one 120,000 square-foot warehouse, is under the threshold of significance for industrial projects, which ranges from 370,000 square feet for an industrial park, to 920,000 square feet for heavy industrial uses. Accordingly, Staff recommends that consideration of approval for Phase 1 of this project move forward without additional environmental analysis. However, Phase 1 is still required to obtain any applicable Air District permits, as reflected in the Development Standards. Staff also recommends that future phases of this project request be considered through the use permit process, which will allow additional CEQA analysis to be conducted, specifically in terms of potential impacts to air quality. This is reflected in the Development Standards applied to this project. (See Exhibit-I – *Project Referral Response received on April 24, 2017, from the San Joaquin Valley Air Pollution Control District.*)

GENERAL PLAN CONSISTENCY

Consistency with the goals, objectives, and policies of the various elements of the General Plan must be evaluated when processing all discretionary project requests. Additionally, in order to approve a rezone, it must be found to be consistent with the General Plan. In this case, the General Plan designation is Agriculture. The Agriculture General Plan designation is consistent with a Planned Development zoning designation when, *"it is used for agriculturally-related uses or for uses of a demonstrably unique character, which due to specific agricultural needs or to their transportation needs or to needs that can only be satisfied in the agriculture designation, may be properly located within areas designated as "agricultural" on the General Plan. Such uses can include facilities for packing fresh fruit, facilities for the processing of agricultural commodities utilized in the County's agriculture community, etc."* Goal One, Policy One, Implementation Measure One of the Land Use Element requires that when reviewing proposals for amendments to land use designations, the County shall evaluate how the proposal would advance the long-term goals of the County. Goal Two and Three of the Land Use Element of the Stanislaus County General Plan aim to ensure compatibility between land uses; and, to promote diversification and growth of the local economy by accommodating the siting of industries with unique requirements, as described in the Land Use Designations section of the Land Use Element.

In December of 2007, Stanislaus County adopted an updated Agricultural Element which incorporated guidelines for the implementation of agricultural buffers applicable to new and expanding non-agricultural uses within or adjacent to the A-2 Zoning District. The purpose of these

guidelines is to protect the long-term health of agriculture by minimizing conflicts such as spray drift and trespassing resulting from the interaction of agricultural and non-agricultural uses. Alternatives may be approved provided the Planning Commission finds that the alternative provides equal or greater protection than the existing buffer standards. The project does currently include six-foot high security chain-link fencing, and cypress trees along the eastern and western property borders and proposes to extend the fencing and cypress trees to act as an agricultural buffer along the northern property line. The proposed buildings meet the required 150-foot setback on the eastern and southern boundaries of the project site. The warehouse proposed on the northern portion of the property, along the western property border, does not meet the required 150-foot buffer setback standard; however, the warehouses are intended for storage and will not be occupied by employees at all times which allows it to be considered as a permitted use within the buffer area if determined to be a low people intensive use similar to a roadway or parking lot. The project site is considered to meet the required 150-foot buffer on the northern boundary, as the proposed office building closest to the northern property line exceeds the 150-foot setback requirement. The proposed administrative building closest to the western property line is setback 108 feet from the nearest agricultural property, which does not meet the required 150-foot setback. Accordingly, the applicant is proposing to utilize the existing fencing and landscaping as an agricultural buffer alternative allowing for a reduced setback on the west property line. Staff believes that because the administration building is proposed to be located in the area already developed by the winery that the alternative can be found to provide equal protection to the existing buffer standards.

Staff believes that the proposed Planned Development is consistent with the General Plan. This project is a request to expand an existing use. The existing P-D (6) and expired P-D (321) rezones were both found to meet the standards of the General Plan, including consistency with the Agricultural Land Use Designation. With mitigation and development standards in place, staff believes the project is consistent with the County's General Plan.

ZONING ORDINANCE CONSISTENCY

The site is currently zoned Planned Development P-D (6) and P-D (317) which includes development plans that outline specific development regulations and design standards applicable to the project's approved uses. The proposed rezone to a new P-D allowing for the expansion of the existing winery and bottling facility must be found consistent with the General Plan's Agricultural designation. The proposed new P-D will replace the Development Standards associated with the existing P-D (6) and expired P-D (321) Planned Development zoning designations with revised uses and Development Standards. All applicable Development Standards from P-D (6) have been incorporated into the Development Standards for this Rezone. This project will maintain zoning consistency by adhering to the uses and development Standards incorporated into this project.

ENVIRONMENTAL REVIEW

Pursuant to the California Environmental Quality Act (CEQA), the proposed project was circulated to all interested parties and responsible agencies for review and comment. As discussed in Section XVI – Transportation/Traffic of the Initial Study prepared for this project, and in the *Issues* Section of this Staff Report, a Traffic Impact Analysis was prepared and mitigation was applied as recommended by the studies to reduce potential impacts from transportation/traffic to a less than significant level. (See Exhibit H - *Traffic Impact Analysis, prepared by KD Anderson & Associates, Inc. dated November 23, 2016, Revised March 15, 2017.*) Additionally, mitigation to prevent impacts from the addition of on-site lighting has also been incorporated into this project, as discussed in Section I – Aesthetics of the Initial Study, reducing potential impacts to a less than significant level. (See Exhibit J – *Initial Study.*)

As discussed in the Issues Section of this Staff Report, a referral response was received from the Air District during the 30-day Initial Study review period indicating that further review of the project's potential impacts to air quality should be conducted. Phase 1 includes one 120,000 square-foot warehouse, which is under the threshold of significance for industrial projects. Therefore, staff is recommending that a use permit be obtained prior to development of any future phases, beyond Phase 1, which will allow additional CEQA analysis to be completed. This is reflected in the Development Standards applied to this project.

A Mitigated Negative Declaration has been prepared for approval prior to action on this Rezone as the project will not have a significant effect on the environment. (See Exhibit L - *Mitigated Negative Declaration*.) Development standards reflecting referral responses have also been placed on the project. (See Exhibit C – *Development Standards and Mitigation Measures*.)

Note: Pursuant to California Fish and Game Code Section 711.4, all project applicants subject to the California Environmental Quality Act (CEQA) shall pay a filing fee for each project; therefore, the applicant will further be required to pay **\$2,273.25** for the California Department of Fish and Wildlife (formerly the Department of Fish and Game) and the Clerk Recorder filing fees. The attached Conditions of Approval ensure that this will occur.

Contact Person: Kristin Doud, Senior Planner, (209) 525-6330

Attachments:

- Exhibit A - Findings and Actions Required for Project Approval
- Exhibit B - Maps
- Exhibit C - Development Standards and Mitigation Measures
- Exhibit D - Development Schedule
- Exhibit E - Applicant Project Description
- Exhibit F - Board of Supervisor's Report for Rezone Application No. REZ74-02 – Bronco Winery, dated April 9, 1974
 - Attachment 1 - Planning Commission Staff Report, March 21, 1974
 - Attachment 2 - Final Environmental Impact Report
 - Attachment 3 - Exhibit C – Development Plan (Performance Standards)
- Exhibit G - Board of Supervisor's Report for Rezone Application No. REZ2009-04 – Bronco Wine Company, dated April 20, 2010 (*with partial attachments*)
 - Attachment 1 - Planning Commission Staff Report, March 18, 2010
 - Exhibit A – Maps
 - Exhibit B – Development Standards
 - Exhibit C – Development Schedule
 - Exhibit H – Surrounding Landowner's Responses
 - Attachment 2 - Planning Commission Minutes, March 18, 2010
- Exhibit H - Traffic Impact Analysis, prepared by KD Anderson & Associates, Inc. dated November 23, 2016, Revised March 15, 2017
- Exhibit I - Project Referral Response received on April 24, 2017, from the San Joaquin Valley Air Pollution Control District
- Exhibit J - Initial Study
- Exhibit K - Mitigation Monitoring Plan
- Exhibit L - Mitigated Negative Declaration
- Exhibit M - Environmental Review Referrals

Exhibit A
Findings and Actions Required for Project Approval

NOTE: The proposed project must obtain approval from the Stanislaus County Board of Supervisors to be permitted. The Planning Commission may make a recommendation to the Board. Should the Commission support the project, the Commission may recommend the following:

1. Adopt the Mitigated Negative Declaration and Mitigation Monitoring Plan pursuant to CEQA Guidelines Section 15074(b), by finding that on the basis of the whole record, including the Initial Study and any comments received, that there is no substantial evidence the project will have a significant effect on the environment and that the Mitigated Negative Declaration reflects Stanislaus County's independent judgment and analysis.
2. Order the filing of a Notice of Determination with the Stanislaus County Clerk-Recorder pursuant to Public Resources Code Section 21152 and CEQA Guidelines Section 15075.
3. Find that:
 - A. The project is consistent with the overall goals and policies of the County General Plan.
 - B. The proposed Planned Development zoning is consistent with the Agriculture General Plan designation.
 - C. The alternative to the Agricultural Buffer Standards applied to this project provides equal or greater protection than the existing buffer standards.
 - D. The project will increase activities in and around the project area, and increase demands for roads and services, thereby requiring dedication and improvements.
4. Approve Rezone No. PLN2016-0066 – Bronco Wine Company, subject to the attached Development Standards/Mitigation Measures and Development Schedule.

**REZ PLN 2016-0066
BRONCO WINE COMPANY
AREA MAP**

CERES

SITE

E. KEYES RD.

99







S. CARPENTER RD.

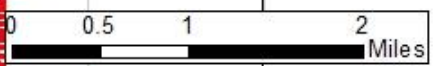
CROWS LANDING RD.

UNION PACIFIC RAILROAD

FULKERTH RD.

Legend

-  SPHERE OF INFLUENCE
-  CERES
-  COUNTY
-  MODESTO
-  TURLOCK
-  RAILROAD



6

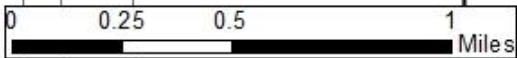
EXHIBIT B

**REZ PLN 2016-0066
BRONCO WINE COMPANY
GENERAL PLAN MAP**

SITE

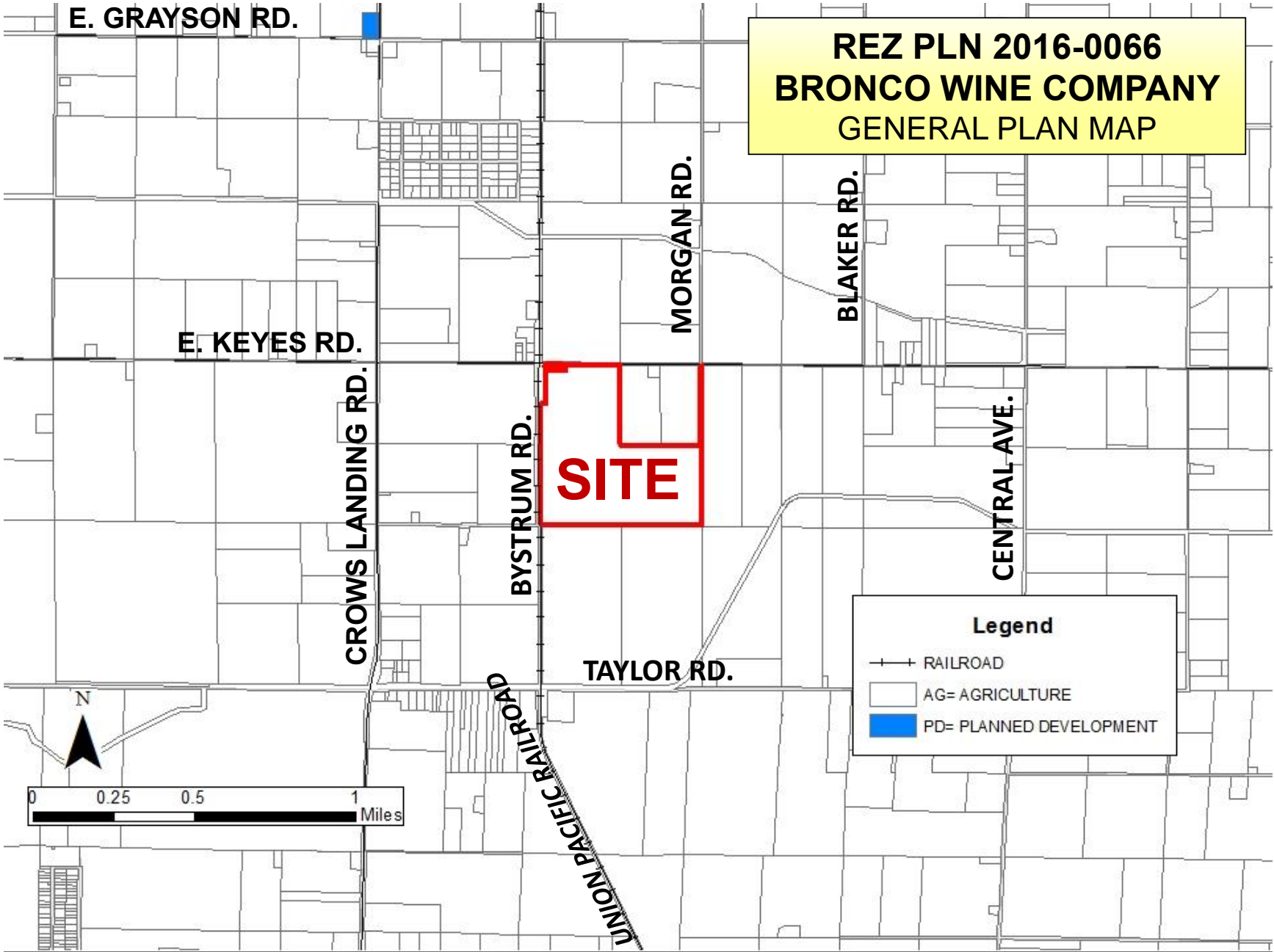
Legend

- +— RAILROAD
- AG= AGRICULTURE
- PD= PLANNED DEVELOPMENT



10

EXHIBIT B-1



E. GRAYSON RD.

**REZ PLN 2016-0066
BRONCO WINE COMPANY
ZONING MAP**

E. KEYES RD.

MORGAN RD.

BLAKER RD.

CROWS LANDING RD.

BYSTRUM RD.




SITE

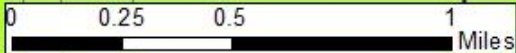
CENTRAL AVE.

TAYLOR RD.

UNION PACIFIC RAILROAD

Legend

- +— RAILROAD
-  A-2-10= AGRICULTURE
-  A-2-40= AGRICULTURE
-  P-D= PLANNED DEVELOPMENT



**REZ PLN 2016-0066
BRONCO WINE COMPANY
2015 COUNTY AERIAL**

E. KEYES RD.

BYSTRUM RD.

CROWS LANDING RD.

12

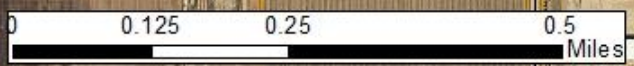
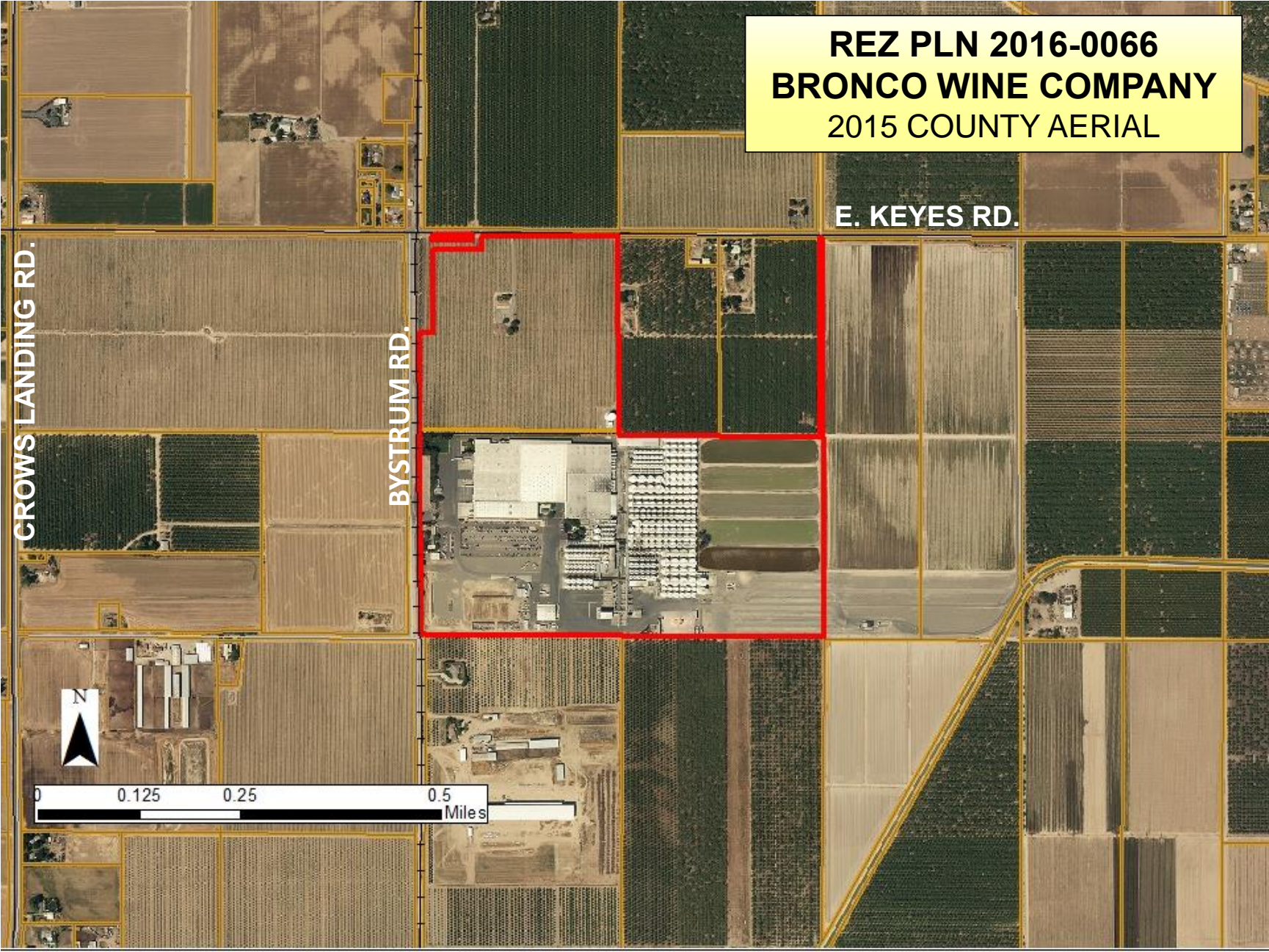
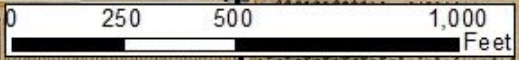


EXHIBIT B-3



**REZ PLN 2016-0066
BRONCO WINE COMPANY
2015 COUNTY AERIAL**

BYSTRUM RD.

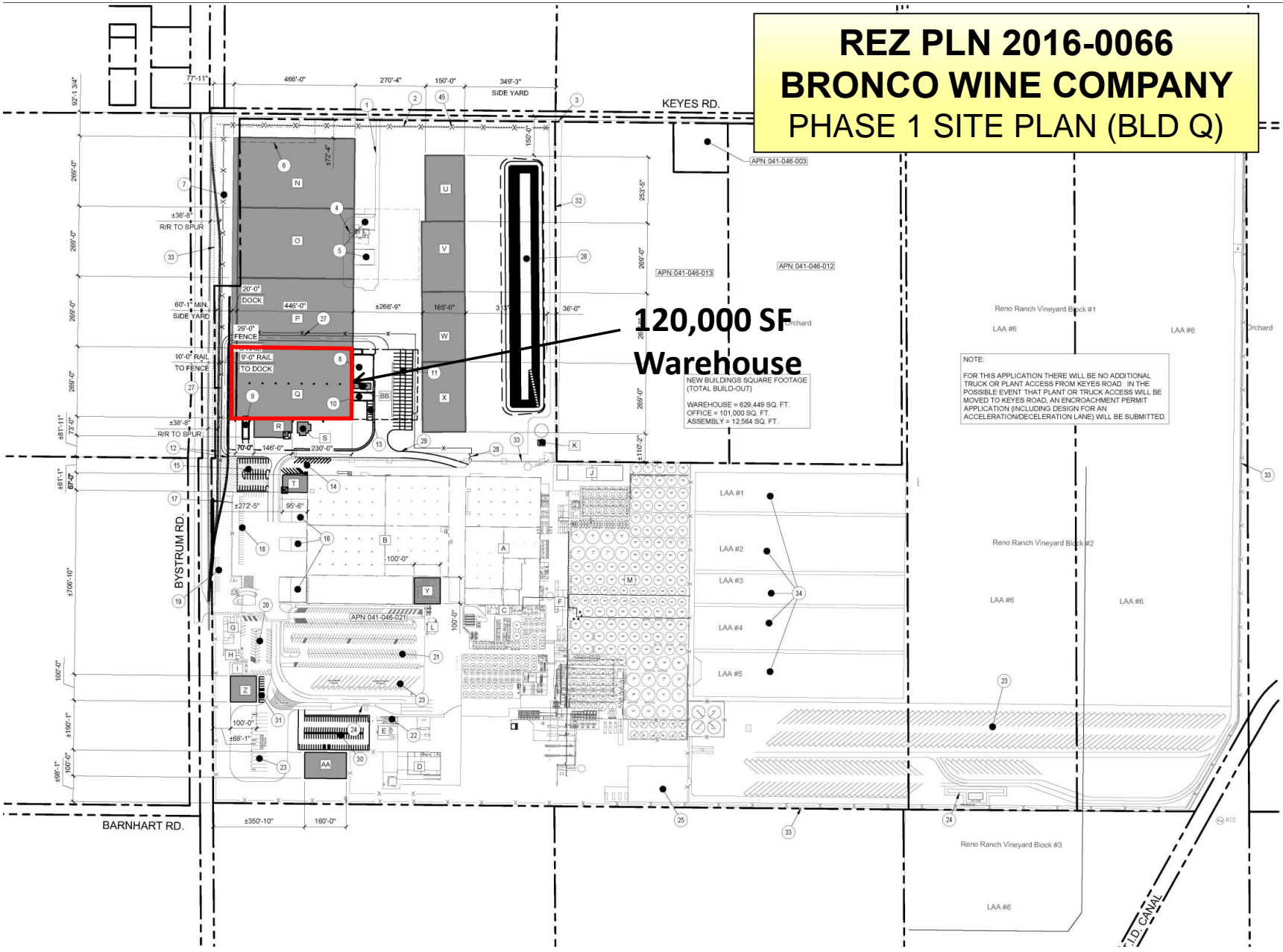


REZ PLN 2016-0066 BRONCO WINE COMPANY PHASE 1 SITE PLAN (BLD Q)

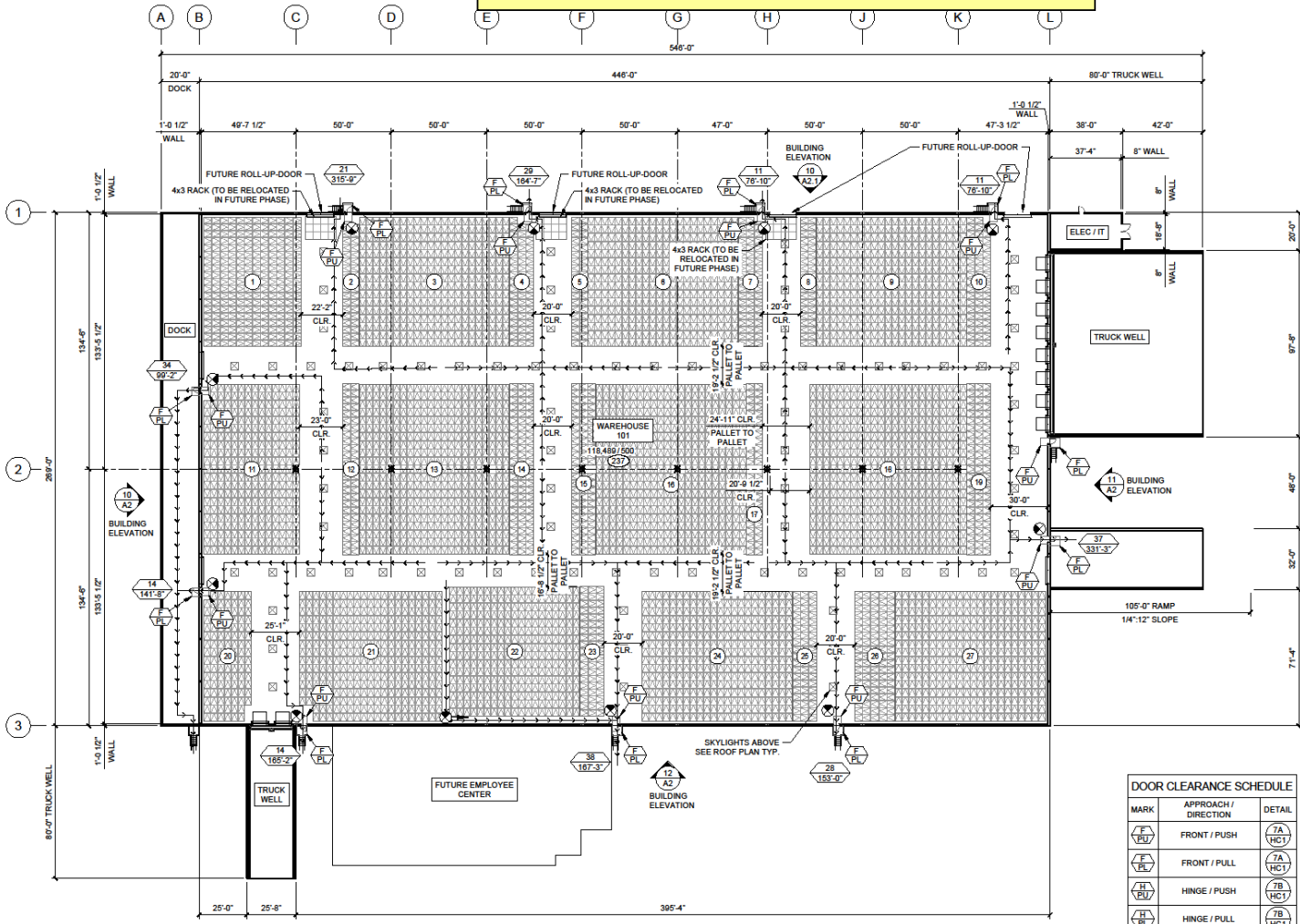
120,000 SF
Warehouse

NEW BUILDINGS SQUARE FOOTAGE
(TOTAL BUILD-OUT)
WAREHOUSE = 629,449 SQ. FT.
OFFICE = 101,000 SQ. FT.
ASSEMBLY = 12,564 SQ. FT.

NOTE:
FOR THIS APPLICATION THERE WILL BE NO ADDITIONAL TRUCK OR PLANT ACCESS FROM KEYES ROAD. IN THE POSSIBLE EVENT THAT PLANT OR TRUCK ACCESS WILL BE MOVED TO KEYES ROAD, AN ENCROACHMENT PERMIT APPLICATION (INCLUDING DESIGN FOR AN ACCELERATION/DECELERATION LANE) WILL BE SUBMITTED.



REZ PLN 2016-0066 BRONCO WINE COMPANY PHASE 1 FLOOR PLAN (BLD Q)



| PALLET SPACE COUNT | | | | |
|-----------------------|----------------|-------------------------------|------------|-----------------------------|
| AREA | COLUMNS x ROWS | PALLETS LOST TO COLL. OR DOOR | AREA TOTAL | FORKLIFT APPROACH DIRECTION |
| 1 | 12 19 | 0 | 228 | EAST-WEST |
| 2 | 2 18 | 0 | 36 | EAST-WEST |
| 3 | 16 22 | 0 | 352 | NORTH-SOUTH |
| 4 | 3 19 | 2 | 55 | EAST-WEST |
| 5 | 2 19 | 0 | 38 | EAST-WEST |
| 6 | 16 22 | 0 | 352 | NORTH-SOUTH |
| 7 | 3 19 | 1 | 56 | EAST-WEST |
| 8 | 14 16 | 0 | 224 | EAST-WEST |
| 9 | 22 16 | 0 | 352 | NORTH-SOUTH |
| 10 | 3 19 | 1 | 56 | EAST-WEST |
| 11 | 14 21 | 3 | 291 | NORTH-SOUTH |
| 12 | 2 25 | 0 | 50 | EAST-WEST |
| 13 | 22 24 | 4 | 524 | NORTH-SOUTH |
| 14 | 3 25 | 0 | 75 | EAST-WEST |
| 15 | 3 25 | 2 | 73 | EAST-WEST |
| 16 | 22 21 | 2 | 460 | NORTH-SOUTH |
| 17 | 2 25 | 2 | 48 | EAST-WEST |
| 18 | 23 21 | 4 | 479 | NORTH-SOUTH |
| 19 | 3 25 | 0 | 75 | EAST-WEST |
| 20 | 7 16 | 3 | 109 | NORTH-SOUTH |
| 21 | 21 16 | 4 | 332 | NORTH-SOUTH |
| 22 | 19 16 | 0 | 304 | NORTH-SOUTH |
| 23 | 3 19 | 0 | 57 | EAST-WEST |
| 24 | 22 16 | 0 | 352 | NORTH-SOUTH |
| 25 | 3 19 | 0 | 57 | EAST-WEST |
| 26 | 5 19 | 0 | 95 | EAST-WEST |
| 27 | 22 16 | 0 | 352 | NORTH-SOUTH |
| TOTAL PALLET SPACES = | | | 5482 | |

| DOOR CLEARANCE SCHEDULE | | |
|-------------------------|----------------------|--------|
| MARK | APPROACH / DIRECTION | DETAIL |
| | FRONT / PUSH | 7A HC1 |
| | FRONT / PULL | 7A HC1 |
| | HINGE / PUSH | 7B HC1 |
| | HINGE / PULL | 7C HC1 |
| | LATCH / PUSH | 7C HC1 |
| | LATCH / PULL | 7C HC1 |

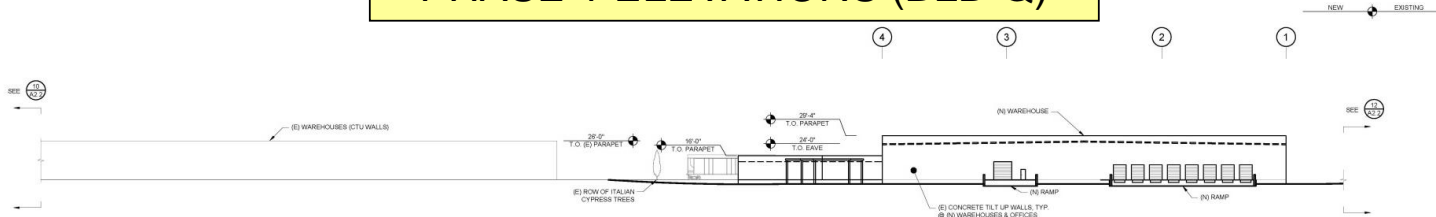
15

EXHIBIT B-6

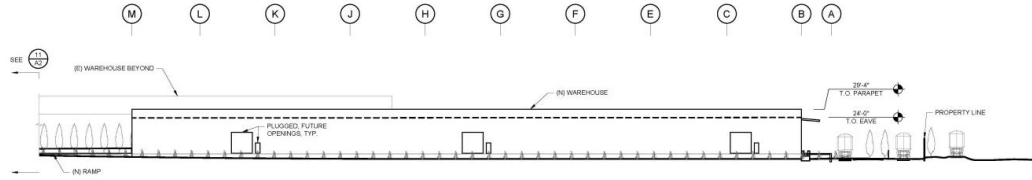
OCCUPANCY AND EGRESS CALCULATIONS
EGRESS WIDTH:

NOTE:

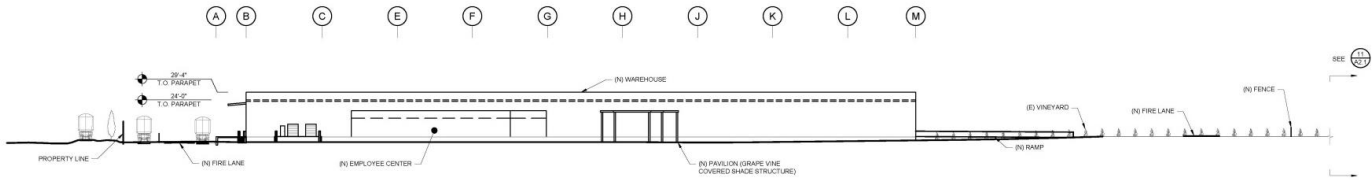
REZ PLN 2016-0066 BRONCO WINE COMPANY PHASE 1 ELEVATIONS (BLD Q)



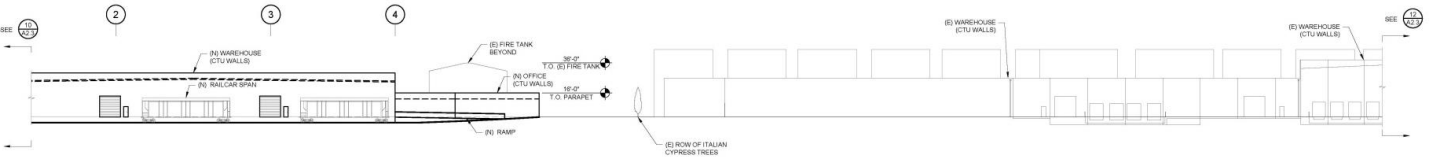
11 SCALE 1/32" = 1'-0" EAST EXTERIOR ELEVATION (FACING WEST - CENTER) WESTERN PORTION OF PARCEL ONLY



12 SCALE 1/32" = 1'-0" NORTH EXTERIOR ELEVATION (FACING SOUTH - WEST SIDE)

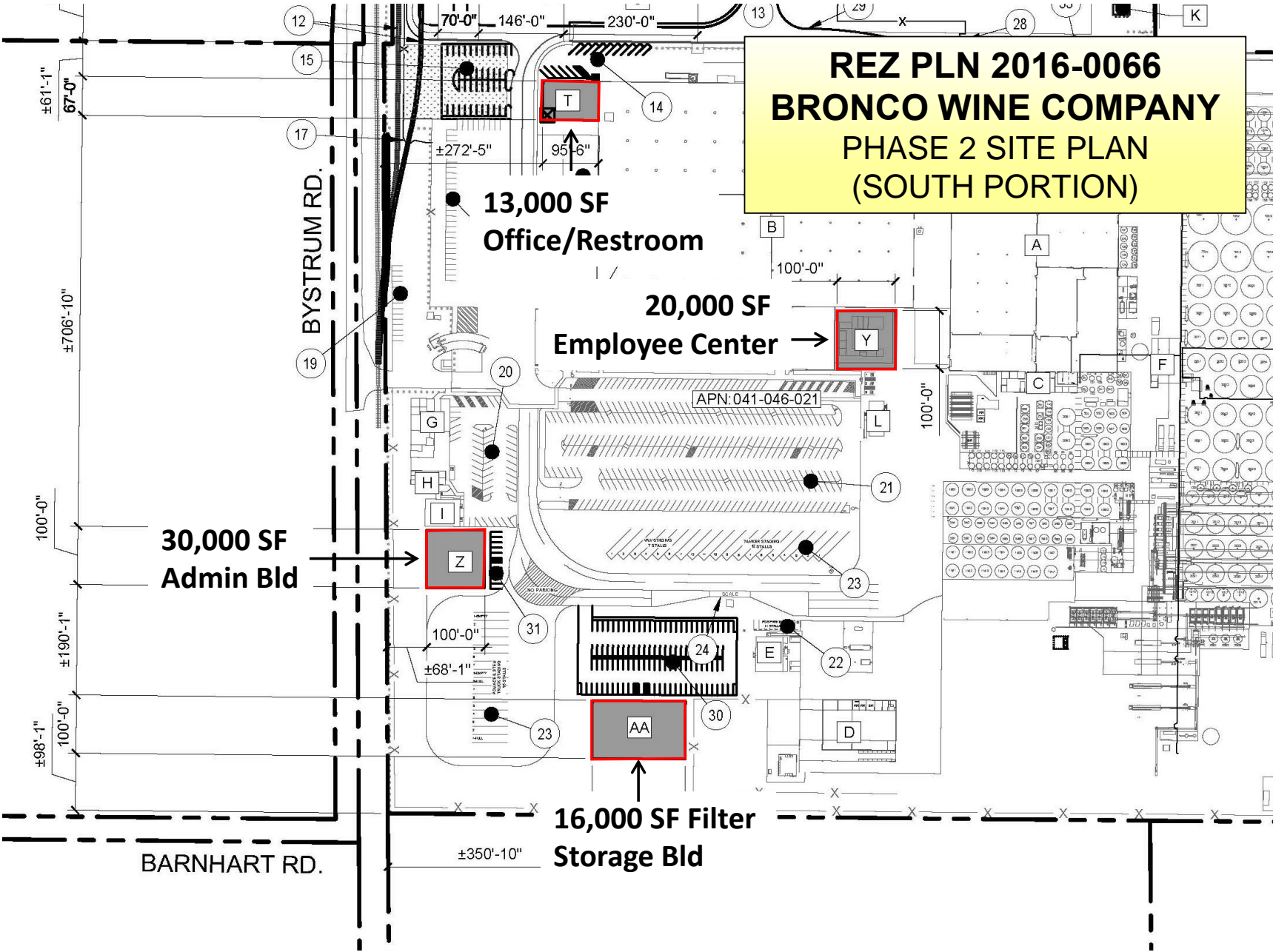


13 SCALE 1/32" = 1'-0" SOUTH EXTERIOR ELEVATION (FACING NORTH - WEST SIDE) NORTHERN PORTION OF PARCEL ONLY

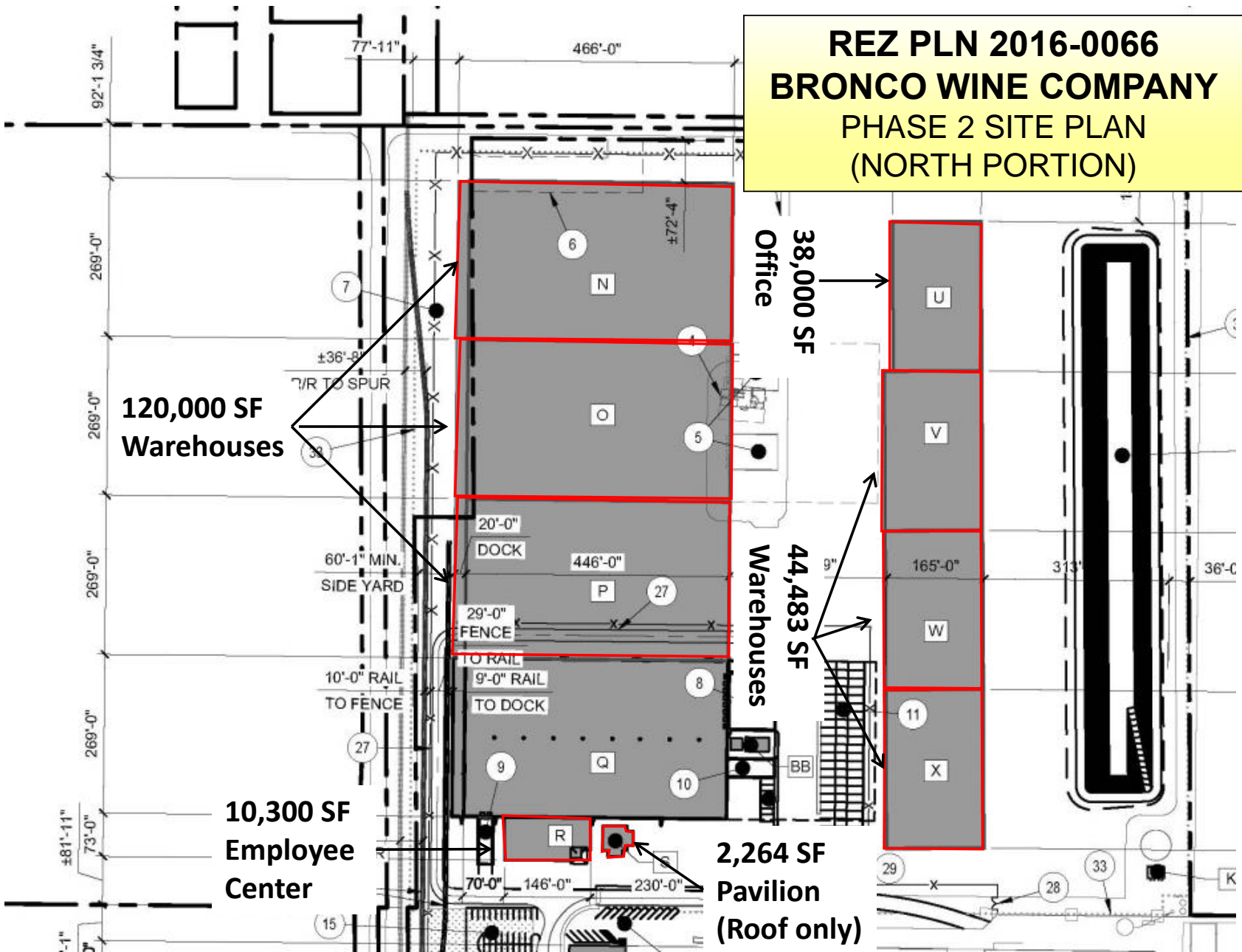


14 SCALE 1/32" = 1'-0" WEST EXTERIOR ELEVATION (FACING EAST - CENTER)

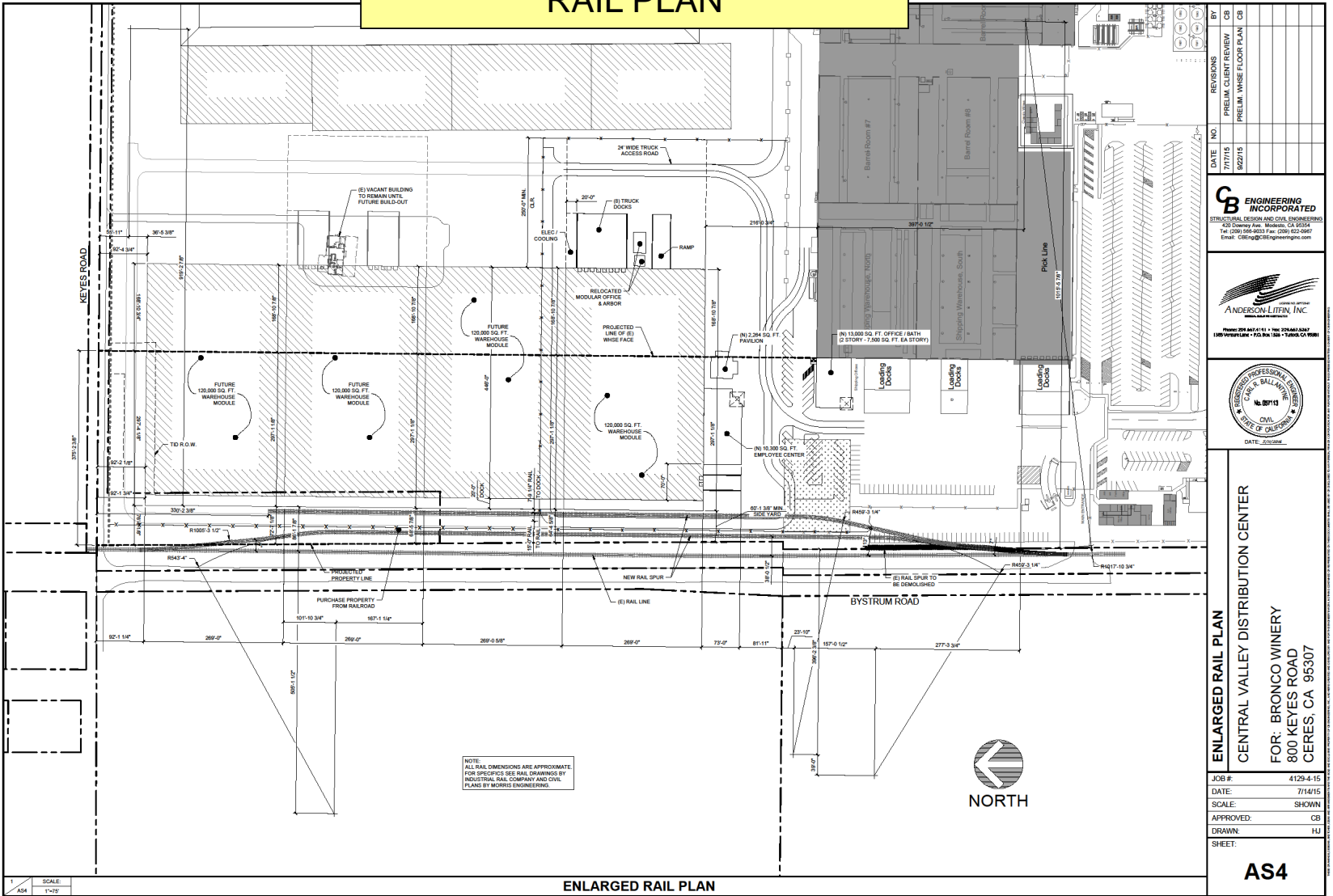
**REZ PLN 2016-0066
BRONCO WINE COMPANY
PHASE 2 SITE PLAN
(SOUTH PORTION)**



**REZ PLN 2016-0066
BRONCO WINE COMPANY
PHASE 2 SITE PLAN
(NORTH PORTION)**



REZ PLN 2016-0066 BRONCO WINE COMPANY RAIL PLAN



NOTE:
ALL RAIL DIMENSIONS ARE APPROXIMATE
FOR SPECIFICS SEE RAIL DRAWINGS BY
INDUSTRIAL RAIL COMPANY AND CIVIL
PLANS BY MORRIS ENGINEERING.

| DATE | NO. | REVISIONS | BY |
|---------|-----|-----------------------------|----|
| 7/17/15 | | PRELIM CLIENT REVIEW | CB |
| 8/22/15 | | PRELIM WAREHOUSE FLOOR PLAN | CB |

ENGINEERING INCORPORATED
STRUCTURAL DESIGN AND CIVIL ENGINEERING
420 Courtney Ave., Modesto, CA 95354
Tel: (209) 568-8231 Fax: (209) 562-5867
Email: CBE@CBEEngineering.com

ANDERSON LITVIN, INC.
REGISTERED PROFESSIONAL ENGINEER
No. 87113
CIVIL
STATE OF CALIFORNIA
DATE: 3/20/15

ENLARGED RAIL PLAN
CENTRAL VALLEY DISTRIBUTION CENTER
FOR: BRONCO WINERY
800 KEYES ROAD
CERES, CA 95307

JOB #: 4129-4-15
DATE: 7/14/15
SCALE: SHOWN
APPROVED: CB
DRAWN: HJ
SHEET:

AS4

ENLARGED RAIL PLAN

**REZ PLN 2016-0066
BRONCO WINE COMPANY
2015 SITE PHOTOS**



**SOUTHEAST PORTION OF SITE
EXISTING WINE TANKS & TRUCK DOCKS**

**SOUTHEAST PORTION OF SITE
EXISTING WINE TANKS**



**REZ PLN 2016-0066
BRONCO WINE COMPANY
2015 SITE PHOTOS**



**WEST ENTRANCE
BYSTRUM RD. AND UNION PACIFIC RR**

**SOUTHWEST CORNER OF SITE
BYSTRUM RD. AND UNION PACIFIC RR**



NOTE: Approval of this application is valid only if the following conditions are met. This permit shall expire unless activated within 18 months of the date of approval. In order to activate the permit, it must be signed by the applicant and one of the following actions must occur: (a) a valid building permit must be obtained to construct the necessary structures and appurtenances; or, (b) the property must be used for the purpose for which the permit is granted. (Stanislaus County Ordinance 21.104.030)

DEVELOPMENT STANDARDS

REZONE APPLICATION NO. PLN2016-0066 BRONCO WINE COMPANY

Department of Planning and Community Development

1. Use(s) shall be conducted as described in the application and supporting information (including the plot plan) as approved by the Planning Commission and/or Board of Supervisors and in accordance with other laws and ordinances. All development standards of P-D (6) shall be superseded and governed by these development standards.
2. Pursuant to Section 711.4 of the California Fish and Game Code (effective January 1, 2017), the applicant is required to pay a California Department of Fish and Wildlife (formerly the Department of Fish and Game) fee at the time of filing a "Notice of Determination." Within five (5) days of approval of this project by the Planning Commission or Board of Supervisors, the applicant shall submit to the Department of Planning and Community Development a check for **\$2,273.25**, made payable to **Stanislaus County**, for the payment of California Department of Fish and Wildlife and Clerk Recorder filing fees.

Pursuant to Section 711.4 (e) (3) of the California Fish and Game Code, no project shall be operative, vested, or final, nor shall local government permits for the project be valid, until the filing fees required pursuant to this section are paid.
3. Developer shall pay all Public Facilities Impact Fees and Fire Facilities Fees as adopted by Resolution of the Board of Supervisors. The fees shall be payable at the time of issuance of a building permit for any construction in the development project and shall be based on the rates in effect at the time of building permit issuance.
4. The applicant/owner is required to defend, indemnify, or hold harmless the County, its officers, and employees from any claim, action, or proceedings against the County to set aside the approval of the project which is brought within the applicable statute of limitations. The County shall promptly notify the applicant of any claim, action, or proceeding to set aside the approval and shall cooperate fully in the defense.
5. During any future construction, if any human remains, significant or potentially unique, are found, all construction activities in the area shall cease until a qualified archeologist can be consulted. Construction activities shall not resume in the area until an on-site archeological mitigation program has been approved by a qualified archeologist. The Central California Information Center shall be notified if the find is deemed historically or culturally significant.

6. Pursuant to Section 404 of the Clean Water Act, prior to construction, the developer shall be responsible for contacting the US Army Corps of Engineers to determine if any "wetlands," "waters of the United States," or other areas under the jurisdiction of the Corps of Engineers are present on the project site, and shall be responsible for obtaining all appropriate permits or authorizations from the Corps, including all necessary water quality certifications, if necessary.
7. Any construction resulting from this project shall comply with standardized dust controls adopted by the San Joaquin Valley Air Pollution Control District (SJVAPCD) and may be subject to additional regulations/permits, as determined by the SJVAPCD.
8. A sign plan for all proposed on-site signs indicating the location, height, area of the sign(s), and message must be approved by the Planning Director or appointed designee(s) prior to installation.
9. Pursuant to Sections 1600 and 1603 of the California Fish and Game Code, prior to construction, the developer shall be responsible for contacting the California Department of Fish and Game and shall be responsible for obtaining all appropriate stream-bed alteration agreements, permits, or authorizations, if necessary.
10. The Department of Planning and Community Development shall record a Notice of Administrative Conditions and Restrictions with the County Recorder's Office within 30 days of project approval. The Notice includes: Conditions of Approval/Development Standards and Schedule; any adopted Mitigation Measures; and a project area map.
11. Pursuant to the federal and state Endangered Species Acts, prior to construction, the developer shall be responsible for contacting the US Fish and Wildlife Service and California Department of Fish and Game to determine if any special status plant or animal species are present on the project site, and shall be responsible for obtaining all appropriate permits or authorizations from these agencies, if necessary.
12. Pursuant to State Water Resources Control Board Order 99-08-DWQ and National Pollutant Discharge Elimination System (NPDES) General Permit No. CAS000002, prior to construction, the developer shall be responsible for contacting the California Regional Water Quality Control Board to determine if a "Notice of Intent" is necessary, and shall prepare all appropriate documentation, including a Storm Water Pollution Prevention Plan (SWPPP). Once complete, and prior to construction, a copy of the SWPPP shall be submitted to the Stanislaus County Department of Public Works.
13. A use permit shall be obtained prior to development of any future phases, beyond the uses included in Phase 1 of this project request. As part of the use permit process, additional CEQA analysis shall be completed to evaluate potential environmental impacts, specifically to air quality.

Prior Development Standards

14. No structures or ponding areas to be located closer than 50 feet to property lines and tree screening to be provided along the property lines adjacent to any structure or ponding area located within 75 feet of such property line.

15. Parking shall be developed as buildings are constructed as required by Chapter 21.76 Off-site Parking. Driveways and parking areas to be blacktopped and on-site drainage provided as approved by the Department of Public Works.
16. Should additional ponding areas be added to the project site, the ponding areas shall be a maximum of six inches in depth and 10 foot wide roadways shall be provided around all ponds to provide access for Turlock Mosquito Abatement District staff. Lighting of new pond areas shall be approved by the Turlock Mosquito Abatement District. All pond wastewater shall be rotated daily and ponding areas shall be kept free of weeds and maintained for mosquito control as required by the Turlock Mosquito Abatement District.
17. All trucks servicing this development must restrict ingress and egress from Keyes Road to the existing entrance on Bystrum Road. In no case shall truck traffic use Barnhart Road or the 20-foot panhandle extending from east side of the subject property to Keyes Road.
18. All future railroad improvements, including but not limited to any crossings at entrances to the site, shall be constructed as approved by the Union Pacific Railroad and all applicable government agencies.
19. Grape pomace shall be removed from the project site on a daily basis.
20. On-site truck parking and circulation shall be constructed as required by the Department of Public Works.
21. Bystrum Road shall not be used for parking or storage by trucks servicing the winery.
22. The ponding operation shall not create a public nuisance as defined by the Code of Civil Procedures.

Department of Public Works

23. Prior to the issuance of any building or grading permit, not including building or grading permits issued for Building Q, street improvement plans, per the California Highway Design Manual, for dedicated turn lanes at the intersection of Keyes Road and Bystrum Road shall be submitted to and approved by the Department of Public Works. These plans shall be approved prior to the issuance of any building or grading permit submitted after Building Q.
24. Prior to acceptance of the road improvements, a set of Record Drawings, as specified in the County's Standards and Specifications, and scanned files for each sheet in a PDF format shall be provided to the Department of Public Works for review and approval.
25. Prior to the issuance of any grading, building, or encroachment permit, not including building or grading permits issued for Building Q, an acceptable financial guarantee for the Keyes Road and Bystrum Road intersection road improvements shall be provided to the Department of Public Works. A financial guarantee is not required if the work in the right-of-way is completed prior to the issuance of any grading or building permit, not including building or grading permits for Building Q.
26. An Engineer's Estimate shall be provided for the road improvements so that the amount of the financial guarantee can be determined for the improvements in the County road right-of-way.

27. An Encroachment Permit shall be obtained for any work completed in the Stanislaus County road right-of-way.
28. No parking, loading or unloading of vehicles will be permitted within the Keyes Road and Bystrum Road rights-of-way. The applicant will be required to install or pay for the installation of any signs and/or markings, coordinating the installation of the signs with Public Works Traffic Section.
29. Public Works shall approve the location and width of any new driveway approaches on any County maintained roadway.
30. Prior to issuance of a grading or building permit, a grading, drainage, and erosion/sediment control plan for the project site shall be submitted before any grading or building occurs. Public Works will review and approve the drainage calculations. The grading and drainage plan shall include the following information:
 - A. Drainage calculations shall be prepared as per the Stanislaus County Standards and Specifications that are current at the time the permit is issued.
 - B. The plan shall contain enough information to verify that all runoff will be kept from going onto adjacent properties and Stanislaus County road right-of-way.
 - C. The grading, drainage, erosion/sediment control plan shall comply with the current State of California National Pollutant Discharge Elimination System (NPDES) General Construction Permit and Stanislaus County storm water treatment and quality standards.
 - D. The grading, drainage, and associated work shall be accepted by Stanislaus County Public Works prior to a final inspection or occupancy, as required by the grading or building permit.
 - E. The permit applicant shall pay the current Stanislaus County Public Works weighted labor rate for the plan review and all on-site inspections required for the grading, drainage, erosion/sediment control, or building permit plan. The Public Works inspector shall be contacted 48 hours prior to the commencement of any grading or drainage work on-site. The plans shall not be released until such time that all plan check and inspection fees have been paid.
31. As reflected on Exhibit B-11 - Traffic and Fire Lanes, dated July 14, 2015, provided as part of the project application, no additional trucks will be allowed to utilize the Keyes Entrance labeled as Keyes Road Entrance "B", also known as Pike Road. Truck trips are limited to the number of trips included on the "Traffic and Fire Lanes" exhibit as follows:

Existing

- In Season:
 - Grape Trucks 105 /day and 735/week
 - Pomace Trucks 23/day and 163/week
- Out of Season:
 - Tanker Trucks 34/day and 230/week

Phase 1 Expansion

- In Season:
 - Grape Trucks 105 /day and 735/week
 - Pomace Trucks 23/day and 163/week
- Out of Season:
 - Tanker Trucks 34/day and 230/week

Department of Environmental Resources

32. The applicant shall contact the Department of Environmental Resources (DER) regarding appropriate permitting requirements for hazardous materials and/or wastes. Applicant and/or occupants handling hazardous materials or generating hazardous wastes must notify DER relative to the following:
- A. Permits for the underground storage of hazardous substances at new or modified tank facilities.
 - B. Requirements for registering as a handler of hazardous materials in the County.
 - C. Submittal of Hazardous Materials Business information into the California Electronic Reporting System (CERS) by handlers of materials in excess of 55 gallons or 500 pounds of hazardous material, or of 200 cubic feet of compressed gas.
 - D. The handling of acutely hazardous materials may require the preparation of a Risk Management Prevention Program, which must be implemented prior to operation of the facility. The list of acutely hazardous materials can be found in SARA, Title III, Section 302.
 - E. Generators of hazardous waste must notify the Department relative to the: (1) quantities of waste generated; (2) plans for reducing wastes generated; and (3) proposed waste disposal practices. Generators of hazardous waste must also use the CERS database to submit chemical and facility information to DER.
 - F. Permits for the treatment of hazardous waste on-site will be required from the Hazardous Materials Division of DER.
33. The California Health and Safety Code Sections 25534 and 25535.1, require that stationary source facilities that handle or store acutely hazardous materials in reportable quantities develop a Risk Management Plan (RMP) and submit it to the local administering agency for review and approval. Stanislaus County Department of Environmental Resources is the administering agency for facilities subject to an RMP. For changes involving chemical engineering, hazardous materials systems and equipment, and in the storage of acutely hazardous materials, that require modification of a facilities RMP shall be documented appropriately and submitted to DER as per requirements of California Health and Safety Code Section 25534.
34. Prior to the issuance of a grading permit, applicants shall determine, to the satisfaction of the Department of Environmental Resources (DER), that a site containing (or formerly containing) residences or farm buildings, or structures, has been fully investigated (via Phase I Study, and if necessary, Phase II Study). DER recommends a thorough records search be conducted to determine the historical types of farming operations performed at the project site; and that based on the results of that information, any suspect areas of the

proposed development site be tested for organic pesticides and metals. Any discovery of underground storage tanks, former underground storage tank locations, buried chemicals, buried refuse, or contaminated soil shall be brought to the immediate attention of DER.

35. Property owner/developer shall obtain the appropriate permit from the Stanislaus County Department of Environmental Resources (DER) prior to installation (or destruction) of monitoring wells, and performance of exploratory soil borings for purposes of geotechnical and/or environmental assessment. All drilling for these purposes shall be performed by a C-57 Licensed California Well Driller, and according to applicable standards set forth in California Well Standards Bulletin 74-90.
36. Prior the installation of any water infrastructure for the site, the property owner shall provide to the Department of Environmental Resources an application for amended water supply permit along with a full technical report demonstrating that the water system will meet all requirements of a Nontransient Noncommunity water system: capacity, source water, treatment plant modifications, water works standards, and the California Environmental Quality Act (CEQA).
37. This site has multiple wastewater treatment systems (WTS) some are conventional septic and others are aerobic treatment units (ATU). Most of these system dispersal systems are under paved area. The groundwater or the water system has shown high levels of nitrates and di-Bromo-Chloro-Propane (DBCP) as of these products in the groundwater the water well on-site has couple of treatments one for the removal of inorganic (Nitrate) and the second is the removal of organic (DBCP).
38. The expansion of this project will have an increase of water treatment which will impact on the wastewater treatment by the side products from the treatments. Therefore, any proposed on-site wastewater treatment system or expansion of existing on-site waste water system (OWTS) need to be referred to Central Valley Regional Water Quality Control Board (CVRWQCB), for review and approval. A centralized OWTS may be required by CVRWQCB with proper treatment of the discharged effluent. The quality of the discharged effluent shall meet EPA Secondary Treatment Guidelines. The focus will be on the ability to reduce nitrate, salt, and organic chemical levels, which have a minimum impact upon the area's groundwater

Building Permits Division

39. Building permits are required and the project must conform to the California Code of Regulations, Title 24.

Keyes Fire District

40. All proposed structures shall obtain building permits, shall meet all applicable Building and Fire codes, and shall be reviewed and approved by the Keyes Fire District.

Turlock Irrigation District

41. There is an existing privately owned 36 inch irrigation pipeline which enters the subject property from the east at approximately 670 feet south of Keyes Road. The portion of this pipeline remaining on the subject parcel shall be removed as the area(s) develop and the opening in the control structure along the east property line sealed.

42. A second 36 inch diameter cast-in-place concrete irrigation pipeline and easement belonging to Improvement District 1121, the Moore, is located in the northwest corner of the project. The pipeline crosses Keyes Road approximately 400 feet east of Blaker Road and then turns westerly parallel to Keyes Road. The portion crossing the road appears to be within the existing irrigation easement. However, the east-west portion is located about 18 feet south of the Keyes Road right-of-way and outside of the easement. It is likely that the existing easement could be quitclaimed in lieu of dedication of a new easement to cover the actual location of the pipeline.
43. The developer shall submit plans detailing the existing irrigation facilities, relative to the proposed site improvements, in order for the District to determine specific impacts and requirements.
44. The District shall review and approve all maps and plans for the project. Any improvements to this property which impact irrigation facilities shall be subject to the District's approval and shall meet all District standards and specifications. If it is determined that irrigation facilities will be impacted, the applicant will need to provide irrigation improvement plans and enter into an Irrigation Improvements Agreement for the required irrigation facility modifications. There is a District Board approved time and material fee associated with this review.
45. Work on irrigation facilities can only be performed during the non-irrigation season which typically runs from November 1, through March 1, but can vary.
46. Upon request, the District will review and quitclaim irrigation easements that are no longer required. There is a \$100.00 application fee for this review.
47. Developed property adjoining irrigated ground must be graded so that finished grading elevations are at least 6 inches higher than irrigated ground. A protective berm must be installed to prevent irrigation water from reaching non-irrigated properties.
48. Developer shall determine how the new electrical load will be connected to their primary metered electrical system and ensure that the total plant load can receive satisfactory service from developer's primary metered service. The new total electrical demand shall be reviewed and approved by the District to ensure total load is within the limits of the District's electrical feeders that serve the development.
49. The District has a transmission and distribution line on the south side of Keyes Road along the northern boundary of the subject property. Applicant proposes to plant Italian Cypress trees parallel to the line. These trees shall be planted 20 feet from the line to ensure safe and reliable operation of the electric system.

Central Valley Regional Water Quality Control Board

50. Project shall obtain all applicable permits in accordance with the National Pollutant Discharge Elimination System and land discharge Waste Discharge Requirements (WDRs). All wastewater discharges must comply with the Antidegradation Policy (State Water Board Resolution 68-16) and the Antidegradation Implementation Policy contained in the Basin Plan.
51. Prior to ground disturbance or issuance of a building permit, the Central Valley Regional Quality Control Board shall be consulted to obtain any necessary permits and to implement any necessary measures, including but not limited to Construction Storm Water General

Permit, Phase I and II Municipal Separate Storm Sewer System (MS4) Permits, Industrial Storm Water General Permit, Clean Water Act Section 404 Permit, Clean Water Act Section 401 Permit (Water Quality Certification), Waste Discharge Requirements, Dewatering Permit, Low or Limited Threat General NPDES Permit, NPDES Permit or any other applicable Regional Water Quality Control Board permit.

San Joaquin Valley Air Pollution Control District

52. The proposed project shall obtain all applicable Air District permits. Prior to the start of construction, the property owner/operator shall contact the District's Small Business Assistance Office at (559) 230-5888 to determine if an Authority to Construct (ATC) is required, or if any other District rules or permits are required.

MITIGATION MEASURES

(Pursuant to California Public Resources Code 15074.1: Prior to deleting and substituting for a mitigation measure, the lead agency shall do both of the following:

- 1) Hold a public hearing to consider the project; and
2) Adopt a written finding that the new measure is equivalent or more effective in mitigating or avoiding potential significant effects and that it in itself will not cause any potentially significant effect on the environment.)***

53. All exterior lighting shall be designed (aimed down and toward the site) to provide adequate illumination without a glare effect. This shall include but not be limited to: the use of shielded light fixtures to prevent skyglow (light spilling into the night sky) and to prevent light trespass (glare and spill light that shines onto neighboring properties).
54. Prior to issuance of a building permit, not including the building permit for Phase 1, which includes construction of the 120,000 square-foot warehouse (Building Q), improvements to alleviate traffic congestion at the intersection of Keyes Road and Bystrum Road and to improve safety conditions along Keyes Road, to include dedicated turn lanes per the California Highway Design Manual, shall be completed. Improvement plans shall be reviewed and approved by the Stanislaus County Department of Public Works.

*Please note: If Conditions of Approval/Development Standards are amended by the Planning Commission or Board of Supervisors, such amendments will be noted in the upper right-hand corner of the Conditions of Approval/Development Standards; new wording is in **bold**, and deleted wording will have a ~~line through it~~.*

DEVELOPMENT SCHEDULE

REZONE APPLICATION NO. PLN 2016-0066 BRONCO WINE COMPANY

Phase 1 – Includes construction of a 120,000 square foot warehouse (Building Q) to be utilized for the storage of bottled wine stock, construction of two rail spurs to be utilized for deliveries, a fleet of 53 foot long trucks and tanker trucks, and fencing around the perimeter of the new warehouse. Development will occur within five years of project approval.

Future Phases – All other proposed uses will be included in future phases to be constructed according to market demand and will require a Use Permit be obtained. The parking lot expansion will occur as required for each building constructed. Fencing and landscaping around the entire project site to be completed with the first building permit to be issued for the next Phase.

Business Model of Bronco Wine Company

In support of its new Land Use Permit

Application before Stanislaus County

March 16, 2017

I. FAMILY HISTORY

Coca Cola Bottling Company of New York purchased the Franzia Brothers Winery, Ripon, California in 1973. Bronco's Founders and owners resigned their position from Coca Cola to start Bronco Wine Company in 1974. Bronco's Founders selected Stanislaus County, rather than San Joaquin County, as the preferred winery site to grow our Bronco Wine Company. During the past 44 years, Bronco Wine Company has grown our winery by selling wine to American consumers at prices they can afford to enjoy wine every day. In order to supplement a low cost supply source for grapes, various Bronco partnerships have acquired and planted over 40,000 acres of wine grape vineyards in eleven (11) counties primarily in the San Joaquin Valley. Bronco purchases grapes from growers and produces wine at Ceres for bottling both still and sparkling wine and also warehouses and distributes these wines at Ceres. Bronco also sells bulk wine from our Ceres winery to dozens of other California wineries. Bronco maintains a wholesale business in California, selling directly to retailers and restaurants (on sale (i. e. wine consumed on the premise where purchased, like a restaurant) and off sale (i. e. wine consumed at a location other than where purchased, like in the purchaser's home) from our Ceres warehouse. Bronco also markets and sells numerous branded wines to distributors (wholesalers) in the other 49 states and exports both bulk and bottled wines to over 70 countries including 10 provinces in Canada. The National and California case sales divisions of Bronco sell both Bronco produced brands and brands produced by other California wineries, as well as wines imported from around the world directly from our warehouse at Ceres. In addition, Bronco and other affiliated companies built or acquired additional bonded wineries and bonded bottling facilities, warehouses and logistics services in five counties other than Stanislaus. All these wines and brands need a consolidation point for shipping to our distributors. This full complement of wines is necessary for Bronco to compete with major national wineries like E&J Gallo (Stanislaus County); The Wine Group - FKA Franzia Brothers Winery (San Joaquin County), and Delicato Family Winery (San Joaquin County). Most recently, Bronco is launching a new distribution logistics business to be more vertically integrated and again, as our first choice, we want to build a consolidation point to ship wine on a more economical basis from a central location from our Ceres warehouse/distribution point. Trucks returning from case good delivery can backhaul wines for consolidation at Ceres. Bronco has been appointed a Freight Consolidator for our largest national retail customer from our supply base to eventually accommodate rail shipments to our major markets.

II. INVENTORY POSITION

The Planning Department inquired about the mix of inventory in the Ceres warehouse. During Bronco's recent physical inventory, audited by our Independent Public Accountants, the mix of case goods was

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1,874,413 cases (88%) produced and owned by Bronco; 179,458 cases (8%) produced by other California wineries and 91,001 cases (4%) imported from other countries.

III. BUSINESS MODEL IN SUPPORT OF REDUCED TRUCK TRAFFIC

The following discusses how Bronco and its affiliated logistics and trucking company will reduce truck loads into and out of the Ceres facility and also addresses proposed new office buildings and related parking.

- 1) Bronco is investing millions of dollars to build a new warehouse which will store finished cases of wine produced by Bronco at the Ceres winery, at affiliated wineries in Napa and Sonoma, other portfolio wineries throughout California and overseas wineries. Bronco is also investing additional millions of dollars for railroad access into the area adjacent to the new warehouse. Each railroad car has a capacity equal to 4 - 53' truck vans. Bronco sells to over 370 distributors throughout the United States whose terms are FOB, Ceres. The wines are picked up by these distributors in 53' vans. One trip in (empty) and one trip out (full). We are initially working with large distributors in 10 states which have already requested wine to be delivered on railroad cars. During the past 3 months, these distributors picked up 142,800 cases of wine in 119 trucks. This required 238 truck trips (in and out). In the future, these distributors will have this same wine delivered in railroad cars and thus will eliminate 238 truck trips (119 empty trips in and 119 full trips out of the winery) during this 3 month period. This equates to an elimination of 952 truck trips annually. The elimination of truck trips will only increase as more and more distributors realize the cost savings of consolidation 4 truckloads of wine into one rail car.
- 2) Bronco bottles wine at the Ceres facility for many non-related wineries throughout California. Currently these non-related wineries send their empty 53' trucks to Ceres, get loaded with cases of wine and depart the winery fully loaded. (2 trips). Separately, Bronco owned wines are bottled at affiliated wineries in Napa and Sonoma. A non-affiliated trucking company picks up the bottled wines in their 53' trucks in Napa or Sonoma and delivers the wine to Ceres and leaves empty. (2 loads).

Bronco, through an affiliate, will purchase four (4) 53' dry goods vans. Over the next five (5) years, the number of vans could grow to 12-15. Through negotiations with the non-related wineries, Bronco will include the cost of delivery in the bottling price and deliver the bottled wine to the non-related winery. The same van will then pick up Bronco's bottled wines at the Napa or Sonoma affiliated wineries and deliver the wine to Ceres. This model has the effect of reducing 50% of the truck traffic for each such event. During the 3 month period of November through January, a total of 878 truck trips arrived in Ceres and 199 truck trips departed Ceres for a 3 month total of 1,077. The average annual truck trips would therefore be 4,308. These trips will be cut in half and result in a truck trip elimination of 2,154 truck trips per year.

- 3) a) Bronco produces bulk wine for dozens of California non-related wineries. These non-related wineries send their empty tanker trucks to pick up the bulk wine (1 trip in). After the tanker is loaded with bulk wine, the truck returns to the non-related winery (1 trip out). Bronco, through an affiliate, will purchase ten (10) bulk wine tankers. Over the next five (5) years, the number of

bulk wine tankers could grow to 20 tankers. Through negotiations with the non-related wineries, Bronco will include the cost of delivering the bulk wine in the selling price of the bulk wine. The result is one tanker trip going out of Ceres loaded and the incoming empty tanker trip is eliminated. The Bronco tanker truck then returns to Ceres full. See discussion in the next paragraph.

b) Separately, other non-related wineries send their produced bulk wine to Ceres to be blended with Bronco's wines and further processed (1 trip in full and 1 trip out empty). The blending and processing takes weeks to finish. The non-related winery then sends an empty tank truck to Ceres for loading of bulk wine and the tank truck returns (another 2 tanker truck trips). This entire task requires 4 bulk wine tanker truck trips by the non-related winery.

Because a Bronco affiliate is purchasing bulk wine tankers, the other non-related winery's wine can be picked up for return to Ceres for the blending and processing, after the wine in 3. a) above is delivered. This will eliminate the other non-related winery's 2 bulk wine tanker truck trips. After the wine is blended, a Bronco affiliate's bulk wine tanker will take the full load to the other non-related winery, and pick up another load of bulk wine from the same non-related winery or a different non-related winery for return to Ceres. Thus, 4 bulk wine truck trips by the other non-related winery will be replaced by 2 truck trips made by Bronco's affiliate, thereby eliminating 50% of this truck traffic. This will greatly reduce the number of tank trips and eliminate the other non-related wineries' empty tankers from entering or leaving the Ceres winery. During the three period of November through January, 224 truckloads of bulk wine entered the Ceres winery and 387 truckloads of bulk wine left the Ceres winery for a total of 611 trips. The average annual truckloads would therefore be 2,444 and based on this business model, 1,222 truck trips will be eliminated.

- 4) Bronco operates as its own wholesale company within the state of California. This entity is known as Classic Wines of California (CWOC). The entity serves 6,100 retail accounts in California. Deliveries of case goods, produced by both Bronco and other non-affiliated wineries are made to retailers four days each week. The delivery trucks leave Ceres, deliver the cases at retail locations and return empty. By better logistics planning, these same trucks will now pick up other winery's wines, (which are for sale through the Bronco wholesale organization), and return to Ceres, thus eliminating a separate incoming truck trip from the other non-affiliated winery. Our logistics manager estimates that we can eliminate 552 truckloads each year which are currently hauled by outside 3rd party trucking companies.
- 5) Items III. (2), (3), and (4) above all discuss business models which include the use of trucks and delivery vans and bulk wine tankers. The pending Land Use Application does not include building a vehicle maintenance shop. Rather, minor maintenance will be provided on-site by either winery maintenance personnel, if qualified, or independent mobile maintenance firms. Major repairs will be performed off-site at truck repair shops which are located within a 5-10 mile radius of our Stanislaus County winery. The number of power units which will haul the vans and bulk wine tankers is 12 and could grow to 30 in the next five (5) years.

- 6) The Public Works and/or Planning Department expressed concerns about dramatic increases in auto trips and parking resulting from the proposed new office buildings identified as Building “U” and Building “T”. There will be NO increase in employee auto trips or parking. Bronco currently has 97 employees working in 11 departments on the Ceres campus. If these new office buildings are built, the existing 97 employees will be repositioned into these buildings. Administrative personnel growth during the next 5 years could range 2-4% total.
- 7) The General Project Summary in the traffic study makes reference to “1 future employee center (commercial kitchen/cafeteria/conference area) at 10,300 square feet (Building “R”)”. The use of this building will be as follows.
 - a) Once or twice each year, Bronco’s National sales force of approximately 68 people travel to California for meetings and educational seminars. They typically meet in Napa for 3-5 days and in Ceres for 1-2 days during the week-long meetings. Management requires the sales force to car pool in vans so that vehicle count will be approximately 11-13 vehicles for 4 days per year maximum. The meeting room currently used in Ceres is too small for this size group and also conflicts with other meeting held by winery personnel.
 - b) Bronco’s wholesale division (CWOC) holds monthly meetings for its Northern California sales force of approximately 50 managers and sales personnel. Currently all meetings are held in our Napa facility. The management of CWOC would like to conduct these meetings in Ceres once each quarter (4 times per year). Again, management requires the sales personnel to car pool, when possible. Since the personnel are disbursed throughout Northern California, the volume of vehicles is expected to be 25-30 vehicles for each quarterly meeting, or an average of 0.38-0.48 vehicles per day during each calendar quarter.
 - c) Neither this facility, nor any portion of Bronco’s Stanislaus County winery, will be open to the public. This facility will not include any on-site amplified outdoor sound systems. Food preparation will be provided by off-site catering firms.

Based on the above information and the creation of additional jobs in Stanislaus County and the increased property tax dollars to Stanislaus County, there is no reason the land use permit should not be issued.

April 9, 1974

TO: BOARD OF SUPERVISORS
FROM: PLANNING DIRECTOR
SUBJECT: BRONCO WINERY - PLANNED DEVELOPMENT

The attached packet includes the Planning Staff Report and Environmental Impact Report with respect to the Bronco Wine Company's Planned Development for construction of a winery on 81.25 acres of property located on the east side of Bystrum Road one-quarter mile south of Keyes Road.

The Planning Commission, following a public hearing, unanimously voted to forward the application to the Board of Supervisors with the following recommendations:

- A. Approval of the Environmental Impact Report
- B. Approval of the proposed development plan for the construction of a winery.
- C. Reclassification of the property from A-2-10 (Exclusive Agricultural) to P-D (Planned Development).

Included with the Staff report is Exhibit "C" (Performance standards for the completion of the Planned Development). Exhibits "A" and "B" as described in the attached Staff report will be on display at your meeting.

Attachments:

1. Staff Report
2. E.I.R.
3. Exhibit "C"

RMA:d1

RESOLUTION NO. 74-2

RESOLUTION OF THE STANISLAUS COUNTY PLANNING COMMISSION TO THE BOARD OF SUPERVISORS RECOMMENDING THE REZONING OF PROPERTY ON THE EAST SIDE OF BYSTRUM ROAD, SOUTH OF KEYES ROAD. (PLANNED DEVELOPMENT - BRONCO WINE CO.)

WHEREAS, the Stanislaus County Planning Commission had on file a verified petition asking that the zoning of the area as shown on the attached map be changed from A-2-10 (Exclusive Agricultural) zoning to P-D (Planned Development) zoning for a winery, and

WHEREAS, a public hearing was held on March 21, 1974, after giving proper notice and publication, and

WHEREAS, the Land Use Element of the General Plan provides for applications of non-agricultural uses within areas designated for agricultural purposes on a planned development basis provided that it is demonstrated that the proposed uses are validly responsive to the needs of the agricultural area and that approval will not result in detriment to adjacent properties or other continued agricultural usage, and

WHEREAS, the Commission finds that the proposal is consistent with the intent of the Land Use Element of the General Plan and is a valid use of the Planned Development process as a facility that is associated with agricultural production, and

WHEREAS, many such winery facilities are located throughout the valley region in rural areas without apparent conflict with surrounding agricultural uses.

NOW, THEREFORE, BE IT RESOLVED that the Stanislaus County Planning Commission recommends that the proposed zone change to P-D (Planned Development) zoning and the winery project be approved after the one necessary public hearing.

I hereby certify that the above is a full, true, and correct copy of a resolution adopted by the Stanislaus County Planning Commission at a public hearing held on the 21st day of March, 1974.

Robert L. Davis, Jr.
Secretary

March 21 1974

TO: PLANNING COMMISSION
FROM: PLANNING DEPARTMENT
SUBJECT: R 74-2 P-D (PLANNED DEVELOPMENT) BRONCO WINERY

I. APPLICATION

- A. Applicant: Bronco Wine Company by Joseph S. Franzia
B. Owners: Gregory Specialty Co. and V.W. Washam
C. Location of property: On the east side of Bystrum Road, one-quarter mile south of Keyes Road.
D. Area of property: 81.25 acres
E. Existing zoning: A-2-10 (Exclusive Agricultural)
F. Request: Reclassification of property to P-D (Planned Development) and approval of a Development Plan for a winery.
G. Applicant's statement: See E.I.R.

II. ENVIRONMENTAL REVIEW

The Environmental Review Committee determined that this project may have a significant effect on the environment. A draft E.I.R. prepared by the Planning Department is attached with the final E.I.R. to be available for your review prior to the meeting of March 21, 1974.

III. ANALYSIS

The proposed Planned Development project involves the construction of a winery and bottling facility on an 81.25 acre parcel of land located on the east side of Bystrum Road, approximately one-quarter mile south of Keyes Road in an A-2-10 (Exclusive Agricultural) zone. The Tidewater Southern Railroad is located along the west side of the subject property adjacent to Bystrum Road. Crows Landing Road, located one-half mile to the west of the project site is indicated on the Highway Transportation Element of the General Plan as a major street connecting Interstate Highway 5 to the west of Crows Landing with State Highway 99 near the City of Modesto. Keyes Road is designated as a collector street connecting with State Highway 99 near the town of Keyes located approximately four miles to the east of the site.

ATTACHMENT 1

Bystrum Road, providing access to the project site from Keyes Road is a county maintained road of indefinite right of way width but established by usage to a traveled width of approximately 40 feet with a native soil surface. Barnhart Road is deeded to a 40 foot right of way width with a blacktop surface in poor condition structurally and extends east from Crows Landing Road to the southwest corner of the site. Additional access to the property is available by means of a private 20 foot wide access road extending south from Keyes Road to the northeast corner of the property.

The vehicular traffic to be generated in the area from the project as indicated by the applicant, includes a total of twenty-six trucks and thirty-three automobiles on a daily basis during the crushing season and six trucks and twenty-three automobiles daily during the off season. A major portion of this traffic generated would be along Keyes Road between the winery facility and State Highway 99 near the town of Keyes.

The surrounding uses in the vicinity of the project are primarily agricultural including grape vineyards, orchards, pasture land and a large dairy operation immediately to the south of the site. To the southwest along Barnhart Road, are three mobile homes and two residences.

The soil types of the subject property are a mixture of grades one and two prime soils classified as Hanford Sandy Loam, Tujunda Sandy Loam and Dinuba Sandy Loam with a water table depth of approximately six feet as indicated by T.I.D. data obtained in 1970.

The applicant indicates that the project is to include the making of wine and champagne with no distillery to be constructed on the site at any time. The crushing operations will take place during the grape harvest season, generally from late August to early November. During this approximate ten week period, grapes will be crushed five days a week during the daylight hours. After crushing, the grapes are fermented in large stainless steel tanks and grape skins and seeds are pressed and discarded with the pressed grape pumace to be sold for poultry feed. After fermenting, the wine is transferred to storage tanks where it is cooled, filtered, blended and bottled.

Waste water created by the project is proposed to be ponded in shallow ponds, located on the premises as shown on the attached plot plan, consisting of five one-acre ponds approximately six inches deep and graded in a level condition. These ponds will be maintained in a weed free condition with maintenance roads separating the ponds for access. Water is to be pumped into the ponds from a collection sump and the ponds are to be rotated daily with the maximum water depth of a pond at any one time approximately four inches. The percolation rate at the site falls within a range of one inch per hour to six inches per day. This shallow waste water ponding method is presently being used by Tri-Valley Growers on Kiernan Avenue north of Modesto and has proved successful in the elimination of odors.

Septic tank facilities are to be used for sewage disposal on the site and domestic water provided by a well also serving as a source of water for fire protection and sprinkler system.

The Agricultural Extension Service has been requested by the Planning Staff to submit comments in respect to this project as it would effect the groundwater at this location and how the winery would be affected by spraying and dusting operations in the surrounding agricultural areas. Also, the County Assessor has been contacted to comment on any effect this project would have on surrounding agricultural land assessments. The applicant is working with Regional Water Quality Control and the County Health Department for compliance with their requirements and a report from these agencies on the project will be forthcoming. These comments will be delivered to your Commission prior to the meeting of March 21, 1974, when the application is to be considered.

The development plan submitted by the applicant for approval in connection with the zoning change to P-D (Planned Development) consists of the following:

Exhibit A March 6, 1974
Vicinity map, plot plan, building and storage tank elevations, and off-street parking plan.

Exhibit B
Development Schedule

Exhibits are available for review in the Planning Office and will be on display at your meeting.

IV. RECOMMENDATION

The following factors influence our recommendation with respect to this project:

1. The proposed project should not be detrimental to the existing agricultural usage of the surrounding neighborhood if developed in compliance with recommended performance standards. (See Exhibit C).
2. The use is in compliance with the General Plan as a facility that is associated with agricultural production and complies with the provisions of Section 118.5 of the Zoning Ordinance (Planned Development zone).
3. The project is to be located near major or collector streets and a railroad facility that would provide the necessary transportation needs of the facility.
4. Many such winery facilities are located throughout the valley region in rural areas without apparent conflict with surrounding agricultural uses.

We therefore recommend that your Commission take the following actions pertaining to this project:

1. Approve the Final EIR prepared by the Planning Department prior to action on this application.
2. Forward a recommendation to the Board of Supervisors for reclassification of the subject property from A-2-10 (Exclusive Agricultural) to P-D (Planned Development) and approval of a Development Plan consisting of the following:

Exhibit A

Vicinity map, plot plan, elevation and off-street parking plan.

Exhibit B

Development Schedule

Exhibit C

Performance standards for completion of the Planned Development as follows:

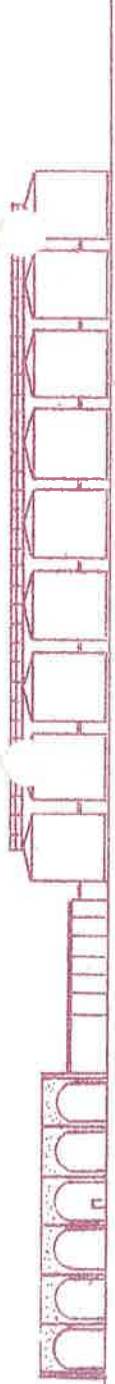
1. No structures or ponding areas to be located closer than 50 feet to property lines and tree screening to be provided along the property lines adjacent to any structure or ponding area located within 75 feet of such property lines.
2. Driveways and parking areas to be blacktopped and on-site drainage provided as approved by the Department of Public Works.
3. The ponding areas to be a maximum of 6 inches in depth with daily rotation of waste water and 10 foot wide roadways around all ponds provided for access to ponds for mosquito personnel. The ponding areas to be kept free of weeds and maintained for mosquito control as required by the Turlock Mosquito Abatement District. Also, night lighting for the project to be installed as approved by the T.M.A.D.
4. Water table levels and ground water quality must be monitored regularly to make certain that no adverse changes occur. If changes do occur, winery operation must be adjusted or additional measures approved by Turlock Irrigation District undertaken to off-set such changes.
5. Crops to be grown during effluent usage when practical and at least each season after effluent applications to make use of the nutrients in the effluent with soil sample to be obtained and analyzed before and after each growing season to determine any chemical or physical change in the soil. The applicant to work with the Agricultural Extension Service for compliance with this condition.
6. The project to be conducted in compliance with State Water Quality Control Board and County Health Department regulations and necessary clearances obtained from these agencies prior to the issuance of building permits.
7. All trucks servicing this development must restrict ingress and egress from Keyes Road along Bystrum Road to the entrance designated on Exhibit A as alternate entrance. In no case shall truck traffic use Barnhart Road or the 20 foot "panhandle" ownership extending from subject property northerly to Keyes Road.
8. The railroad crossing at the entrance to be constructed as approved by the Tidewater Southern Railroad and all applicable government agencies.

9. Pumace to be removed from the project site on a daily basis.
10. That section of Bystrum Road lying between the alternate entrance as shown on Exhibit A and Keyes Road shall be improved by the applicant to provide a 28 foot wide paved section to a design section minimum of 2 inches of asphalt concrete over a 6 inch aggregate base.
11. On-site truck parking and circulation facilities to be provided to the satisfaction of the Public Works Department.
12. Bystrum Road shall not be used for parking or storage by trucks servicing the winery.
13. Plans for the alternate entrance access road intersection to Bystrum Road shall be submitted to and approved by the Public Works Department.
14. A financial guarantee shall be posted by the applicant to pay the County's share of cost of providing additional railroad crossing protection at the Keyes Road Tidewater Southern tracks if such protection is necessary within two years after the winery begins operation.

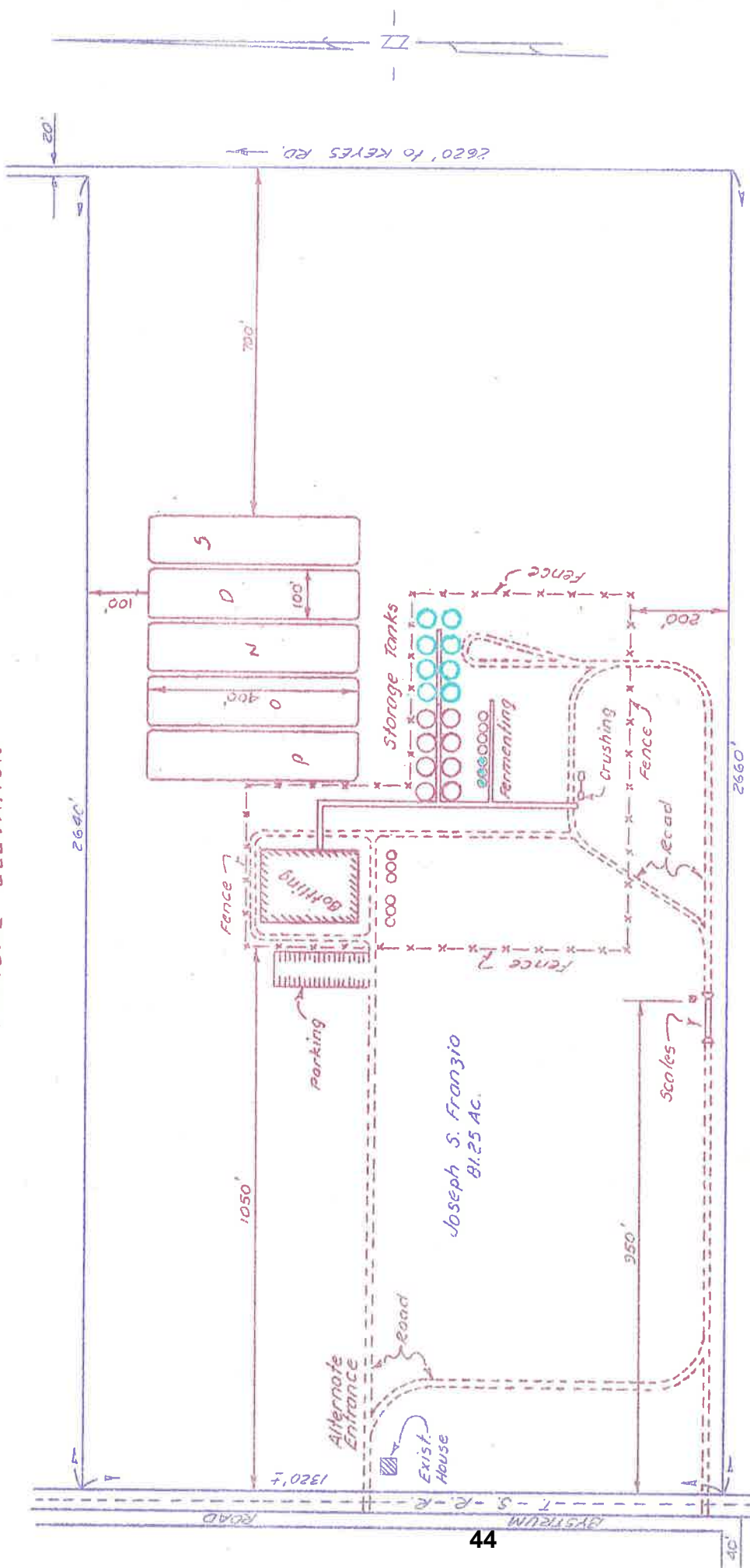
Attachments:

1. Letters from Ag. Extension
2. Letter from Mosquito Abatement
3. Letter from T.I.D.

RMA:dl



TYPICAL ELEVATION



Scale: 1" = 300'

MAP SHOWING AREA PROPOSED TO BE REZONED FROM A-2-1C EXCLUSIVE AGRICULTURE TO P-D-2 PLANNED DEVELOPMENT.

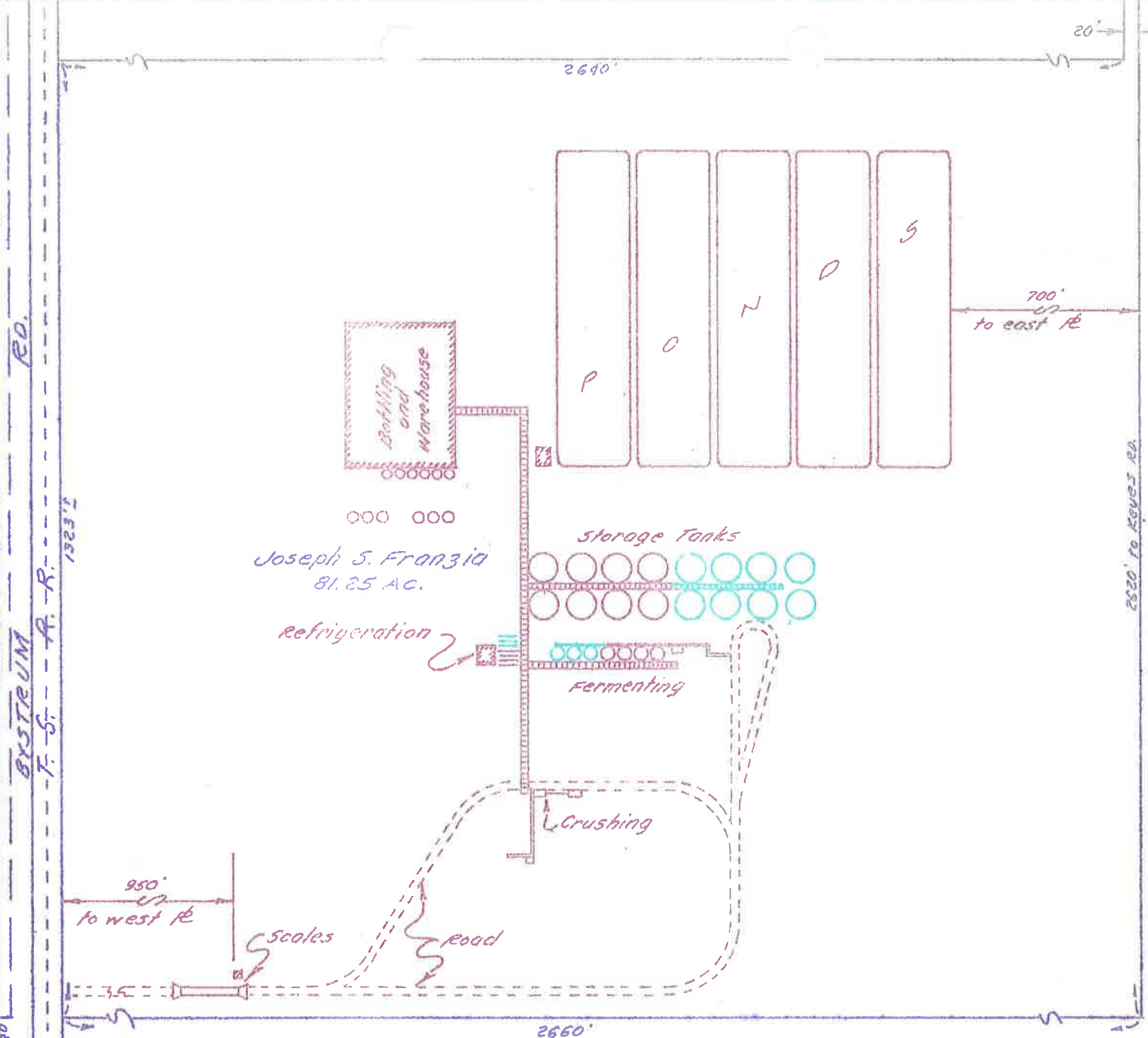
By Joseph S. Franzio to construct a winery and bottling facility. Winery to be known as BEONCO WINE COMPANY

LEGEND:

Red = 1st year development
Green = 3rd year development



VICINITY MAP



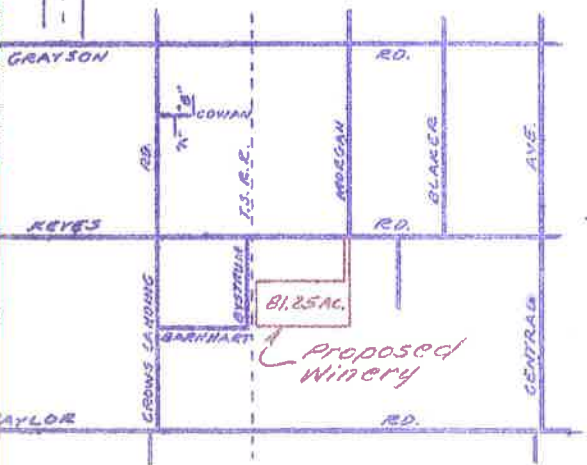
Joseph S. Franzia
81.25 AC.

scale: 1" = 200'

MAP SHOWING AREA PROPOSED TO BE REZONED FROM A-2-10 EXCLUSIVE AGRICULTURE TO P-D-2 PLANNED DEVELOPMENT.

By Joseph S. Franzia to construct a winery and bottling facility. Winery to be known as BRONCO WINE COMPANY.

LEGEND
Red = 1st year development
Green = 3rd year development



VICINITY MAP
scale: 1" = 1 Mile

FINAL ENVIRONMENTAL IMPACT REPORT

PLANNED DEVELOPMENT

ZONING RECLASSIFICATION

BRONCO WINE COMPANY

Stanislaus County Planning Commission

March 21, 1974

EIR 74-2

ATTACHMENT 2

BACKGROUND

On January 21, 1974, the Bronco Wine Company applied for a P-D zone to develop a winery to be located at the northwest corner of Barnhart and Bystrum Roads, four miles west of Keyes.

Pursuant to Stanislaus County regulations, the Environmental Review Committee determined in February, 1974, that an Environmental Impact Report was required. A Draft Environmental Impact Statement was prepared by the Stanislaus County Planning Department in February, 1974. The Draft EIR was sent to concerned public agencies, newspapers, and was made available to any individual who expressed an interest in the project. The review period, during which comments could be made, ended on March 15, 1974.

The Final EIR has been prepared as the last step in the environmental review process. Pursuant to Section 15146 of the State Administrative Code, the Final EIR contains:

1. The Draft EIR
2. The Comments received during the review
3. The response of the County to the significant environmental issues raised by the comments.

The Final EIR must be adopted by the decision-making body before a determination is made on the proposed project itself. The EIR becomes, upon adoption, part of the project report. Its contents must be considered when evaluating the project.

DRAFT ENVIRONMENTAL IMPACT REPORT

REZONING APPLICATION

PLANNED DEVELOPMENT

BRONCO WINERY
814 14th Street
Modesto, California

Prepared by

Stanislaus County Planning Commission

February 15, 1973

BRONCO WINERY PLANNED DEVELOPMENT REZONING

BYSTRUM ROAD-BARNHART ROAD AREA

I. PROJECT DESCRIPTION

A. The project is an 81-acre site located approximately four to five miles south of Modesto and four miles west of the Town of Keyes. The site, consisting of two parcels of about equal size is south of Keyes Road, at the northwest corner of the intersection of Bystrum and Barnhart Roads. The Tidewater Southern Railroad is located on the west boundary of the property.

B. The objective of the Planned Development is to construct a winery. The winery will begin operation on a contract basis, ultimately hoping to market its own brand of wine. The applicant desires to find a location which will allow room for future expansion, as economics permit.

On this site, Bronco plans to make wine and champagne, bottle and ship via truck and rail car. A distillery will not be built on the proposed site at any time in the future. The winery will be constructed near the center of the 81-acre site.

C. 1. Description of Operation

Crushing. Crushing operations take place during the grape harvest which is normally from late August to early November. Bronco Winery will be set up so that it can process all its grape requirements during this 10-week period. Grapes will be crushed five days a week during the daylight hours, all crushing operations should be complete by 9:00 p.m.

Fermenting and Pressing. After crushing the grapes are fermented in large stainless steel tanks. Grape skins and seeds are pressed and discarded. The pressed grape pomace has commercial value as poultry feed, and Bronco plans to sell its pomace for this use. Fermenting and pressing operations take place 16 - 24 hours a day during the crushing season.

Processing. After fermenting, the wine is transferred to storage tanks where it is cooled, filtered, blended and bottled. Flow sheets depicting these operations are attached.

4. Disposal

Stems

Trucked away daily to sanitary landfill.

Pomace

Trucked away daily for commercial use.

Wine Lees

Filtered on dry cake discharge filters and trucked to a sanitary landfill daily.

Still Slops

No distillery at this location, therefore, no still slops.

Wash Water

Collected and pumped to shallow ponds on the site.

Sanitary Sewer

Via septic tank on the site.

5. Water

Water will be pumped from a deep well on the premises. This well will serve as a source of domestic water and also for fire protection and sprinklers.

6. Ponds

Waste water will be ponded in shallow ponds on the premises. Initial plans call for five 1-acre ponds approximately six inches deep and graded level. Water will be pumped into the ponds from a collection sump. Ponds will be rotated daily. Ten foot roadways will be provided around all ponds and vegetation will be controlled.

Estimated daily flows are:

| <u>Source</u> | <u>Crushing Season</u> | <u>Off Season</u> |
|-----------------|------------------------|-------------------|
| Crushing | 15,000 | 0 |
| Pressing | 25,000 | 0 |
| Cooling | 10,000 | 10,000 |
| Filtering | 24,000 | 24,000 |
| Bottling | 10,000 | 10,000 |
| Misc. | 25,000 | 25,000 |
| Total | 109,000 | 69,000 |
| Depth on 1 acre | 4" | 2 1/2" |

III. ENVIRONMENTAL IMPACTS--LONG & SHORT TERM

A. 1. Impacts on Natural Systems

The project as proposed would have several impacts on the physical environment. The ultimate scale of many of the impacts will ultimately depend upon the growth of the winery. Although the present proposal is limited to growth in the next three years, continued growth is very possible and could serve to compound the impacts of the present proposal.

Perhaps the most obvious impact will be the conversion of this agriculturally used property to what is basically a manufacturing operation. However, this conversion of uses will be limited to only about one-quarter to one-third of the property in question, although future expansion could increase this ratio. The remaining acreage will continue in its present state for at least the near future.

A second area of major concern is the ponding operation, which carries with it a number of potential hazards. One of the most obvious of these is odor. In the past, other ponding operations have been criticized because of odors. However, these complaints have resulted largely in areas where deep ponds (up to 14 feet) are used. These ponds, with their great depths of standing water became particularly offensive at certain times of the year. The Bronco Winery proposes to use the newer ponding technique of shallow (6 inches) ponds and daily rotation. As indicated in the project description, the waste water will be at a depth of only about four inches. The percolation rate at the site falls within a range of one inch per hour to six inches per day. Because the waste water will contain some solids which will settle out, the percolation rates will be slowed to some extent. Nevertheless, there are no problems anticipated with standing water, as the maximum depth of a pond at any one time will be four inches. The ponds will be periodically disced to prevent any hardpan formation.

An example of deep ponds versus shallow ponds can be found in the case of the Tri-Valley Growers plant on Kiernan Avenue. This plant used deep ponds for many years, and from time to time received complaints about odors. Two years ago the plant converted to a shallow ponding system with rapid rotation similar to the type of system proposed by Bronco. In two canning seasons since the use of these ponds, no complaints about odor have arisen. Although the wastes at a cannery differ somewhat from those of a winery, the ponding operations are similar enough that comparisons can be made. It should be pointed out that if, for some unknown reason, the ponds do not operate properly, odor could become a problem.

Associated with the increase in traffic in this agricultural area are the conditions of the roads that provide access to the site. Crows Landing Road is presently a heavily used major road, ultimately planned as an expressway. Keyes Road is designated as a collector. It is presently a 2-lane road, seemingly adequate to handle anticipated traffic. The two small county roads leading directly to the site, Barnhart and Bystrum, are poorly maintained and inadequate to handle any significant increase in traffic. Both roads will need improvement, including paving, in order to properly serve the winery, as well as the present users of the roads. There will have to be a crossing constructed over the Tidewater Southern tracks. This will produce a hazard, but one which is no greater than that at any other railroad crossing.

The initial stages of the project do not call for the construction of a railroad spur track, but ultimately a spur will be constructed to serve the property. The major impact of this will be the noise resulting from the loading and switching activities associated with the spur. The magnitude of this impact will depend upon the amount of rail traffic that is actually demonstrated.

The winery will certainly have a visual impact on the surrounding area. Introducing a winery, with its structures and large holding tanks, will produce a substantial impact on the visual character of the site. The effects of this change can be either beneficial or adverse, depending upon the exact style of the structures and upon the taste of the viewers. The Planned Development zoning allows the County to require landscaping and other controls on the visual character of the site.

The winery may have some effect upon land values of the surrounding land, although this should be rather minor. The agricultural potential of the land should not be diminished by this Planned Development. The operation could decrease the value of the adjacent land for residential use, but the agricultural zoning prohibits residential uses except on large parcels or in existing residences. Expansion of the operation could lead to adjacent landowners selling their property to Bronco. Others may wish to leave the area due to the changes in its character which the winery will introduce.

The conflict in land use between the wine processing and the surrounding agricultural activities could be a problem. Orchard spraying could be a hazard to both the production of the wine and to the persons working at the

to prevent nighttime insects can be required. Lighting can also be placed in such a way that it does not disturb neighboring residences.

The plant itself will be completely fenced to provide safety to both the plant and the surrounding area. Proper signals at the railroad crossing can decrease the hazard that will exist there.

Hazards to the area groundwater can be minimized by placing septic tanks at locations to which they are best suited. The County Health Department can aide in this respect. The fact that the winery will be pumping large amounts of groundwater will help lower the water table in the area, which will aid in preventing any problems in this respect

D. Alternatives to the Proposed Action

Alternative No. 1 "No Project." The "No Project" alternative would leave the site and surrounding area in their present condition. The land would very likely continue in agricultural use for the foreseeable future. The applicants would have to find and purchase an existing winery where there would be no new impacts.

Alternative No. 2 "Different Location." This is a realistic alternative to the proposed site. The applicants could search for a site, either in this County or another, at which there would be fewer impacts. Since the operation requires both street and rail traffic, the number of such sites would be limited. If such a site were located in an agricultural area, the impacts would be basically the same as at the present site. It may be desirable to find a site to which better road access is available, as the present site is somewhat distant from easy access to major routes.

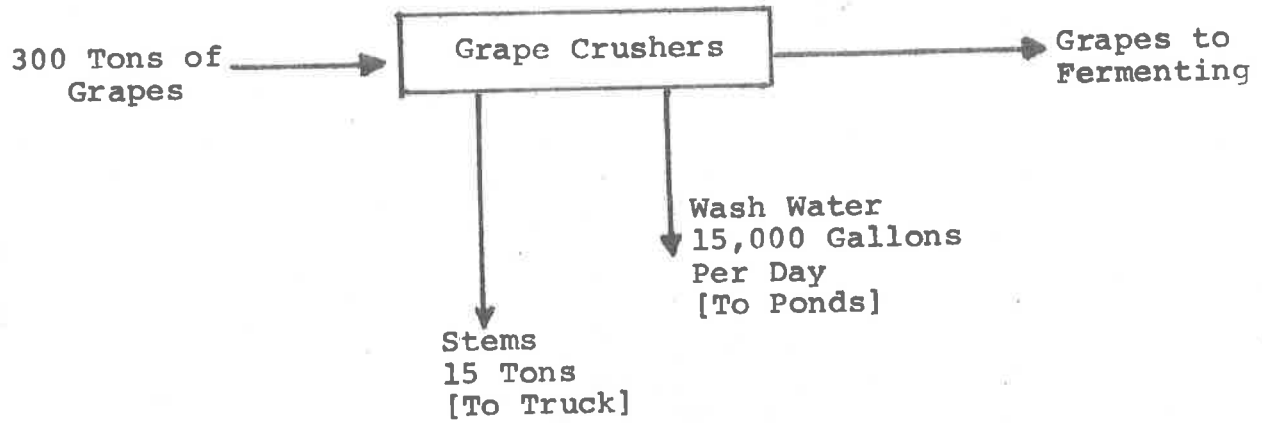
Location in an area presently zoned for industrial use is possible. This could eliminate many of the impacts at this location that are associated with the change in land use. Impacts such as increased noise levels in the agricultural area could be minimized. Sewer service is also available in some areas, such as the Beard Industrial Tract of Modesto, and could eliminate any septic tank problems. The costs of locating in an industrial area may make this alternative undesirable to the applicants.

E. Short-Term Vs. Long-Term Impacts and Irreversible Commitments

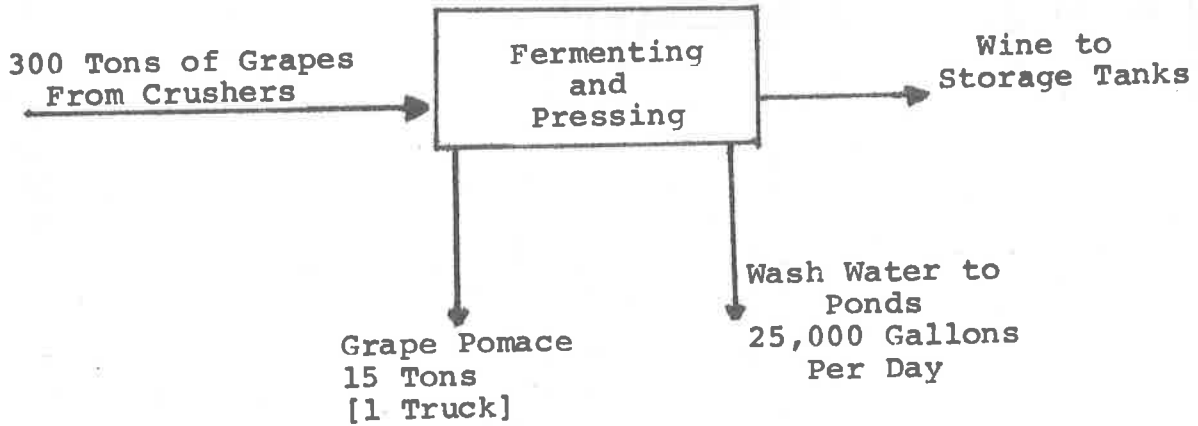
Basically, the present proposal would commit the prime agricultural soil found at the site to an industrial use. Long-term risks to health and safety would be related to proper functioning of the ponding operation. The ponding

BRONCO WINE COMPANY

FLOW CHART
CRUSHING
DAILY OPERATION



FLOW CHART
FERMENTING
AND
PRESSING
DAILY AVERAGE



The Draft EIR was circulated to the following agencies and organizations with regard to its comments:

- . State Department of Fish and Game (Local and Regional Offices)
- . Turlock Irrigation District
- . Keyes Fire Protection District
- . County Agricultural Extension Service
- . Modesto Bee
- . State Reclamation Board
- . County Health Department
- . State Regional Water Quality Control Board
- . County Public Works Department
- . Yokut Wilderness Group
- . Turlock Mosquito Abatement District
- . State Alcoholic Beverage Control
- . Federal Bureau of Alcohol, Tobacco, Firearms and Taxes
- . Tidewater Southern Railroad
- . Turlock Daily Journal

Comments were received during the 30-day review period from the following:

- . Turlock Irrigation District
- . County Agricultural Extension Service
- . County Health Department
- . State Regional Water Quality Control Board
- . County Public Works Department
- . Turlock Mosquito Abatement District
- . State Department of Fish and Game
- . State Reclamation Board
- . Price, Martin and Crabtree (Attorneys)
- . Sierra Club (Yokut Wilderness Group)

BRONCO WINE CO. PLANNED DEVELOPMENT
P-D (6)
"DEVELOPMENT PLAN"
EXHIBIT C (PERFORMANCE STANDARDS)

Performance standards for completion of Planned Development P-D (6) are as follows:

1. No structures or ponding areas to be located closer than 50 feet to property lines and tree screening to be provided along the property lines adjacent to any structure or ponding area located within 75 feet of such property lines.
2. Driveways and parking areas to be blacktopped and onsite drainage provided as approved by the Department of Public Works.
3. The ponding areas to be a maximum of 6 inches in depth with daily rotation of waste water and 10 foot wide roadways around all ponds provided for access to ponds for mosquito personnel. The ponding areas to be kept free of weeds and maintained for mosquito control as required by the Turlock Mosquito Abatement District. Also, night lighting for the project to be installed as approved by the T.M.A.D.
4. Water table levels and ground water quality must be monitored regularly to make certain that no adverse changes occur. If changes do occur, winery operation must be adjusted or additional measures undertaken to off-set such changes.
5. Crops to be grown during effluent usage when practical and and at least each season after effluent applications to make use of the nutrients in the effluent with soil sample to be obtained and analysed before and after each growing season to determine any chemical or physical change in the soil. The applicant to work with the Agricultural Extension Service for compliance with this condition.
6. The project to be conducted in compliance with the State Water Quality Control Board and County Health Department regulations.
7. All trucks necessary for the farming operation and servicing of the home will use the "Panhandle Road to Keyes Road. All other trucks must restrict ingress and egress off Bystrum Road via Keyes Road to the entrance designated on Exhibit A as "Alternate Access".

8. The railroad crossing at the entrance to be constructed as approved by the Tidewater Southern Railroad and all applicable government agencies.
9. Pumace and stems to be removed from the project site on a daily basis.
10. That section of Bystrum Road lying between Barnhart Road and Keyes Road shall be improved in conjunction with the County. Bronco Wine will bring the road to grade with required fill, if necessary, and apply road base to a thickness of six inches. The County will do the necessary grading and applying of two inches of asphalt concrete which would be normal under standards for a country road. The fill and base would be built to specifications approved by the Public Works Department.
11. On-site truck parking and circulation facilities to be provided to the satisfaction of the Public Works Department.
12. Bystrum Road shall not be used for parking or storage by trucks servicing the winery.
13. Plans for the alternate entrance access road intersection to Bystrum Road shall be submitted to and approved by the Public Works Department.
14. The ponding operation shall not create a public nuisance as defined by the Code of Civil Procedures.

THE BOARD OF SUPERVISORS OF THE COUNTY OF STANISLAUS
ACTION AGENDA SUMMARY

DEPT: Planning and Community Development N

BOARD AGENDA # 6:45 p.m.

Urgent

Routine

AGENDA DATE April 20, 2010

CEO Concurs with Recommendation YES NO
(Information Attached)

4/5 Vote Required YES NO

SUBJECT:

Public Hearing to Consider Planning Commission's Recommendation for Approval of Rezone Application No. 2009-04, Bronco Wine Co., a Request to Rezone a Parcel from A-2-40 (General Agriculture) to PD (Planned Development) on Property Located at 800 E. Keyes Road, at the Southeast Corner of E. Keyes and Bystrum Roads, in the Ceres Area

PLANNING COMMISSION RECOMMENDATIONS:

After conducting a duly advertised public hearing at its regular meeting of March 18, 2010, the Planning Commission, on a 5-0 vote, recommended the Board approve the project as follows:

1. Adopt the Mitigated Negative Declaration pursuant to CEQA Guidelines Section 15074(b), by finding that on the basis of the whole record, including the Initial Study and any comments received, that there is no substantial evidence the project will have a significant effect on the environment and that the Negative Declaration reflects Stanislaus County's independent judgement and analysis.

(Continued on page 2)

FISCAL IMPACT:

There are no fiscal impacts associated with this project. In accordance with the adopted Department of Planning and Community Development Fee Schedule, this project is subject to payment of the 'actual cost' for process. All costs associated with this project have been paid and approval of this project will have no impact on the County's General Fund.

BOARD ACTION AS FOLLOWS:

No. 2010-218

On motion of Supervisor Chiesa, Seconded by Supervisor O'Brien

and approved by the following vote,

Ayes: Supervisors: O'Brien, Chiesa, Monteith, DeMartini, and Chairman Grover

Noes: Supervisors: None

Excused or Absent: Supervisors: None

Abstaining: Supervisor: None

1) X Approved as recommended

2) _____ Denied

3) _____ Approved as amended

4) _____ Other:

MOTION: INTRODUCED, ADOPTED, AND WAIVED THE READING OF ORDINANCE C.S. 1084 FOR REZONE APPLICATION #2009-04.

ATTEST: Christine Ferraro
CHRISTINE FERRARO TALLMAN, Clerk

Public Hearing to Consider Planning Commission's Recommendation for Approval of Rezone Application No. 2009-04, Bronco Wine Co., a Request to Rezone a Parcel from A-2-40 (General Agriculture) to PD (Planned Development) on Property Located at 800 E. Keyes Road, at the Southeast Corner of E. Keyes and Bystrum Roads, in the Ceres Area

Page 2

PLANNING COMMISSION RECOMMENDATION: (Continued)

2. Order the filing of a Notice of Determination with the Stanislaus County Clerk-Recorder's Office pursuant to Public Resources Code Section 21152 and CEQA Guidelines Section 15075.
3. Find that the project is consistent with the overall goals and policies of the County General Plan.
4. Find that the proposed PD zoning is consistent with the Planned Development General Plan designation.
5. The alternative to the Agricultural Buffer Standards applied to this project provides equal or greater protection than the existing buffer standards.
6. The project will increase activities in and around the project area, and increase demands for roads and services, thereby requiring dedication and improvements.
7. Approve Rezone Application No. 2009-04 - Bronco Wine Co., subject to the attached Development Standards and Development Schedule.

DISCUSSION:

This is a request to rezone a 35.78-acre parcel from A-2-40 (General Agriculture) to P-D (Planned Development) for expansion of the adjoining Bronco Wine facility by conversion of an existing house into an office, construction of two (2) new 14,400 square foot office buildings, an associated parking lot and two (2) driveways on E. Keyes Road.

The project site is located at 800 E. Keyes Road, south of Ceres, and is improved with a single-family dwelling and a vineyard. The surrounding area consists of agricultural uses, primarily orchards and vineyards. Bronco Wine Co. is south of and adjacent to the project site. There are scattered single-family dwellings in the area, with the closest off-site dwelling being approximately 60 feet from the project site's eastern property line.

The applicants are proposing to begin construction on the driveways and parking lots by fall of 2010. No development schedule was provided for the 14,000 square foot offices as the applicants are not proposing to construct those buildings at this time. They are requesting to "reserve" the footprints of the office buildings to allow the administrative portion of the business to be relocated as necessary to accommodate the expansion of the adjacent wine processing facility located at 6342 Bystrum Road.

Public Hearing to Consider Planning Commission's Recommendation for Approval of Rezone Application No. 2009-04, Bronco Wine Co., a Request to Rezone a Parcel from A-2-40 (General Agriculture) to PD (Planned Development) on Property Located at 800 E. Keyes Road, at the Southeast Corner of E. Keyes and Bystrum Roads, in the Ceres Area

Page 3

Days and hours of operation from December to June are Monday thru Friday, 24 hours a day, and from July to November, seven (7) days a week, 24 hours a day. Currently, there are 180 employees on a maximum shift, 10 customers/visitors on site at peak times, 60 truck deliveries/loadings per day off-season, and 300 truck deliveries/loadings per day during peak season. Peak seasonal operation hours are from mid-July to mid-November. The project will be served by a private well for water and on-site septic facilities will provide for sewage disposal.

On March 18, 2010, the Planning Commission considered this application at a properly advertised public hearing. No one spoke in support of the project. Alice Roche spoke in opposition to the project citing conflict between tractors crossing E. Keyes Road and additional truck and employee traffic resulting from the proposed project. The tractors are used on properties, in the immediate vicinity, which are farmed as a part of the Bronco operation.

Following the closing of the hearing, the Commission unanimously voted 5-0 (Ramos/Assali) to forward the project to the Board of Supervisors for approval. A detailed discussion of the request and staff's recommendation of approval can be found in the attached Planning Commission Staff Report.

POLICY ISSUES:

The Board should determine if approval of the proposed rezone furthers the goals of efficient delivery of government services and a well-planned infrastructure system.

STAFFING IMPACT:

There are no staffing impacts associated with this item.

CONTACT PERSON:

Kirk Ford, Planning and Community Development Director. Telephone: (209) 525-6330

ATTACHMENTS:

1. Planning Commission Staff Report, March 18, 2010
2. Planning Commission Minutes, March 18, 2010

I:\Staff\p\REZ\2009\REZ 2009-04 - Bronco Wine Co\BOS\BOS Report.wpd

STANISLAUS COUNTY PLANNING COMMISSION

March 18, 2010

STAFF REPORT

REZONE APPLICATION NO. 2009-04
BRONCO WINE CO.

REQUEST: TO REZONE A 35.78-ACRE PARCEL FROM A-2-40 (GENERAL AGRICULTURE) TO PLANNED DEVELOPMENT FOR EXPANSION OF THE ADJOINING BRONCO WINE FACILITY BY CONVERSION OF AN EXISTING HOUSE INTO AN OFFICE, CONSTRUCTION OF TWO NEW 14,400 SQUARE FOOT OFFICE BUILDINGS, AN ASSOCIATED PARKING LOT AND TWO DRIVEWAYS ON E. KEYES ROAD. THE PROJECT SITE IS LOCATED AT 800 E. KEYES ROAD, SOUTH OF CERES.

APPLICATION INFORMATION

| | |
|-----------------------------|---|
| Applicant: | Bronco Wine Co. |
| Engineer: | R.B. Welty & Associates |
| Location: | 800 E. Keyes Road, at the southeast corner of E. Keyes and Bystrum Roads, in the Ceres area. |
| Section, Township, Range: | 33-4-9 |
| Supervisory District: | Two (Supervisor Chiesa) |
| Assessor's Parcel: | 041-046-019 |
| Referrals: | See Exhibit "I" Environmental Review Referrals |
| Area of Parcels: | 36.62 acres |
| Water Supply: | Private well |
| Sewage Disposal: | Septic |
| Existing Zoning: | A-2-40 (General Agriculture) |
| General Plan Designation: | Agriculture |
| Community Plan Designation: | Not applicable |
| Williamson Act: | Not applicable |
| Environmental Review: | Mitigated Negative Declaration |
| Present Land Use: | Single-family dwelling and a vineyard |
| Surrounding Land Use: | Scattered single-family dwellings, vineyards, and orchards to the north, east, and west, Bronco Wine Co., dairies, and scattered single-family dwellings to the south |

PROJECT DESCRIPTION

This is a request to rezone a 35.78-acre parcel from A-2-40 (General Agriculture) to P-D (Planned Development). The project site is north of and adjacent to the existing Bronco Wine Co. site, located at 6342 Bystrum Road. The request includes adding two (2) driveways onto E. Keyes Road, the conversion of an existing house to a shipping and receiving office, and the construction

of two (2) truck scales, a guard shack, employee and truck parking lots, a 14,400 square foot administration building, and a 14,400 square foot sales building. The proposed driveways and employee and truck parking lots will serve both the proposed and existing Bronco Wine facility. The parking lot, access roads, and driveways will be paved.

On the existing site, days and hours of operation from December to June are Monday thru Friday, 24 hours a day, and from July to November, seven (7) days a week, 24 hours a day. Currently there are 180 employees on a maximum shift, 10 customers/visitors on site at peak times, 60 truck deliveries/loadings per day off-season, and 300 truck deliveries/loadings per day during peak season. Peak seasonal operation hours are from mid-July to mid-November. The project will be served by a private well for water and on-site septic facilities will provide for sewage disposal.

SITE DESCRIPTION

The project site is located at 800 E. Keyes Road and is improved with a single-family dwelling and a vineyard. The surrounding area consists of agricultural uses, primarily orchards and vineyards. Bronco Wine Co. is south of and adjacent to the project site. There are scattered single-family dwellings in the area, with the closest off-site dwelling being approximately 60 feet from the project site's eastern property line.

DISCUSSION

According to County records, the current Bronco Wine Co. facility, located at 6342 Bystrum Road, was approved to operate as a winery and bottling facility since the 82-acre property was rezoned in 1974. The Board of Supervisors approved the rezone from A-2-10 (General Agriculture) to P-D (6) (Planned Development) based on the following factors:

1. The proposed project should not be detrimental to the existing agricultural usage of the surrounding neighborhood if developed in compliance with the recommended performance standards; and
2. The use is in compliance with the General Plan as a facility that is associated with agricultural production and complies with the provisions of Section 118.5 of the Zoning Ordinance (Planned Development zone); and
3. The project is to be located near major or collector streets and a railroad facility that would provide the necessary transportation needs of the facility; and
4. Many such winery facilities are located throughout the valley region in rural areas without apparent conflict with surrounding agricultural uses.

Since its approval in 1974, Bronco Wine Co. has produced wine and sparkling wine and has a license to produce malt beverages. Grapes are trucked to the site and crushing operations take place during the grape harvest season, generally from July to November. After crushing, the grapes are fermented in large stainless steel tanks and grape skins and seeds are pressed and discarded with the pressed grape pomace to be sold for feed. After fermenting, the wine is transferred to storage tanks where it is cooled, filtered, blended and bottled.

The project site includes an existing single-family dwelling which will be converted to a shipping and receiving office. The proposed improvements to the site will include two (2) truck scales, a guard shack, the construction of employee and truck parking lots and two (2) 14,400 square foot office buildings, new septic tanks, and landscaping. The proposed office buildings and parking lots will be located in the southern half of the property. Construction of the driveways and parking lots off

of E. Keyes Road will move vehicular traffic away from the railroad crossing currently utilized off of Bystrum Road. The applicants are proposing to begin construction on the driveways and parking lots by fall of 2010. No development schedule was provided for the offices as the applicants are not proposing to construct those buildings at this time. They are requesting to "reserve" the footprints of the office buildings to allow the administrative portion of the business to be relocated as necessary to accommodate the expansion of the wine processing facility.

In order to approve a rezone, it must be found to be consistent with the General Plan. In this case, the General Plan designation is "Agriculture." The "Agriculture" General Plan designation is consistent with a Planned Development zoning designation when, *"it is used for agriculturally-related uses or for uses of a demonstrably unique character, which due to specific agricultural needs or to their transportation needs or to needs that can only be satisfied in the agriculture designation, may be properly located within areas designated as "agricultural" on the General Plan. Such uses can include, facilities for packing fresh fruit, facilities for the processing of agricultural commodities utilized in the County's agriculture community, etc."* Staff believes that the proposed Planned Development is logical considering the unique characteristics of this site, such as the close proximity to the existing Bronco Wine Co. site. The proposed use should not be detrimental to agricultural uses and other property in the area which consists mainly of orchards, vineyards, dairies, and the existing Bronco Wine Co. Staff finds the proposal to rezone this parcel to Planned Development to be consistent with the General Plan.

The existing County parking standards require manufacturing or assembly plants and wholesale warehouses provide one (1) parking space for each employee on a maximum shift plus three (3) additional spaces. Office buildings are required to provide one (1) space for every 300 square feet of office space. The site plan identifies 345 employee parking spaces and 32 truck parking spaces. The proposed office buildings, at build out, would require a total of 94 parking spaces (see Exhibit "A" - Maps). If needed, additional parking spaces could be provided since the project site does have area that will remain in grape production.

The site plan for the proposed expansion indicates that a two foot by three foot directional sign will be located at the entrance to the truck and employee driveways (see Exhibit "A" - Maps). All final sign approvals rest with the Director of Planning and Community Development and will require the Planning Director's (or designee's) approval prior to the placement of such signs (see Exhibit "B" - Development Standards).

In December of 2007, Stanislaus County adopted an updated Agricultural Element which incorporated guidelines for the implementation of agricultural buffers applicable to new and expanding non-agricultural uses within or adjacent to the A-2 zoning district. The purpose of these guidelines is to protect the long-term health of agriculture by minimizing conflicts resulting from the interaction of agricultural and non-agricultural uses. Current buffer guidelines require a project that is expanding a non-agricultural use to provide a minimum building setback of 150-feet, fencing, and vegetative screening; the same is required for new non-agricultural uses.

Appendix "A" - Buffer and Setback Guidelines of the Agricultural Element allows for alternative buffers to be proposed, provided the Stanislaus County Planning Commission makes a finding that the buffer alternative is found to provide equal or greater protection to surrounding agricultural uses. Alternatives proposed by a project applicant shall be reviewed and supported by the Stanislaus County Agricultural Advisory Board prior to consideration by the Planning Commission.

On September 8, 2008 and November 2, 2009, planning staff asked the Agricultural Advisory Board to support a series of 'generic', non-project specific buffer alternatives applicable to uses such as nut hulling, shelling, dehydrating, grain warehousing, and agricultural processing facilities (without incidental tasting rooms or sales). The Agricultural Advisory Board supported these alternatives.

The supported alternatives applicable to this project include:

- Providing an overall distance of 150 feet or greater exists between the proposed use and the property line, no vegetative screening shall be required.
- When trespassing onto neighboring property is determined not to be an issue, the fencing requirement may be waived.

Based on Appendix "A" - Buffer and Setback Guidelines of the Agricultural Element, "property line" refers to the property line of any adjoining parcels for this supported alternative. The project will exceed the required 150-foot distance between the use and adjoining agricultural uses in each direction; therefore, a vegetative screen will not be required. Because the proposed use and the product produced is agricultural in nature, the applicants intend to keep as much of the property as possible planted in vineyards. An agricultural buffer two (2) feet wide and planted in evergreen trees, six (6) feet high, and five (5) feet apart will be planted along the east and north property lines. The western property line is planted in cypress trees; trespassing will not be an issue and the fencing requirement may be waived. Additional landscaping will be installed around the perimeter of the employee parking lot (see Exhibit "A" - *Maps*). Landscaping and buffer installation will be reviewed as a part of the building and/or grading permit.

Staff has received two (2) phone calls, an email, and a letter from neighboring property owners who were concerned about dust, negative impacts to air quality, the increase in traffic, traffic safety, and the impact to existing driveways (see Exhibit "H" - *Surrounding Landowner's Responses*). According to the Stanislaus County 2008 aeriels, the driveways of the parcels directly north and east of the project site are located across and adjacent to the proposed Bronco driveways. The applicants are proposing to pave the proposed access roads and parking lots which will reduce dust. A referral response was received from the San Joaquin Valley Air Pollution Control District which stated that project specific emissions were not expected to exceed District significance thresholds. Therefore, the District concluded that project specific pollutant emissions would have no significant adverse impact on air quality. The project will be subject to the Indirect Source Review as reflected in the Development Standards. Finally, the Department of Public Works reviewed the proposed project and responded with conditions of approval and mitigation measures to address and mitigate impacts on traffic, driveways, and safety (see Exhibit "B" - *Development Standards*). Existing and proposed driveways and the feasibility of their locations will be reviewed as a part of the encroachment permit process.

ENVIRONMENTAL REVIEW

Pursuant to the California Environmental Quality Act (CEQA), the proposed project was circulated to all interested parties and responsible agencies for review and comment (see Exhibit "I" - *Environmental Review Referrals*). Based on the Initial Study prepared for this project, adoption of a Mitigated Negative Declaration is being recommended (see Exhibits "E" - *Initial Study* and "F" -

Mitigated Negative Declaration). The mitigation measures included in the project address light and traffic related standards; these measures include light shielding and traffic operations and improvements. Responses received from agencies and mitigation measures have been incorporated into this project as Development Standards (see Exhibit "B" - *Development Standards*).

RECOMMENDATION

Based on all evidence on the record, and on the ongoing discussion, staff recommends that the Planning Commission recommend that the Board of Supervisors take the following actions regarding this project:

1. Adopt the Mitigated Negative Declaration pursuant to CEQA Guidelines Section 15074(b), by finding that on the basis of the whole record, including the Initial Study and any comments received, that there is no substantial evidence the project will have a significant effect on the environment and that the Mitigated Negative Declaration reflects Stanislaus County's independent judgement and analysis.
2. Order the filing of a Notice of Determination with the Stanislaus County Clerk-Recorder's Office pursuant to Public Resources Code Section 21152 and CEQA Guidelines Section 15075.
3. Find that:
 - A. The project is consistent with the overall goals and policies of the County General Plan;
 - B. The proposed Planned Development zoning is consistent with the Agriculture General Plan designation;
 - C. The alternative to the Agricultural Buffer Standards applied to this project provides equal or greater protection than the existing buffer standards; and
 - D. The project will increase activities in and around the project area, and increase demands for roads and services, thereby requiring dedication and improvements.
4. Approve Rezone Application No. 2009-04 - Bronco Wine Co., subject to the attached Development Standards and Development Schedule.

Note: Pursuant to California Fish and Game Code Section 711.4, all project applicants subject to the California Environmental Quality Act (CEQA) shall pay a filing fee for each project. Therefore, the applicant will further be required to pay \$2,067.25 for the Department of Fish and Game, and the Clerk Recorder filing fees. The attached Development Standards ensure that this will occur.

Report written by: Rachel Wyse, Assistant Planner, March 1, 2010
Report reviewed by: Bill Carlson, Senior Planner

REZ 2009-04
Staff Report
March 18, 2010
Page 6

Attachments:

Exhibit A - Maps
Exhibit B - Development Standards
Exhibit C - Development Schedule
Exhibit D - Application Information
Exhibit E - Initial Study
Exhibit F - Mitigated Negative Declaration
Exhibit G - Mitigation Monitoring Plan
Exhibit H - Surrounding Landowner's Responses
Exhibit I - Environmental Review Referrals

(I:\Staffrpt\REZ\2009\REZ 2009-04 - Bronco Wine Co\Staff Report\Staff Report.wpd)

**REZ 2009-04 – BRONCO WINE CO
AREA MAP**

CERES

W GRAYSON RD

TID LATERAL NO 2 1/2

W KEYES RD

WESTPORT DRAIN

HATCH DRAIN

CROWS LANDING RD

BYSTRUM RD

E TAYLOR RD

Union Pacific

MORGAN RD

BLAKER RD

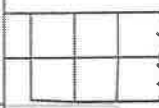
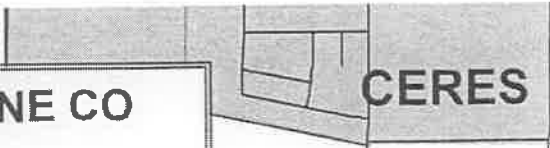
CENTRAL AVE

CERES MAIN

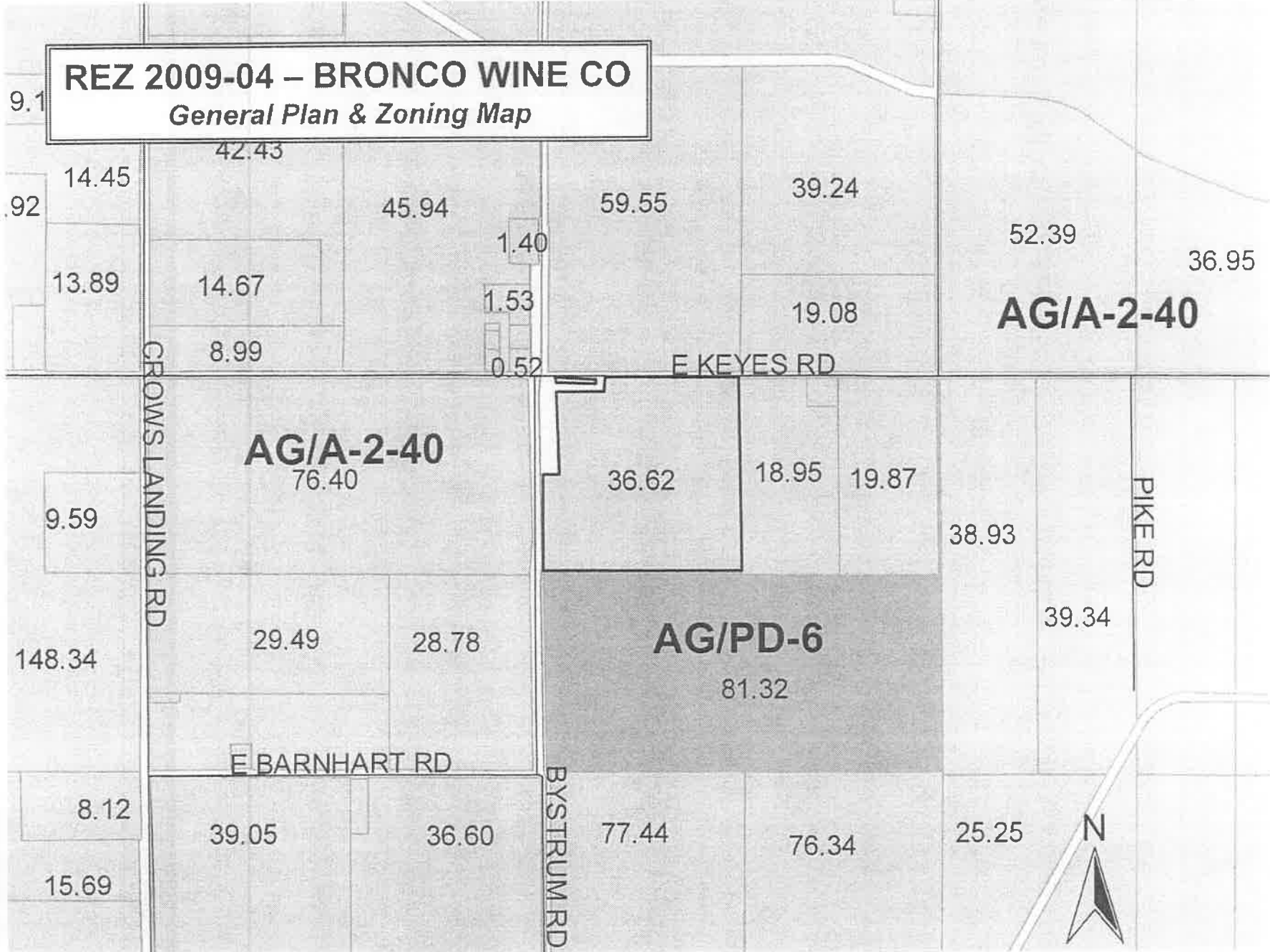


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EXHIBIT A



REZ 2009-04 – BRONCO WINE CO
General Plan & Zoning Map



68

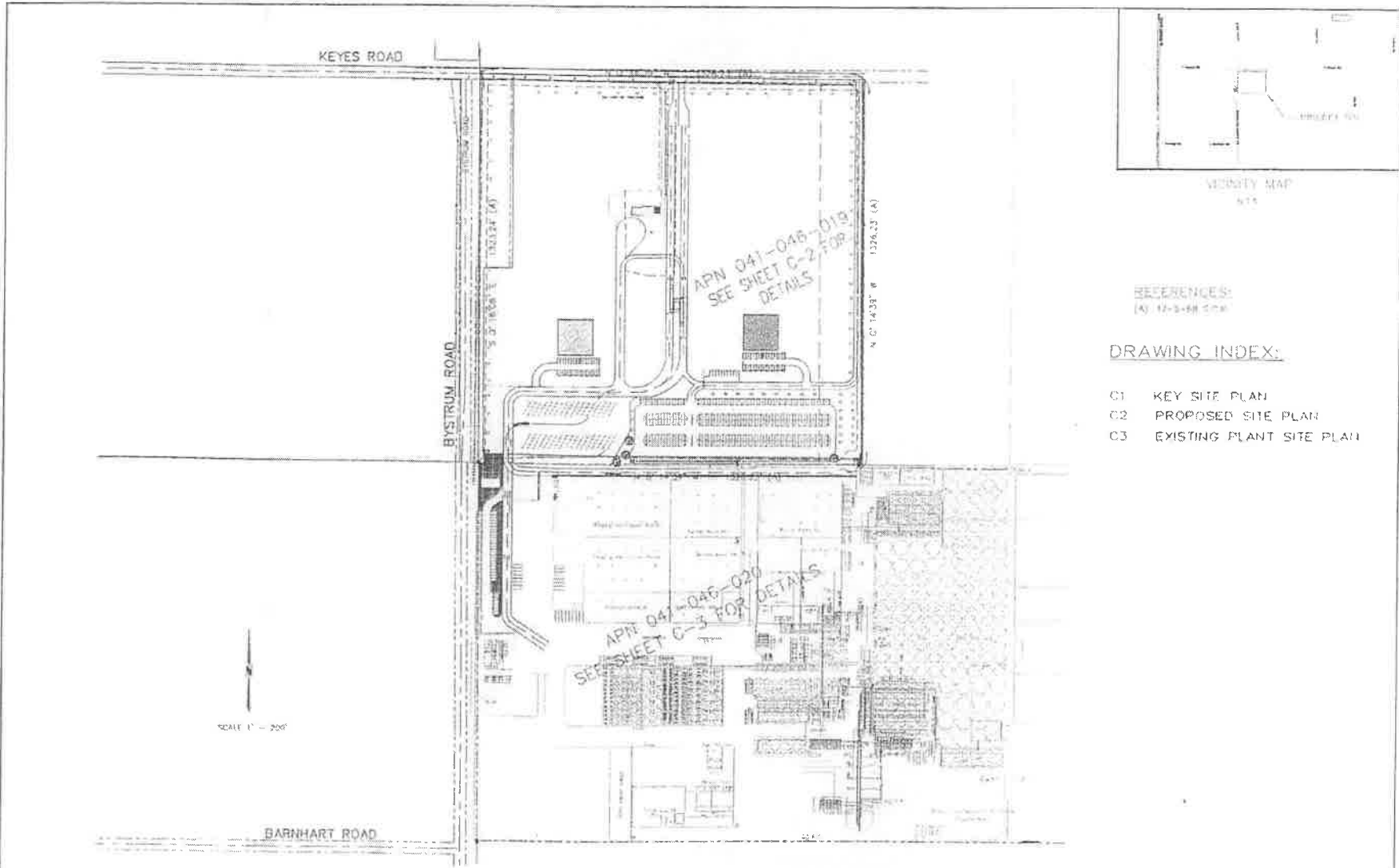
EXHIBIT A-1

REZ 2009-04 – BRONCO WINE CO
2008 STANISLAUS COUNTY AERIAL

E KEYES RD

BYSTRUM RD





VICINITY MAP
SITE

REFERENCES:
(A) 12-9-98 CPA

DRAWING INDEX:

- C1 KEY SITE PLAN
- C2 PROPOSED SITE PLAN
- C3 EXISTING PLANT SITE PLAN

All other parties and interests are notified in this case and the site owner is to be held responsible for any errors and omissions. This drawing is not to be used for construction and it is not to be used for any other purpose. The drawing is not to be used for construction and it is not to be used for any other purpose.

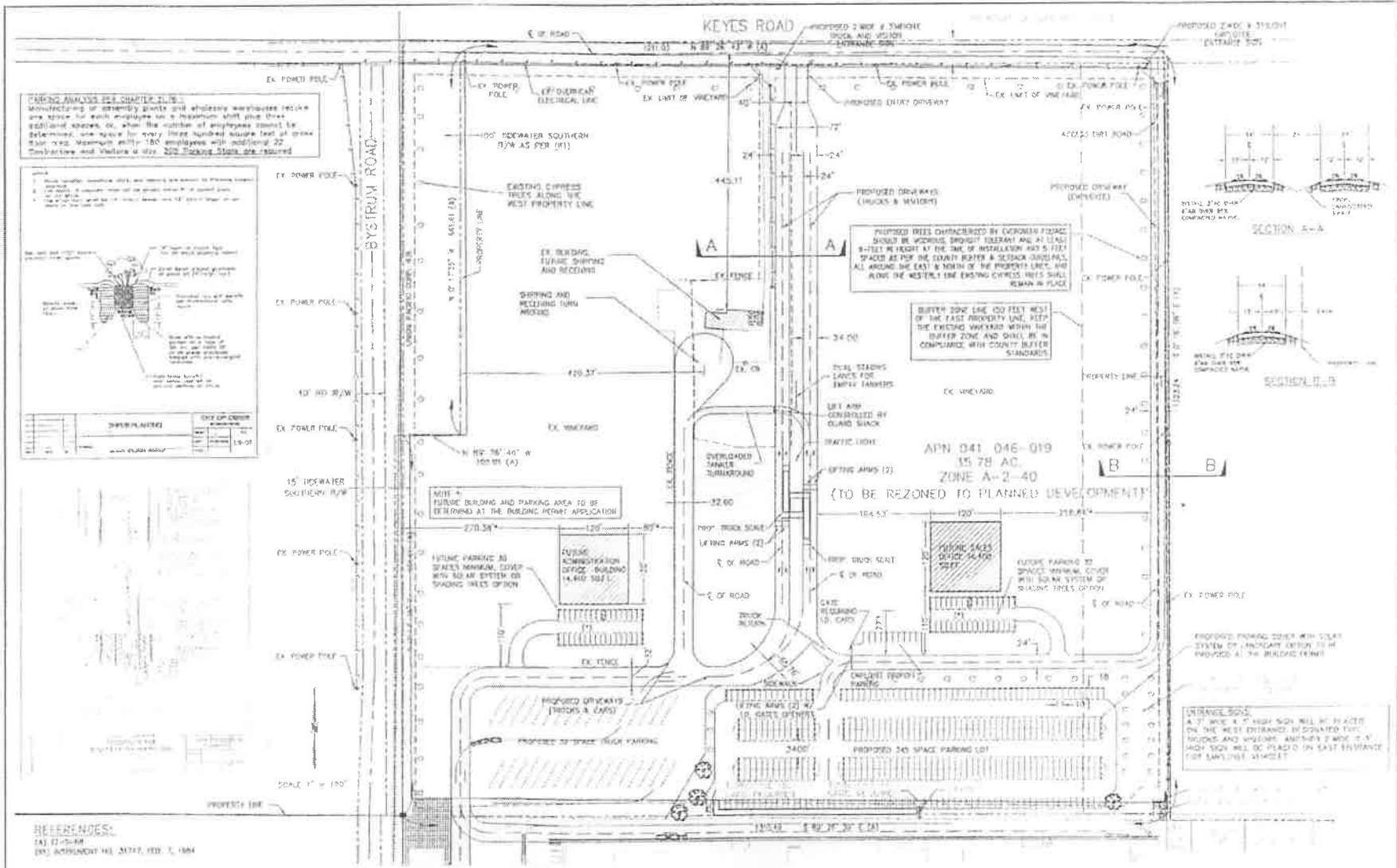
R. B.
P.O. BOX
521 316

REZ 2009-04 – BRONCO WINE CO
Existing & Proposed Sites

DATE
BY
APPROVED
6-15-15

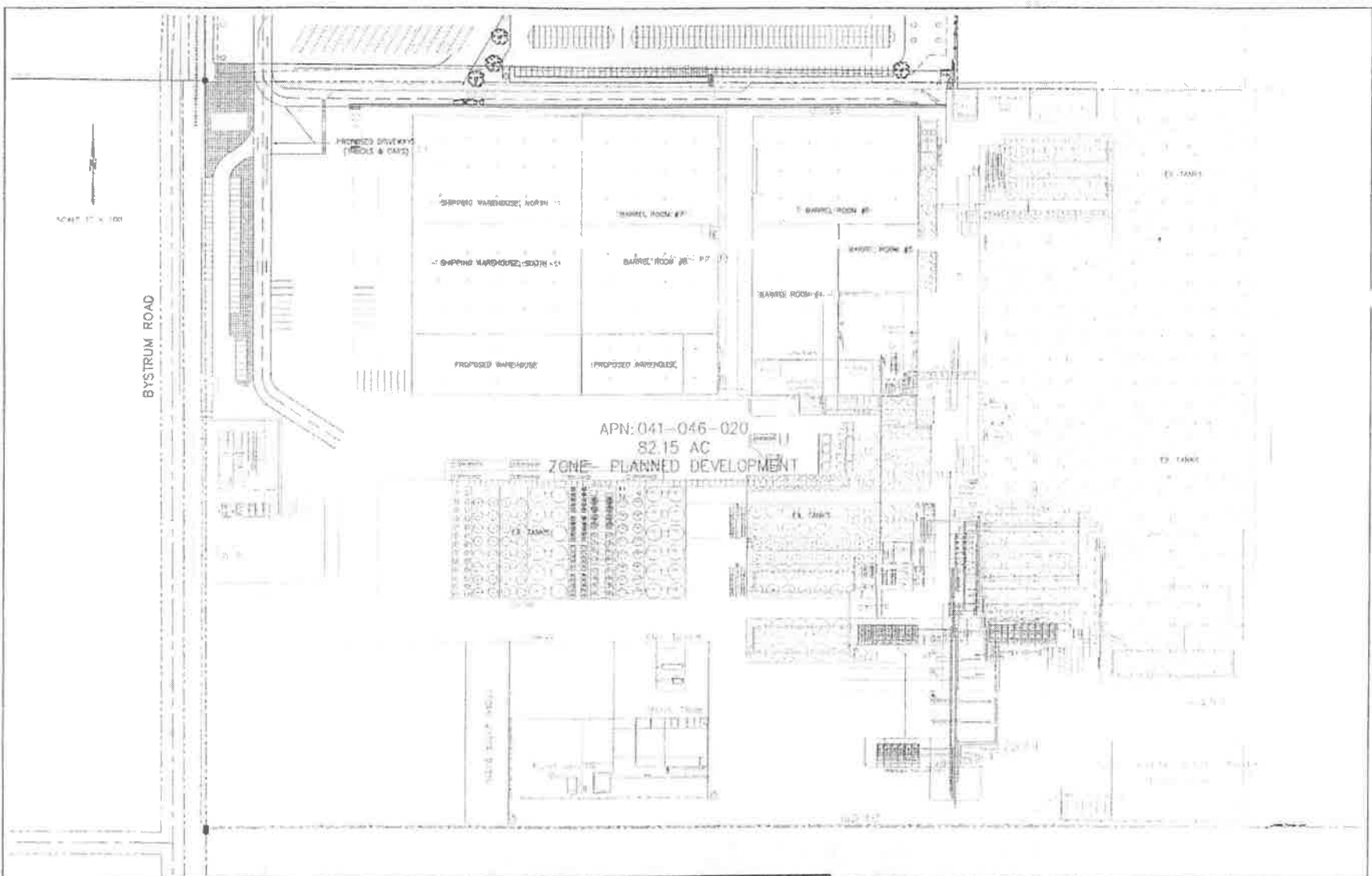
REZONING
BRONCO WINE CO
8342 BYSTRUM ROAD
CERES, CA 95107

JOB No. 1509
Sheet 1
C1
01



REZ 2009-04 - BRONCO WINE CO
Proposed Site Plan

| | | | |
|------------------------------------|---|--|------------------------------------|
| ENGINEERS (209) 526-1515 | DATE 1-28-04 BY CHECKED APPROVED | REZONING BRONCO WINE CO 8342 SYSTEM ROAD CERES CA, 95307 | JOB NO. 2004 SHEET C2 |
| | PROJECT NO. | | |



RE
P.O. B
521 13

REZ 2009-04 – BRONCO WINE CO
Existing Planned Development (PD-6)

NEERS
596 1515

DATE: 1-28-09
BY:
APPROVED:

REZONING
 BRONCO WINE CO
 6342 BYSTRUM ROAD
 CERES, CA, 95407

APPROVED
C3
 04

AREA MAP

NOT TO SCALE
PROJECT ADDRESS: 6342 BYSTRUM ROAD
CERES, CA 95307

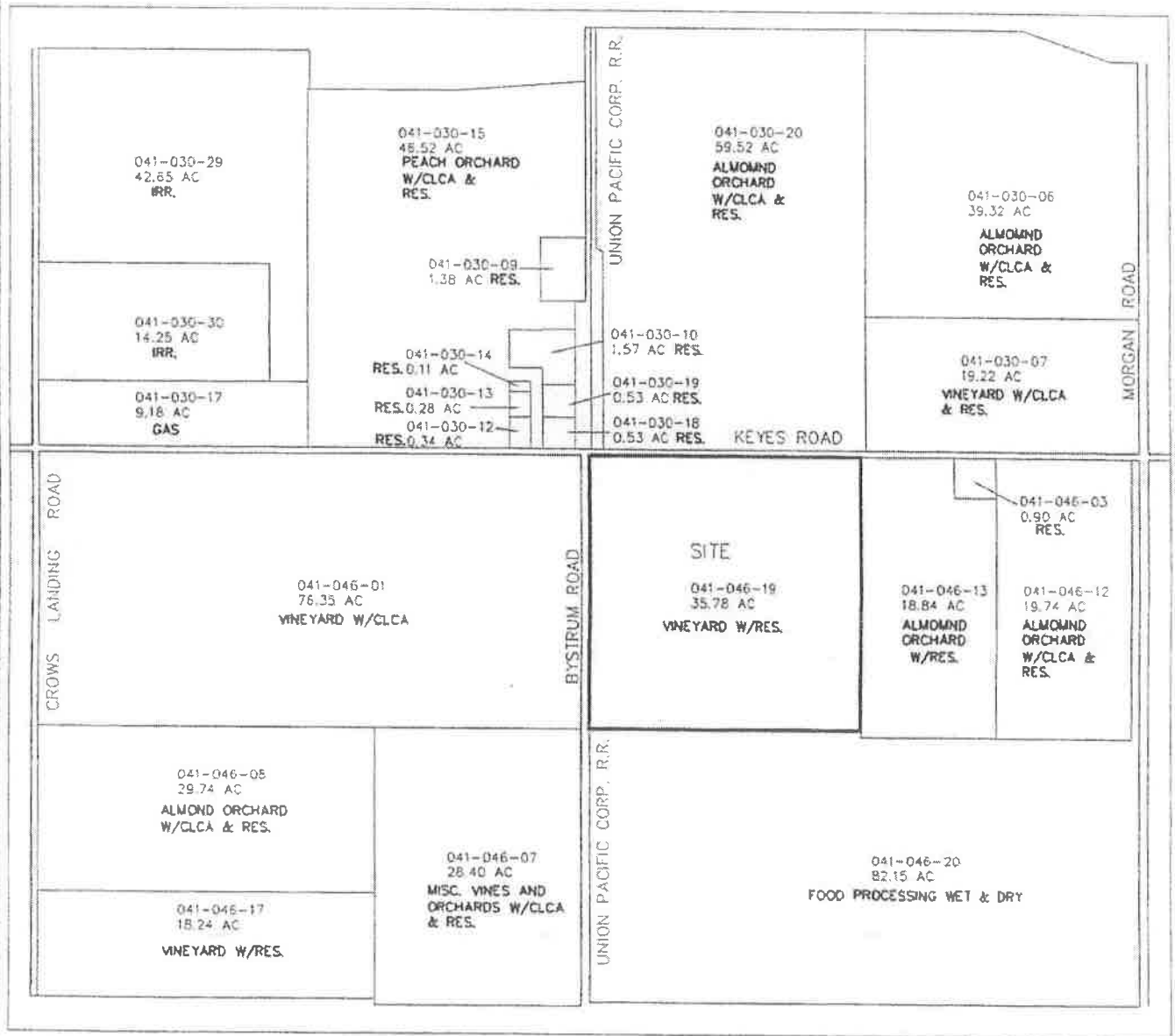


KEY:

- IRR - IRRIGATED OPEN LAND
W/ CLCA & RES.
- GAS - GAS STATION-AUTO
REPAIR SHOP W/ RES
- RES - RESIDENTIAL

LEGEND:

— PROJECT SITE



R. B. WELTY & ASSOCIATES, Inc.
521 13th STREET / P.O. BOX 1724
MODESTO, CA. 95353-1724
(209) 526-1515

MAP / ILLUSTRATION
G3 PROPERTIES, INC.
502 E. WHITMORE AVENUE
MODESTO, CA 95358

| |
|----------------------|
| JOB NO. 7809 |
| DATE: 02/13/09 |
| BY: DJR |
| SHEET |
| 1 OF 1 |

DEVELOPMENT STANDARDS

**REZONE APPLICATION NO. 2009-04
BRONCO WINE CO.**

Department of Planning & Community Development

1. This use is to be conducted as described in the application and supporting information (including the plot plan), as approved by the Board of Supervisors and in accordance with other laws and ordinances.
2. Construction of the project shall comply with standardized dust controls adopted by the San Joaquin Valley Air Pollution Control District.
3. A plan for any proposed signs indicating the location, height, area of the sign, and message must be approved by the Planning Director (or their appointed designee) prior to installation.
4. Trash bins shall be kept in trash enclosures constructed of materials compatible with the architecture of the development. Trash enclosures shall be placed in locations as approved by the refuse collecting agency and the Planning Director.
5. All outside storage and mechanical equipment shall be screened from the view of any public right-of-way by a screen fence of uniform construction or landscaping as approved by the Planning Director. Any required water tanks for fire suppression shall be painted to blend with the surrounding landscape or screened with landscaping and shall not be used as a sign unless approved by the Planning Director.
6. Applicant, and/or subsequent property owner(s), must obtain building permits for all proposed structures, equipment, and utilities. Plans shall be prepared by a California licensed engineer working within the scope of their license.
7. Prior to occupancy, a landscaping plan indicating the type of plants, initial plant size, location, and method of irrigation shall be submitted to and approved by the Planning Director.
8. The applicant, or subsequent property owner, shall be responsible for maintaining landscape plants in a healthy and attractive condition. Dead or dying plants shall be replaced with materials of equal size and similar variety. Any dead trees shall be replaced with a similar variety of a 15-gallon size or larger.
9. Should any archeological or human remains be discovered during development, work shall be immediately halted within 150 feet of the find until it can be evaluated by a qualified archaeologist. If the find is determined to be historically or culturally significant, appropriate mitigation measures to protect and preserve the resource shall be formulated and implemented.

10. The developer shall pay all applicable Public Facilities Impact Fees and Fire Protection Development/Impact Fees as adopted by Resolution of the Board of Supervisors. For the Public Facilities Impact Fees, the fees shall be based on the Guidelines Concerning the Fee Payment Provisions established by County Ordinance C.S. 824 as approved by the County Board of Supervisors, and shall be payable at the time determined by the Department of Public Works.
11. The applicant is required to defend, indemnify, or hold harmless the County, its officers and employees from any claim, action, or proceedings against the County to set aside the approval of the project which is brought within the applicable statute of limitations. The County shall promptly notify the applicant of any claim, action, or proceeding to set aside the approval and shall cooperate fully in the defense.
12. Pursuant to Section 404 of the Clean Water Act, prior to construction, the developer shall be responsible for contacting the US Army Corps of Engineers to determine if any "wetlands," "waters of the United States, or other areas under the jurisdiction of the Corps of Engineers are present on the project site, and shall be responsible for obtaining all appropriate permits or authorizations from the Corps, including all necessary water quality certifications, if necessary.
13. Pursuant to Section 1600 and 1603 of the California Fish and Game Code, prior to construction, the developer shall be responsible for contacting the California Department of Fish and Game and shall be responsible for obtaining all appropriate stream-bed alteration agreements, permits or authorizations, if necessary.
14. Prior to construction, the developer shall be responsible for contacting the California Regional Water Quality Control Board to determine if a "Notice of Intent" is necessary, and shall prepare all appropriate documentation, including a Storm Water Pollution Prevention Plan (SWPPP). Once complete, and prior to construction, a copy of the SWPPP shall be submitted to the Department of Planning and Community Development.
15. Pursuant to the federal and state Endangered Species Acts, prior to construction, the developer shall be responsible for contacting the US Fish and Wildlife Service and California Department of Fish and Game to determine if any special status plant or animal species are present on the project site, and shall be responsible for obtaining all appropriate permits or authorizations from these agencies, if necessary.
16. Prior to issuance of a building permit, the applicant shall provide a written release from the San Joaquin Valley Air Pollution Control District confirming that the applicant has submitted an Air Impact Assessment application and paid all applicable off-site mitigation fees as required to comply with District Rule 9510.
17. Pursuant to Section 711.4 of the California Fish and Game Code (effective January 1, 2010), the applicant is required to pay a Department of Fish and Game filing fee at the time of recording a "Notice of Determination." Within five (5) days of approval of this project by the Planning Commission or Board of Supervisors, the applicant shall submit to the Department of Planning and Community Development a check for \$2,067.00, made payable to Stanislaus County, for the payment of Fish and Game, and Clerk Recorder filing fees.

Pursuant to Section 711.4 (e)(3) of the California Fish and Game Code, no project shall be operative, vested, or final, nor shall local government permits for the project be valid, until the filing fees required pursuant to this section are paid.

18. The Department of Planning and Community Development shall record a Notice of Administrative Conditions and Restrictions with the County Recorder's Office within 30 days of project approval. The Notice includes: Conditions of Approval/Development Standards and Schedule; any adopted Mitigation Measures; and a project area map.

Stanislaus Fire Prevention Bureau

19. Comply with California Fire Code as amended by the Keyes Fire Protection District.

Department of Environmental Resources

20. When converting the existing residence to an office for shipping and receiving, the existing septic system is to be destroyed and a new waste-water treatment system is to be installed which meets Measure X requirements. The new on-site wastewater disposal system (OSWDS) shall be by individual Primary & Secondary wastewater treatment units, operated under conditions and guidelines established by Measure X.
21. Future development of the administration office and the sales building shall require the wastewater disposal system(s) to meet Measure X requirements. The new on-site wastewater disposal system (OSWDS) shall be by individual Primary & Secondary wastewater treatment units, operated under conditions and guidelines established by Measure X.
22. The engineered on-site wastewater disposal system (OSWDS) design shall be designed for the maximum occupancy of the building. The leach field shall be designed and sized using data collected from soil profile and percolation tests performed at the location. The OSWDS designed system shall provide 100% expansion area.
23. The applicant shall determine, to the satisfaction of the Department of Environmental Resources (DER), that a site containing (or formerly containing) residences or farm buildings, or structures, has been fully investigated (via Phase I and II studies) prior to the issuance of a grading permit. Any discovery of underground storage tanks, former underground storage tank locations, buried chemicals, buried refuse, or contaminated soil shall be brought to the immediate attention of DER.
24. Any existing on-site well(s), utilized as a part of this project, shall comply with the following requirements:

Permits: Section 116550 (a) no person operating a public water system shall modify, add to or change his or her source of supply or method of treatment of, or change his or her distribution system unless the person first submits an application to the department and receives an amended permit as provided in this chapter authorizing the modification, addition, or change in his or her source of supply; and,

Technical report: A technical report for the public water system shall be submitted to the department as part of the permit application or when otherwise required by the department. This report may include, but not be limited to, detailed plans and specifications, water quality information, physical descriptions of the existing or proposed system, and financial assurance information. (A qualified registered engineer with at least three years experience in public water system design should prepare the report.)

25. If an additional well is required as a part of this project then water supply for the project is defined by State regulations as a public water system. Water system owner must submit plans for the water system construction or addition and obtain approval from this Department prior to construction. Prior to construction, the Supply Permit Application must include a technical report, prepared by a qualified professional engineer, that demonstrates compliance with State regulations and includes the technical, managerial, and financial capabilities of the owner to operate a public water system. Contact DER for the required submittal information.

Department of Public Works

26. A grading and drainage plan for the property shall be approved by the Department of Public Works prior to the issuance of any building permit. This plan shall verify all runoff is being kept on-site and not draining onto neighboring properties, railroad, or road rights-of-way. After the plan is determined to be acceptable to the Department of Public Works, the plans shall be implemented prior to the final and/or occupancy of any building.
27. If the street improvements are completed and accepted by the Department of Public Works before the issuance of a building permit, then a financial guarantee will not be required.
28. Prior to approval of the off-site improvement plans, the developer shall file a Notice of Intention (NOI) with the California Regional Water Quality Control Board and a Waste Discharge Identification Number must be obtained and provided to the Department of Public Works prior to building occupancy.
29. An Encroachment Permit must be obtained for any work in the County right-of-way.
30. No parking, loading, or unloading of vehicles shall be permitted within the right-of-way of Keyes Road.
31. Any new driveway locations and widths shall be approved by this Department.

Building Permits Division

32. Building permits are required for all structures and must comply with California Code of Regulations Title 24. Handicap accessibility to the entire site and all structures is required.

Turlock Irrigation District

33. District electric utility maps show existing distribution and transmission facilities within or near the proposed project. The owner/developer must apply for a facility change for any pole or electrical facility relocation. Facility changes are performed at developer's expense.

San Joaquin Valley Air Pollution Control District

34. Based on information provided to the District, the proposed project would equal or exceed 25,000 square feet of light industrial space. Therefore, the District concludes that the proposed project is subject to District Rule 9510 (Indirect Source Review).
35. Prior to issuance of any building permit the applicant shall demonstrate compliance with District Rule 9510, including payment of all applicable fees before issuance of a building permit.
36. The proposed project may require District permits. Prior to the start of construction, the project proponent should contact the District's Small Business Assistance Office to determine if an Authority to Construct (ATC) is required.
37. The proposed project may be subject to the following District rules:
- Regulation VIII (Fugitive PM 10 Prohibitions)
 - Rule 4102 (Nuisance)
 - Rule 4601 (Architectural Coatings)
 - Rule 4641 (Cutback, Slow Cure, Emulsified Asphalt, Paving and Maintenance Operations)
38. In the event an existing building will be renovated, partially demolished or removed, the project may be subject to District Rule 4002 (National Emission Standards for Hazardous Air Pollutants).

Mitigation Measures

(Pursuant to California Public Resources Code 15074.1: Prior to deleting and substituting for a mitigation measure, the lead agency shall do both of the following:
1) Hold a public hearing to consider the project; and
2) Adopt a written finding that the new measure is equivalent or more effective in mitigating or avoiding potential significant effects and that it in itself will not cause any potentially significant effect on the environment.)

39. All exterior lighting shall be designed (aimed down and toward the site) to provide adequate illumination without a glare effect. This shall include but not be limited to the use of shielded light fixtures to prevent skyglow (light spilling into the night sky) and the installation of shielded fixtures to prevent light trespass (glare and spill light that shines onto neighboring properties).

40. Roadway improvement plans shall be submitted to Public Works prior to the issuance of a building or grading permit, whichever comes first. The improvement plans shall include left turn acceleration and deceleration lanes for the proposed main (truck) entrance and a left turn lane for the employees entrance on the east side of the property from Keyes Road. The plans shall use CalTrans Traffic Manual and Stanislaus County Standards and Specifications. A four-foot asphalt shoulder, as per Stanislaus County Standards and Specifications, will be included on Keyes Road. The roadway improvement plans shall be approved and installed prior to occupancy of any building permit associated with this site.
41. Keyes Road is classified as a 60-foot collector in this area. The applicant's engineer or surveyor shall prepare an Easement Deed for 30-feet south of the centerline of Keyes Road along the entire frontage of the project's parcel. If additional road right-of-way is needed for Keyes Road along the parcel frontage as per the approved roadway improvement plans, that additional width shall be included in the Easement Deed. The Easement Deed shall be submitted to Public Works after the roadway improvement plans are approved and prior to occupancy of any building associated with this site.
42. An Engineer's Estimate shall be provided so the amount of the financial guarantee can be determined. This will be based on the County approved street improvement plans. This shall be submitted prior to issuance of a building permit and once the improvement plans have been approved by the County.
43. A Financial Guarantee in a form acceptable to the Department of Public Works shall be deposited for the street improvement installation along the frontage on Keyes Road with the department prior to the issuance of the first building permit.

*Please note: If Development Standards are amended by the Planning Commission or Board of Supervisors, such amendments will be noted in the upper right hand corner of the first page of the Development Standards, new wording is in **bold**, and deleted wording will have a ~~line through it~~.*

As Approved by the Planning Commission
March 18, 2010

DEVELOPMENT SCHEDULE

**REZONE APPLICATION NO. 2009-04
BRONCO WINE CO.**

The conversion of the single-family dwelling to a shipping and receiving office, construction of the employee and truck parking lots and access roads, and compliance with all applicable development standards shall begin within 18 months of project approval.

(I:\Staff\pt\REZ\2009\REZ 2009-04 - Bronco Wine Co\Staff Report\Staff Report.wpd)

TO WHOM IT MAY CONCERN;

REGARDING REZONING OF BRONCO WINE CO.

I AM EVELYN BURNS AND I OWN THE PROPERTY ON THE EAST SIDE OF PROPOSED REZONING. MY SON NICK DOES THE FARMING. MY CONCERN IS THE ROAD THAT WILL RUN IN FRONT OF MY HOUSE. MY BEDROOM IS ABOUT 30 FEET FROM THE PROPOSED ROAD. IT WILL BE LIKE CARS DRIVING IN MY BEDROOM,.. BUSHES ARE NOT THE ANSWER TO THIS PROBLEM. A SOUND BARRIER WALL MIGHT HELP. I AM 75 YEARS OLD AND SHOULD BE ABLE TO ENJOY MY LAST YEARS HERE. THE TRAFFIC WILL BE LIKE KEYES RD,

NOW THE ROAD ISSUE. THERE WILL BE 3 DRIVEWAYS MAKING AN ENTRANCE AND EXIT AT KEYES RD.. I THINK THAT IS A LITTLE BIT MUCH FOR THAT AREA. WE HAVE TROUBLE GETTING ON THE ROAD NOW AT QUITTING TIME AND IT IS A HALF A MILE AWAY AND NOW THEY WILL BE CLOSE COMING AND GOING.,NOT TO SAYA MADHOUSE AND DANGEROUS AND UNSAFE. HEAVEN HELP US.

RECEIVED

STATEWIDE LAND PLANNING & COMMUNITY DEVELOPMENT DEPT

EXHIBIT H

THE LAST ISSUE IS THE REZONING OF FARM GROUND
TO WAREHOUSE STATUS. IF YOU WANT A WAREHOUSE
FACILITY GO TO THE BEARD TRACT AND LEAVE THE
FARM GROUND FOR FARMING AS IT SHOULD BE.

Evelyn Burns

From: Planning Planning
To: Wyse, Rachel
CC: Ford, Kirk
Date: 3/5/10 8:03 AM
Subject: Fwd: REZONE APPLICATION NO. 2009-04-BRONCO WINE CO.

-- -- -- Let Us Know How We Are Doing -- -- --

Please take a moment and complete the Customer Satisfaction Survey by clicking on the following link:

<http://www.co.stanislaus.ca.us/SurveyChoice.htm>

>>> "Pat Titus" <patpilot@clearwire.net> 3/4/10 6:37 PM >>>

To whom it might concern:

We are residents at 1112 E. Keyes Rd., directly in front of Bronco Winery. In the 40 plus years we have lived here this road has gone from one where our children could ride bikes and run their 4-H sheep down the road to one where you take your life into your hands to try to get out of the driveway. After the Highway 99 and Keyes Rd. overpass was completed the traffic increased at least threefold. With the addition of the winery the traffic again increased greatly. The hundreds of trucks and vehicles entering and existing the road create severe congestion and dangerous conditions.

The speed limit has not changed and passing is still allowed on most of Keyes Road and I'm certain you would find that most of the traffic is going faster than 55 mph. The commuters to the bay area treat this road like a freeway. They don't treat it as a country road. Yet, it is a two lane country road and a dangerous one. There needs to be a posted 45 mph zone with no passing where ever these trucks and other vehicles are accessing the road.



Putting a employee road on the East side of the property with 180 or more vehicles will create a serious noise problem for the residents living adjacent to it. They built that house well off the road to avoid noise. Now they have the noise of the winery and if the winery has its way they will have 180 vehicles driving right by their bedroom. How would you like that? Day and night!

Mrs. Patricia Titus

Chair Navarro and Commissioners Layman and Pires left the Chambers.

- D. REZONE APPLICATION NO. 2009-04 - BRONCO WINE CO. - Request to rezone**
a 35.78 acre parcel from A-2-40 (General Agriculture) to P-D (Planned Development) to allow expansion of the adjoining Bronco Wine facility by conversion of an existing house into an office, construction of two (2) new 14,400 square foot office buildings, an associated parking lot and two (2) driveways on E. Keyes Road. The project site is located at 800 E. Keyes Road, at the southeast corner of E. Keyes and Bystrum Roads, in the Ceres area. The Planning Commission will consider a CEQA Mitigated Negative Declaration on this project.
APN: 041-046-019
Staff Report: Rachel Wyse Recommends **APPROVAL**.
Public hearing opened.
OPPOSITION: Alice Roche, 1130 E Keyes Road, Ceres.
FAVOR: No one spoke.
Public hearing closed.
Ramos/Assali, 5-0, **APPROVED THE STAFF RECOMMENDATIONS AS OUTLINED IN THE STAFF REPORT.**

Chair Navarro and Commissioners Layman and Pires returned to the Chambers.

| |
|---|
| <p style="text-align: center;">EXCERPT</p> <p style="text-align: center;">PLANNING COMMISSION</p> <p style="text-align: center;">MINUTES</p> <p style="text-align: center;"> _____ Secretary, Planning Commission</p> <p style="text-align: center;"> _____ Date</p> |
|---|

TRAFFIC IMPACT ANALYSIS
FOR
BRONCO WINERY DISTRIBUTION CENTER
Stanislaus County

Prepared For:

Anderson-Litfin, Inc.
1385 Venture Lane
Turlock, CA 95380

Prepared By:

KD Anderson & Associates, Inc.
3853 Taylor Road, Suite G
Loomis, CA 95650
(916) 660-1555

November 23, 2016
Revised March 15, 2017

0480-01



KD Anderson & Associates, Inc.

Transportation Engineers

**TRAFFIC IMPACT ANALYSIS FOR
BRONCO WINERY DISTRIBUTION CENTER**
Stanislaus County

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**TRAFFIC IMPACT ANALYSIS FOR
BRONCO WINERY DISTRIBUTION CENTER**
Stanislaus County

INTRODUCTION

This report summarizes **KD Anderson & Associates** analysis of the traffic impacts associated with development of the proposed Bronco Winery Distribution Center. The project consists of development of additional warehouse and building space at the existing winery facility located south of Keyes Road and east of Bystrum Road. The project includes development of a rail spur connection to the existing U.P.R.R. line to permit shipment of product by rail. The existing U.P.R.R. line runs along the west border of the site adjacent to Bystrum Road. Warehouse space will be developed in phases over a number of years and will dependent on market conditions. Figures 1 displays the project location.

This study provides a focused analysis of traffic impacts in the immediate vicinity of the site associated with the expanded winery facilities. The scope of the analysis is based upon input from Stanislaus County following the County's initial review of the project application. The analysis focuses on impacts to the Keyes Road / Bystrum Road intersection immediately adjacent to the U.P.R.R. crossing of Keyes Road. The main access to the existing winery is located on Bystrum Road approximately 2,000 feet south of Keyes Road. A second project access is located on Keyes Road 3/4 of a mile east of Bystrum Road.

Traffic operations have been quantified relative to "in season" conditions which include trucking and employee operations associated with grape harvest and crush in addition to typical shipping and receiving winery operations. In season operations typically occur from mid July through mid November and include 24 hour facility operations, 7 days a week. Additionally, although the proposed winery expansion would be realized over a number of years, this study assumes build out of the proposed project to quantify resulting "Existing plus Project" traffic operating conditions.

Project Description

Build out of the entire project will include construction of eight warehouses totaling 629,500 sf, 4 office buildings totaling 101,000 sf and 2 assembly buildings totaling 12,600 sf immediately north of the company's existing winery facilities. The proposed Phase 1 portion of the project will consist of one 120,000 sf warehouse building and the railroad spur lines. The railroad spur lines will extend for approximately 1,400' immediately east of the existing U.P.R.R. line between Keyes Road and the project main access. The two existing access gates will continue to serve the expanded project site. Figure 2 displays the proposed site plan.

Project proponents expect that the project will not increase the capacity of the site for wine making. As a result, the project is not expected to see an increase in the number of trucks

bringing grapes to the site during Crush. However, the project involves creation of an appreciable amount of wine storage to accommodate wine produced on-site or to accommodate wine created or bottled elsewhere and trucked to this site for bottling and/or storage prior to eventual shipment. As such, the project would involve some additional employee trips to and from the site by automobile, as well as wine deliveries and shipments by truck and rail.

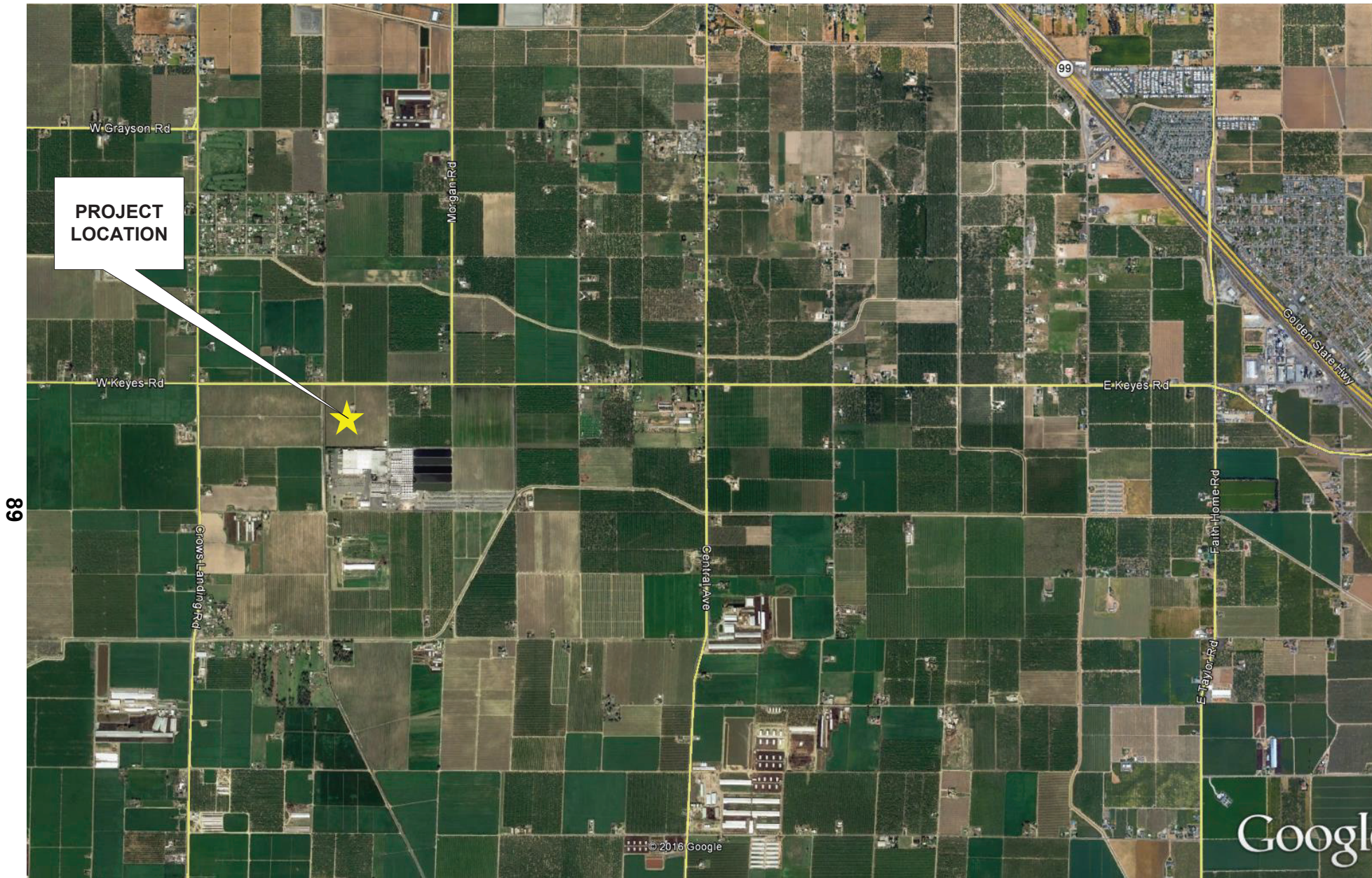
General Study Methodology

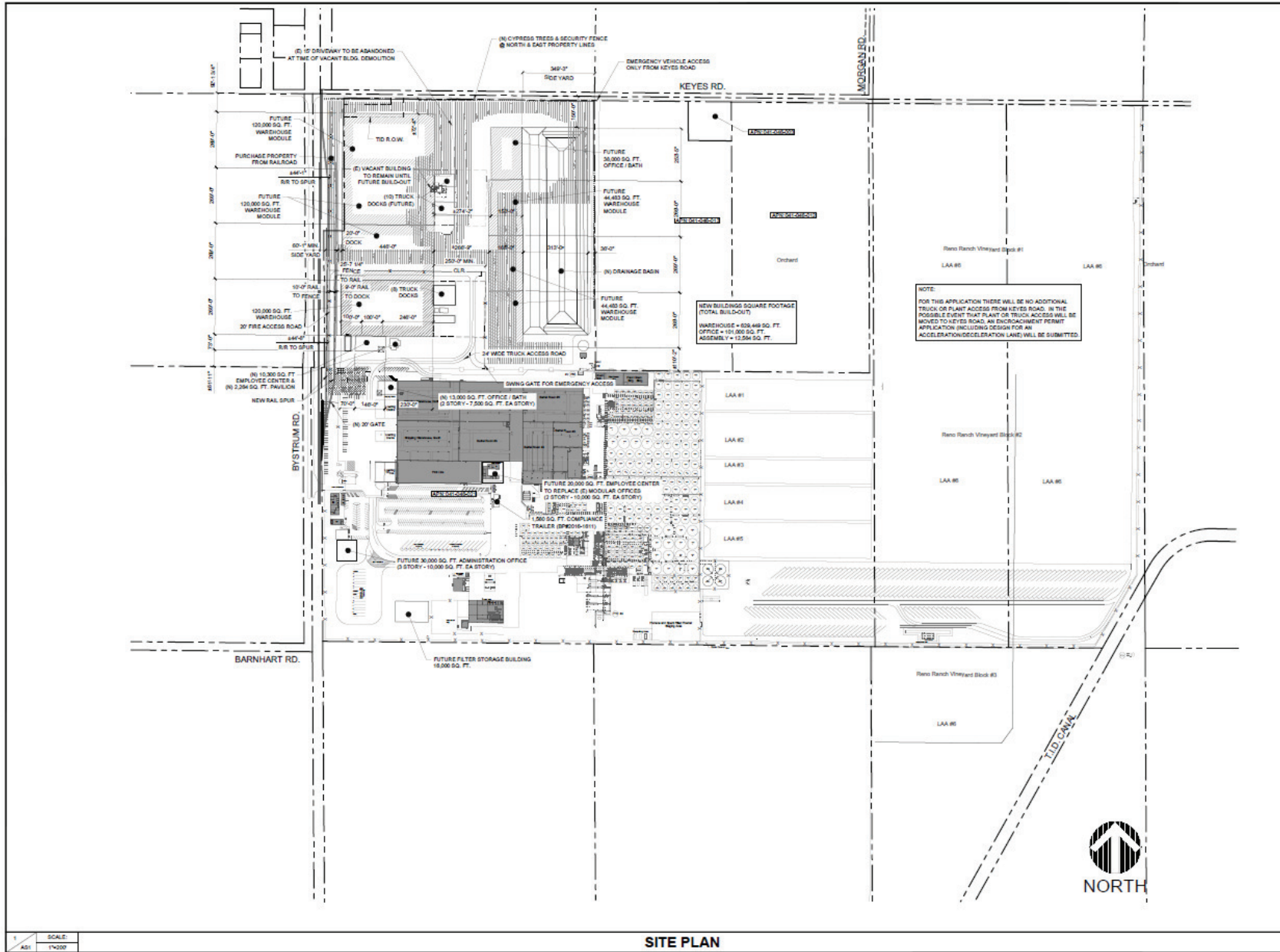
The methodology used to prepare this Traffic Impact Study follows an approach that is recognized by members of the traffic engineering profession, is consistent with CEQA guidelines and conforms to Stanislaus County guidelines for traffic impact studies.

The first phase of the study included the collection of traffic data and the analysis of that data to determine existing operating conditions. Peak hour and daily traffic counts were conducted in the vicinity of the project site. This data was used to calculate current operating Levels of Service using procedures accepted by Stanislaus County.

The second phase of the analysis involved identifying the number of trips expected to be generated by the proposed project. Traffic count data together with information on existing and proposed employee numbers and truck traffic numbers has been used to estimate trip generation quantities associated with the wine facility expansion.

Lastly, new trips associated with the proposed project were assigned to the study area street system to quantify Existing plus Project operating conditions. The analysis considers new automobile and truck traffic quantities as well as rail operations.





SITE PLAN

| DATE | NO. | REVISIONS | BY |
|----------|-----|-----------------------------|----|
| 7/7/15 | | PRELIM CLIENT REVIEW | CB |
| 10/27/15 | | PRELIM WAREHOUSE FLOOR PLAN | CB |

ENGINEERING INCORPORATED
 STRUCTURAL DESIGN AND CIVIL ENGINEERING
 420 DeWey Ave., Berkeley, CA 94704
 Tel: (925) 596-9033 Fax: (925) 532-0867
 Email: CDEng@CDEngEngineering.com

ANDERSON LTRIN, INC.
 CIVIL ENGINEERING
 Phone: 209.643.8141 • Fax: 209.643.5201
 1381 Ventura Lane • P.O. Box 1330 • Turlock, CA 95381



SITE PLAN
CENTRAL VALLEY DISTRIBUTION CENTER
 FOR: BRONCO WINERY
 800 KEYES ROAD
 CERES, CA 95307

| | |
|-----------|------------|
| JOB #: | 4129-4-15 |
| DATE: | 7/14/15 |
| SCALE: | SHOWN |
| APPROVED: | CB |
| DRAWN: | HJ |
| SHEET: | AS1 |

SITE PLAN

figure 2

EXISTING SETTING

Study Area

The limits of this analysis were identified in consultation with Stanislaus County staff and include intersections and roadway segments in the vicinity of the project site as well as access to the site. The traffic impact analysis investigates the operational characteristics of the following intersections.

1. Keyes Road / Bystrum Road (Bystrum Rd stop sign controlled)
2. Keyes Road / Bronco Winery East Truck Access (East access stop sign controlled)

The locations of these intersections along with the existing road network are shown on Figure 3. The text that follows describes the characteristics of each facility.

Keyes Road is an east-west facility extending through the southerly portion of Stanislaus County. The roadway extends from Laird Road in the west past the east county line, a distance of approximately 27 miles. Interchange access to SR 99 is provided approximately 4 miles east of the project site. Keyes Road is a 2-lane rural roadway and classified as a Collector Road adjacent to the project site. The roadway provides 12' travel lanes and 1'-2' paved shoulders. No left turn channelization is provided at intersecting streets other than in the immediate vicinity of SR 99. Keyes Road has a 55 mph prima facie speed limit. The roadway currently carries approximately 6,650 daily vehicles adjacent to the project site, with 11% large truck traffic based upon classification counts conducted for this study.

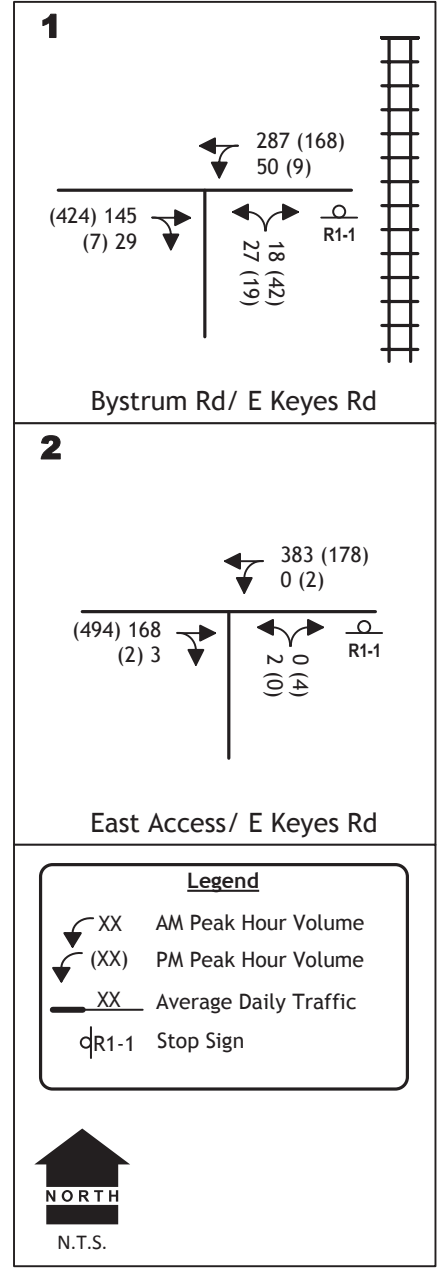
Bystrum Road is a local road on the west border of the project site which extends from Keyes Road south to Taylor Road. The roadway is paved for approximately 2,000' to the Bronco Winery entrance and then continues as a dirt/gravel facility to the south with a 1-lane bridge crossing of the canal immediately north of Taylor Road. Bystrum Road provides a connection to Barnhart Road at the southwest corner of the winery site and also provides access to other agricultural uses south of the winery site. North of the winery access, the roadway provides two travel lanes and 24' of pavement. Bystrum Road is stop sign controlled at Keyes Road. Traffic counts conducted for this analysis indicate the roadway carries approximately 1,480 daily vehicles north of the winery access.

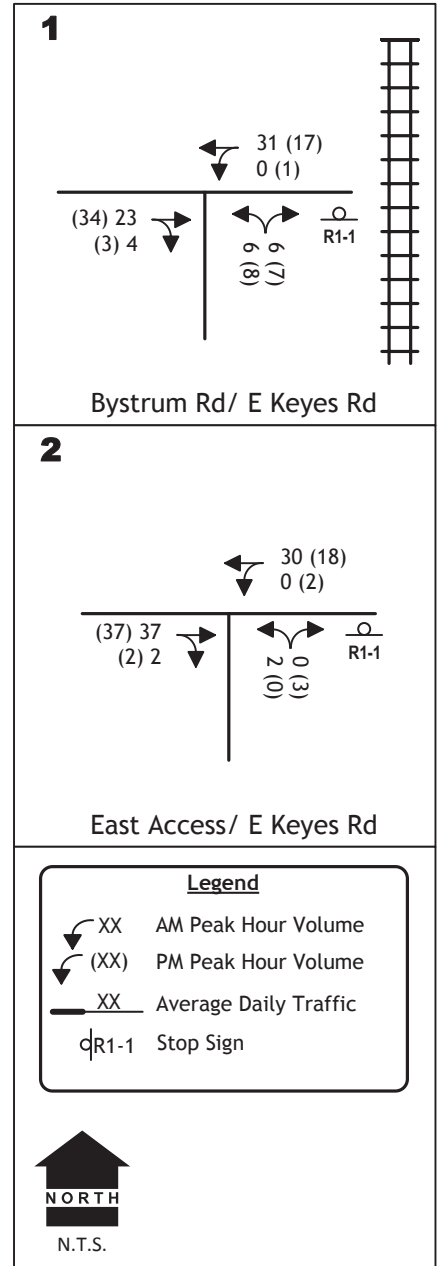
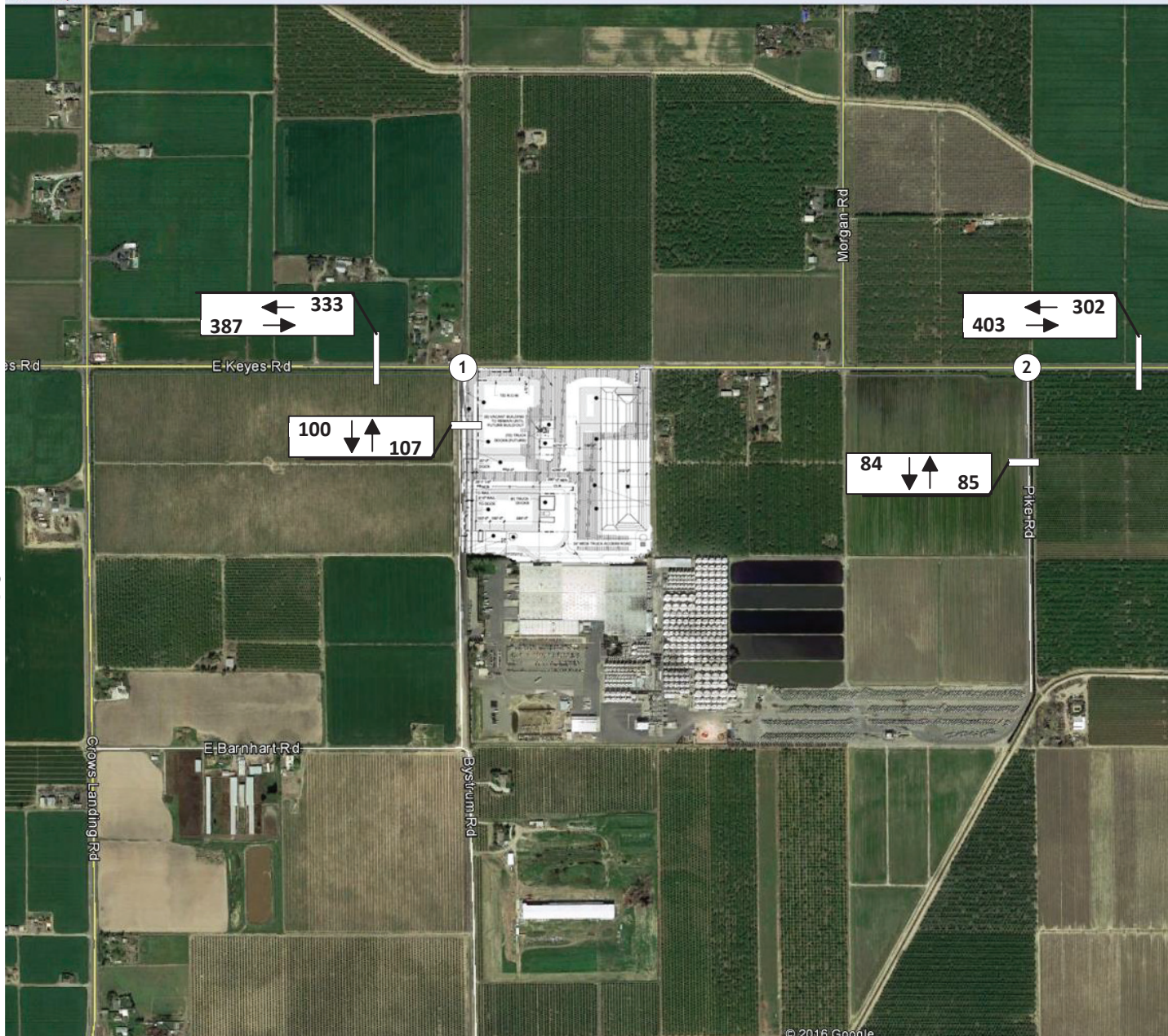
Union Pacific Rail Line. A north-south U.P. rail line extends along the west border of the project site. This local line extends from Modesto in the north to Turlock in the south. The rail line crosses Keyes Road approximately 45' east of the centerline of Bystrum Road. The rail crossing has active traffic controls, consisting of crossing arms, warning lights and pavement delineation. Advance pavement delineation and signs are also provided on Keyes Road approximately 400' to the east and west. Pavement condition at the crossing is judged to be "good". There are no vehicle pull outs at the crossing. Adequate sight distance is provided to the arms and warning lights from eastbound and westbound Keyes Road. The crossing conforms to requirements presented in the CA MUTCD for active traffic control devices for grade crossings.

Existing Traffic Volumes. To determine existing traffic volumes and obtain more information about traffic conditions in the study area, information regarding daily, a.m. and p.m. peak hour traffic volumes was assembled. New weekday intersection and roadway counts were conducted on October 4, 2016. Intersection counts were performed from 7:00 - 9:00 a.m. and 4:00 - 6:00 p.m. at the two study intersections. Daily 24 hour roadway counts were also conducted on four roadway segments. These included:

- Keyes Road west of Bystrum Road
- Keyes Road east of the East Truck Access to Bronco Winery
- Bystrum Road south of Keyes Road
- East Truck Access road south of Keyes Road

All intersection and roadway counts were conducted in 15 minute increments and included separate truck classification counts. The peak hour intersection volumes and daily roadway volumes are shown in Figures 3 and 4. Figure 3 displays total traffic volumes, while Figure 4 displays truck traffic volumes. All traffic counts are included in the Appendix to this report. Table 1 summarizes hourly volumes on each of the roadways providing access to the project site to illustrate the distribution of traffic throughout the day. As shown, traffic volumes on each roadway are dispersed throughout the day and nighttime hours over the 24 hour period. This reflects the 24 hour operation and multiple employee shifts associated with the existing winery operations.





EXISTING TRUCK TRAFFIC VOLUMES AND LANE CONFIGURATIONS

**TABLE 1
HOURLY VOLUME SUMMARY
ON ACCESS ROADS SERVING PROJECT SITE**

| | Bystrum Road (Auto and Truck Access) | | East Access Road (Truck Access) | |
|------------|--------------------------------------|--------------------------|---------------------------------|--------------------------|
| | Total Traffic | Percent of Daily Traffic | Truck Traffic | Percent of Daily Traffic |
| 12-1 a.m. | 26 | 1.7% | 7 | 4.0% |
| 1-2 | 9 | 0.6% | 9 | 5.5% |
| 2-3 | 19 | 1.3% | 11 | 6.5% |
| 3-4 | 22 | 1.5% | 13 | 7.5% |
| 4-5 | 32 | 2.1% | 4 | 2.5% |
| 5-6 | 86 | 5.8% | 7 | 4.0% |
| 6-7 | 111 | 7.5% | 1 | 0.5% |
| 7-8 | 110 | 7.5% | 2 | 1.0% |
| 8-9 | 87 | 5.9% | 2 | 1.0% |
| 9-10 | 44 | 3.0% | 5 | 3.0% |
| 10-11 | 55 | 3.7% | 12 | 7.0% |
| 11-12 p.m. | 51 | 3.5% | 11 | 6.5% |
| 12-1 | 67 | 4.5% | 2 | 1.0% |
| 1-2 | 93 | 6.3% | 13 | 7.5% |
| 2-3 | 144 | 9.8% | 12 | 7.0% |
| 3-4 | 101 | 6.8% | 3 | 1.5% |
| 4-5 | 76 | 5.2% | 3 | 1.5% |
| 5-6 | 104 | 7.0% | 7 | 4.0% |
| 6-7 | 63 | 4.3% | 8 | 4.5% |
| 7-8 | 34 | 2.3% | 4 | 2.5% |
| 8-9 | 19 | 1.3% | 5 | 3.0% |
| 9-10 | 24 | 1.6% | 10 | 6.0% |
| 10-11 | 40 | 2.7% | 11 | 6.5% |
| 11-12 | 60 | 4.0% | 7 | 4.0% |
| | 1,477 | 100% | 169 | 100% |

Information has been assembled by the project proponents to quantify the average number of existing truck trips generated by the site for "in season" operating conditions. This is as presented in "Exhibit A" of the initial project application to the County. This information has been compared to traffic counts conducted by the consultant on 10/4/16 to establish an in season baseline traffic condition for purposes of evaluating project impacts. Table 2 summarizes the average number of in season truck trips generated by the winery site. This information is summarized by the type of distribution truck. As shown, five categories of truck traffic have been identified with an average of 183 trucks per day. These trucks in turn generate an average of 366 truck trips per day.

Table 3 provides a comparison of this average daily truck information verse that observed by the consultant on 10/4/16. As shown, a slightly lower number of trucks was generated by the site on that day when roadway and intersection counts were conducted for this analysis. The site was observed to generate 347 truck trips over a 24 hour period, or 5% less than the volume discussed above. Total truck traffic generated by the site has therefore been increased to reflect average in season conditions for purposes of this analysis.

**TABLE 2
BRONCO WINERY AVERAGE DAILY TRUCK TRAFFIC VOLUMES
EXISTING CONDITIONS (IN SEASON)**

| Distribution Type | Existing Baseline Conditions | | | |
|------------------------------|------------------------------|---------------------|-----|-------|
| | Number Trucks | Average Daily Trips | | |
| | | In | Out | Total |
| Shipping Trucks | 14 | 14 | 14 | 28 |
| Tanker Trucks | 34 | 34 | 34 | 68 |
| Grape Trucks ⁽¹⁾ | 105 | 105 | 105 | 210 |
| Pomace Trucks ⁽¹⁾ | 23 | 23 | 23 | 46 |
| Delivery Trucks | 7 | 7 | 7 | 14 |
| | 183 | | | 366 |

⁽¹⁾ Grape and Pomace trucks only operate in season during grape harvest and crush.

**TABLE 3
BRONCO WINERY SITE DAILY TRUCK VOLUME COMPARISON**

| 10/4/16 Traffic Counts | | | Estimated Average Daily Trucks, Baseline Condition throughout Season | | |
|------------------------|-----|-------|--|-----|-------|
| In | Out | Total | In | Out | Total |
| 173 | 174 | 347 | 183 | 183 | 366 |

Total trucks counted on 10/4/16 was 5% lower than estimated average daily trucks throughout the season.

Existing Train Volumes. Observations conducted on Tuesday, 10/04/16, indicated one southbound and one northbound train crossing during this 24 hour period. The duration of these train crossings (railroad arms down) were 40 seconds and 54 seconds, respectively. Table 4 summarizes this information.

TABLE 4
24 HOUR TRAIN VOLUME AT KEYES ROAD CROSSING (10/04/2016)

| No. | Direction | Gates Down | | Duration |
|-----|------------|------------|----------|----------|
| | | Start | End | |
| 1 | Southbound | 8:02:48 | 8:03:28 | 40 sec |
| 2 | Northbound | 10:16:01 | 10:16:55 | 54 sec |

Standards of Significance: Capacity / Level of Service Analysis

Level of Service. The quality of traffic flow through intersections and on individual roadway segments is described in terms of operating Level of Service. "Level of Service (LOS)" is a qualitative measure of traffic operating conditions whereby a letter grade "A" through "F", corresponding to progressively worsening operating conditions, is assigned to an intersection or roadway segment. Tables 5 presents the characteristics associated with each LOS grade.

The *Highway Capacity Manual* presents methodologies for calculating practical capacity and Level of Service at intersections. At signalized intersections and intersections controlled by all-way stop signs, traffic conditions are described in terms of the average length of the delays experienced by all motorists. Intersection configuration, traffic volumes and traffic signal timing are all factors that enter into determination of the length of average delay and the resulting Level of Service. The delays experienced at intersection controlled by side street stop signs are different. Motorists waiting to turn must yield the right of way to through traffic, and the length of delays can vary on each approach to the intersection. For this analysis the length of delays experienced by motorists on each approach has been calculated. Intersection operations have been quantified based upon Highway Capacity Manual procedures, consistent with Stanislaus County requirements.

**TABLE 5
LEVEL OF SERVICE DEFINITION**

| Level of Service | Signalized Intersection | Unsignalized Intersection | Roadway (Daily) |
|------------------|---|--|---|
| "A" | Uncongested operations, all queues clear in a single-signal cycle. Delay \leq 10.0 sec | Little or no delay. Delay \leq 10 sec/veh | Completely free flow. |
| "B" | Uncongested operations, all queues clear in a single cycle. Delay $>$ 10.0 sec and \leq 20.0 sec | Short traffic delays. Delay $>$ 10 sec/veh and \leq 15 sec/veh | Free flow, presence of other vehicles noticeable. |
| "C" | Light congestion, occasional backups on critical approaches. Delay $>$ 20.0 sec and \leq 35.0 sec | Average traffic delays. Delay $>$ 15 sec/veh and \leq 25 sec/veh | Ability to maneuver and select operating speed affected. |
| "D" | Significant congestions of critical approaches but intersection functional. Cars required to wait through more than one cycle during short peaks. No long queues formed. Delay $>$ 35.0 sec and \leq 55.0 sec | Longer traffic delays. Delay $>$ 25 sec/veh and \leq 35 sec/veh | Unstable flow, speeds and ability to maneuver restricted. |
| "E" | Severe congestion with some long standing queues on critical approaches. Blockage of intersection may occur if traffic signal does not provide for protected turning movements. Traffic queue may block nearby intersection(s) upstream of critical approach(es). Delay $>$ 55.0 sec and \leq 80.0 sec | Very long traffic delays, failure, extreme congestion. Delay $>$ 35 sec/veh and \leq 50 sec/veh | At or near capacity, flow quite unstable. |
| "F" | Total breakdown, stop-and-go operation. Delay $>$ 80.0 sec | Intersection blocked by external causes. Delay $>$ 50 sec/veh | Forced flow, breakdown. |

Sources: Highway Capacity Manual.

Significance Thresholds. A traffic impact is considered significant if it renders an unacceptable Level of Service on a street segment or at an intersection, or if it worsens already unacceptable conditions. Local jurisdictions typically adopt minimum Level of Service standards for use in traffic studies and environmental impact reports.

The Stanislaus County General Plan Circulation Element indicates that the County shall maintain LOS “D” or better for all County roadways and intersections, except within the sphere of influence of a city that has adopted a lower level of service standard, the City standard shall apply. As such, the LOS “D” standard has been used for this analysis to quantify the significance of traffic impacts at intersections.

Signal Warrant Criteria. At intersections controlled by side street stop signs, a supplemental signal warrant analysis is also typically used in determining the adequacy of operations and/or the need for improvements. As minor street traffic can experience significant delays when accessing a major street, side street delays at any single approach are typically not considered significant

unless side street volumes are large enough to meet peak hour warrants for installation of a traffic signal. Peak hour traffic signal warrants as presented in the California Manual of Uniform Traffic Control Devices (MUTCD) have been used for this analysis.

Criteria for Determining the Need for Left Turn Channelization. Lastly, as the subject intersections on Keyes Road do not provide left turn lane channelization, guidelines for the installation of left turn lanes have been reviewed for this analysis. The American Association of State Highway Transportation Officials (AASHTO) has identified guidelines for the installation of left turn lanes in their publication *A Policy on Geometric Design of Highways and Streets*. These guidelines, which are presented in their Table 9-23 of the publication and summarized below in Table 6, base the need for a left turn lane on the volume of traffic on the mainline road and the relative percentage of that traffic which turns left.

**TABLE 6
TRAFFIC VOLUMES JUSTIFYING LEFT TURN LANES**

| Opposing Volume (veh/hr) | Advancing Volume (veh/hr) | | | |
|-------------------------------|---------------------------|----------------|----------------|----------------|
| | 5% Left Turns | 10% Left Turns | 20% Left Turns | 30% Left Turns |
| 40-mph operating speed | | | | |
| 800 | 330 | 240 | 180 | 160 |
| 600 | 410 | 305 | 225 | 200 |
| 400 | 510 | 380 | 275 | 245 |
| 200 | 640 | 470 | 350 | 305 |
| 100 | 720 | 515 | 390 | 340 |
| 50-mph operating speed | | | | |
| 800 | 280 | 210 | 165 | 135 |
| 600 | 350 | 260 | 195 | 170 |
| 400 | 430 | 320 | 240 | 210 |
| 200 | 550 | 400 | 300 | 270 |
| 100 | 615 | 445 | 335 | 295 |
| 60-mph operating speed | | | | |
| 800 | 230 | 170 | 125 | 115 |
| 600 | 290 | 210 | 160 | 140 |
| 400 | 365 | 270 | 200 | 175 |
| 200 | 450 | 330 | 250 | 215 |
| 100 | 505 | 370 | 275 | 240 |

Source: *A Policy on Geometric Design of Highway and Streets, AASHTO, 2012.*

Existing Intersection Operation / Levels of Service. Existing study area intersection operations are summarized in Table 7. As shown, study area intersections currently operate within acceptable standards. Satisfactory level of service “A” to “C” operations are currently experienced at each of the study intersections in the a.m. and p.m. peak traffic hours. These calculations consider the peak hour percentage of truck traffic at each approach to the intersections.

Existing peak hour volumes at the side street stop sign controlled study intersections do not warrant installation of a traffic signal. Existing side street volumes are below the minimum volume threshold required to meet the peak hour signal warrant.

The a.m. peak hour volumes at the Keyes Road / Bystrum Road intersection meet the AASHTO guideline criteria for consideration of left turn channelization. However, this threshold is only met for the one morning hour and review of hourly roadway volumes throughout the balance of the day indicates that these threshold volumes would not be met during any other hours of the day.

**TABLE 7
EXISTING INTERSECTION LEVELS OF SERVICE**

| Location | Control | AM Peak Hour | | PM Peak Hour | |
|-------------------------------|---------|--------------|---------------|--------------|---------------|
| | | LOS | Average Delay | LOS | Average Delay |
| Keyes Road / Bystrum Road | NB Stop | | | | |
| NB Approach | | B | 12.6 | B | 13.5 |
| WB Approach | | A | 1.5 | A | 0.5 |
| Keyes Road / East Access Road | NB Stop | | | | |
| NB Approach | | C | 15.7 | B | 14.2 |
| WB Approach | | A | 0.0 | A | 0.2 |

PROJECT IMPACTS

To evaluate the impacts of the proposed project on traffic conditions in the study area it is necessary to identify the volume of traffic expected to be generated by the proposed facility and to superimpose this traffic onto current background traffic conditions.

Project Characteristics

Project Employee Traffic. The winery operation currently has 486 in season employees, 389 of which are employed in wholesale / production and operate under three shifts. Build out of the proposed facility expansion is projected to result in modest employee increases, with total employees increasing to 516 persons, an increase of 30 employees or 6%. As occurs today, employees will access the facility via the Bystrum Road entrance.

To quantify this employee increase in terms of traffic volumes, automobile traffic at the Bystrum Road / Keyes Road intersection associated with the existing winery operations has been increased by this same 6%. Inbound and outbound patterns, as well as the directional distribution of employee trips has been assumed to be the same as existing employee traffic. Table 8 displays this employee trip generation information. Existing employee traffic volumes are based upon gate counts at the winery main access. As shown in Table 8, an additional 60 daily employee trips are projected to be generated by the site with the proposed project. Figure 5 displays peak hour and daily employee generated traffic volumes projected to be added to the study street system.

Project Truck Traffic. The proposed project will generate additional truck traffic. In season truck traffic generated by the site consists of shipping trucks, tanker trucks, grape trucks, pomace trucks and various delivery trucks. This is as previously presented in Table 2. Shipping, tanker, pomace and delivery trucks utilize the Bystrum Road main access, while grape trucks utilize the easterly access during the season. Build out of the project is projected to result in an increase in shipping truck traffic, while other truck traffic is projected to remain at existing levels. An additional 25 shipping trucks are projected to exit and enter the site with build out of the proposed expansion project. The additional truck traffic is expected to have similar travel patterns to existing truck traffic generated by the site, with regards to both the distribution of traffic to Keyes Road as well as arrival and departure times to and from the site. Truck traffic volumes are summarized in Table 9. As shown, an additional 50 daily truck trips are projected to be generated by the site with the proposed project. Figure 6 displays peak hour and daily truck traffic volumes projected to be added to the study street system with the proposed project.

**TABLE 8
EXISTING PLUS PROJECT
EMPLOYEE TRIP GENERATION (IN SEASON)**

| Existing Conditions | | | Existing Plus Project Buildout | | | | Net Increase | | | | |
|--------------------------------|-------------------------------------|-----|--------------------------------|--------------------------------|----------------------|-----|--------------|---------------------|----|-----|-------|
| Number of Employees (3 shifts) | Daily Employee Trips ⁽¹⁾ | | | Number of Employees (3 shifts) | Daily Employee Trips | | | Number of Employees | In | Out | Total |
| | In | Out | Total | | In | Out | Total | | | | |
| 486 | 427 | 549 | 976 | 516 | 453 | 583 | 1,036 | 30 | 26 | 34 | 60 |

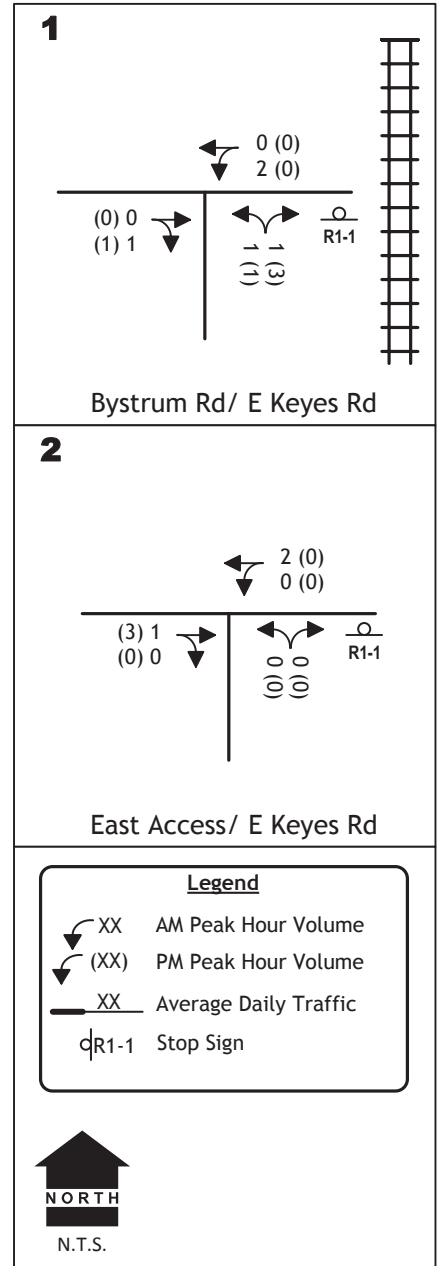
⁽¹⁾ Employee Trip Gate Count, 10/4/16.

**TABLE 9
EXISTING PLUS PROJECT TRIP GENERATION
BRONCO WINERY AVERAGE DAILY TRUCK TRAFFIC VOLUMES (IN SEASON)**

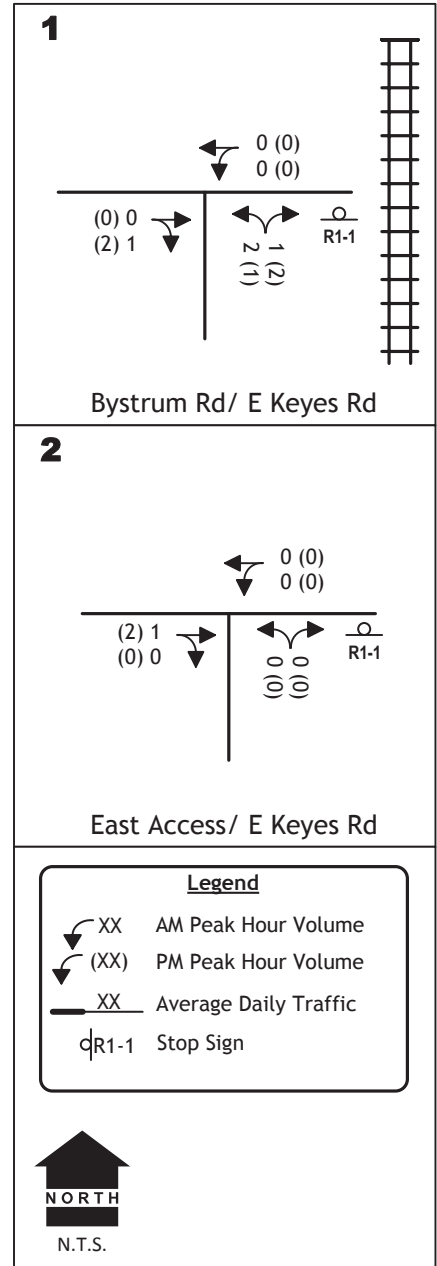
| Distribution Type | Existing Baseline Conditions | | | | With Project Buildout | | | | Net Increase Total Trips |
|------------------------------|------------------------------|-------------|-----|-------|-----------------------|-------------|-----|-------|--------------------------|
| | Number Trucks | Daily Trips | | | Number Trucks | Daily Trips | | | |
| | | In | Out | Total | | In | Out | Total | |
| Shipping Trucks | 14 | 14 | 14 | 28 | 39 | 39 | 39 | 78 | 50 |
| Tanker Trucks | 34 | 34 | 34 | 68 | 34 | 34 | 34 | 68 | 0 |
| Grape Trucks ⁽¹⁾ | 105 | 105 | 105 | 210 | 105 | 105 | 105 | 210 | 0 |
| Pomace Trucks ⁽¹⁾ | 23 | 23 | 23 | 46 | 23 | 23 | 23 | 46 | 0 |
| Delivery Trucks | 7 | 7 | 7 | 14 | 7 | 7 | 7 | 14 | 0 |
| | 183 | | | 366 | 208 | | | 416 | 50 |

⁽¹⁾ Grape and Pomace trucks only operate in season during grape harvest and crush.

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PROJECT ONLY AUTOMOBILE
TRAFFIC VOLUMES AND LANE CONFIGURATIONS



Rail Car Traffic. Construction of the proposed railroad spur lines will permit shipping of product via rail. Rail car volume is projected at five cars per day, four days a week, resulting in one train trip to and from the site four days a week. Rail cars will be delivered and picked up as part of the existing train schedule serving this rail route. This is summarized in Table 10.

**TABLE 10
PROPOSED RAIL SERVICES**

| | |
|---------------------------------------|---|
| Number of Trains serving site per day | 1 |
| Number of Rail Cars per Train | 5 |
| Projected Train Service days per week | 4 |

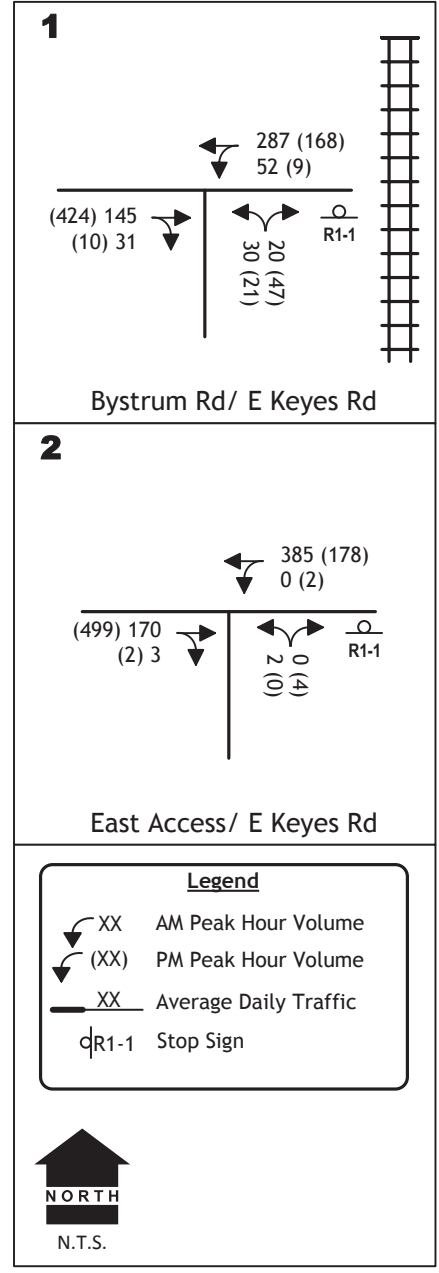
Existing Plus Project Traffic Volumes and Levels of Service

Figure 7 displays resulting “Existing Plus Project” traffic volumes with project traffic added to existing background baseline traffic volumes. Projected intersection Levels of Service are presented in Table 11.

As shown in Table 11, traffic generated by build out of the proposed winery expansion project will have a very minor effect on current intersection operations. No changes to current operating levels of service are projected and any increases in delay are projected to be very minor. Satisfactory operating levels of service are projected to continue. The minor increases in peak hour traffic will not warrant signalization of the study intersections. Similarly, project traffic will not measurably effect the need for left turn channelization at the Keyes Road / Bystrum Road intersection. As such, while no significant project impacts have been identified, to respond to existing concerns the Keyes Road / Bystrum Road intersection will be improved as part of the proposed project.

**TABLE 11
INTERSECTION LEVELS OF SERVICE
EXISTING PLUS PROJECT BUILD OUT**

| Intersection | | Existing | | | | Existing Plus Project | | | | Net Changes/Increase | | | |
|--|-------------|--------------|-------|--------------|-------|-----------------------|-------|--------------|-------|----------------------|-------|--------------|-------|
| | | AM Peak Hour | | PM Peak Hour | | AM Peak Hour | | PM Peak Hour | | AM Peak Hour | | PM Peak Hour | |
| Location | Control | LOS | Delay | LOS | Delay | LOS | Delay | LOS | Delay | LOS | Delay | LOS | Delay |
| Keyes Rd / Bystrum Rd | NB | | | | | | | | | | | | |
| | Stop | B | 12.6 | B | 13.5 | B | 12.8 | B | 13.7 | - | 0.2 | - | 0.2 |
| | WB Approach | A | 1.5 | A | 0.5 | A | 1.5 | A | 0.5 | - | 0.0 | - | 0.0 |
| Keyes Rd / East Access Rd | NB | | | | | | | | | | | | |
| | Stop | C | 15.7 | B | 14.2 | C | 15.7 | B | 14.2 | - | 0.0 | - | 0.0 |
| | WB Approach | A | 0.0 | A | 0.2 | A | 0.0 | A | 0.2 | - | 0.0 | - | 0.0 |
| LOS = Level of Service Delay = Average Delay in seconds | | | | | | | | | | | | | |



EXISTING BASELINE PLUS PROJECT BUILDOUT
TRAFFIC VOLUMES AND LANE CONFIGURATIONS

Railroad Spur Line Operation

The proposed railroad spur line will be located immediately east of the existing U.P.R.R. line along the west border of the site and will extend for approximately 1,400' between Keyes Road and the winery main entrance to the south. Figure 8 displays the proposed design. Two parallel spur lines will be constructed to facilitate train car pick-up and drop off and to minimize potential delays to traffic on Keyes Road. Projected operations are as follows: 1) The train will originate from the north and pass the southerly spur switch, 2) Train will back into the easterly spur to pick up loaded cars and transfer them to the westerly spur, 3) Train then moves back to the easterly spur to drop off empty cars, 4) Train proceeds south to Turlock where it turns around and then proceeds north picking up loaded cars along the service route, 5) At the winery, the train will back into the westerly spur line at the northerly spur switch to pick up loaded cars, then proceeds north.

With respect to current train activity, the addition of the winery spur lines will not significantly increase delays to Keyes Road for the southbound train trip. The train crossing duration may increase somewhat, as the train will be slowing in order to stop and back-up into the southerly spur line switch south of Keyes Road. As previously discussed, this existing southbound crossing time was observed to be 40 seconds and this would be expected to increase somewhat due to the train slowing as it crosses Keyes Road.

The northbound trip for picking up loaded cars will require the train to stop just north of Keyes Road and then back into the westerly spur line at the northerly spur switch, pick up loaded cars, and then proceed north. The time to cross Keyes Road and perform this maneuver is estimated at four (4) to eight (8) minutes by U.P.R.R. personnel. As previously discussed, the current northbound train crossing duration was observed at 54 seconds, and this resulted in observed vehicle queues at the crossing on eastbound and westbound Keyes Road of two (2) vehicles and four (4) vehicles, respectively. Extrapolating this information out for the additional three minutes of delay associated with the shorter duration estimate, it would be expected that vehicle queues of 8 vehicles and 16 vehicles would form on eastbound and westbound Keyes Road, respectively. For the longer eight minute duration estimate, vehicle queues of 16 and 32 vehicles would be expected on eastbound and westbound Keyes Road. This information is summarized in Table 12. Following departure of the train, it is estimated that a 32 vehicle queue would require another 70 seconds to disperse.

Observations of the northbound train crossing indicated that the northbound crossing occurred at 10:15 a.m. and that this is roughly typical of the train schedule serving the area. Vehicle queue estimates identified above assume a similar train crossing schedule, with northbound trains crossing Keyes Road during the late morning hours.

The sensitivity of the train schedule on vehicle queue estimates for Keyes Road has been evaluated based upon hourly counts conducted for the roadway. Review of 24 hour traffic counts indicates volumes on westbound Keyes Road are fairly consistent for the hours from 10:00 a.m. to about 1:00 p.m. As such, a train picking up product from the winery during this three hour period would be expected to cause vehicle queues on westbound Keyes Road as discussed above.

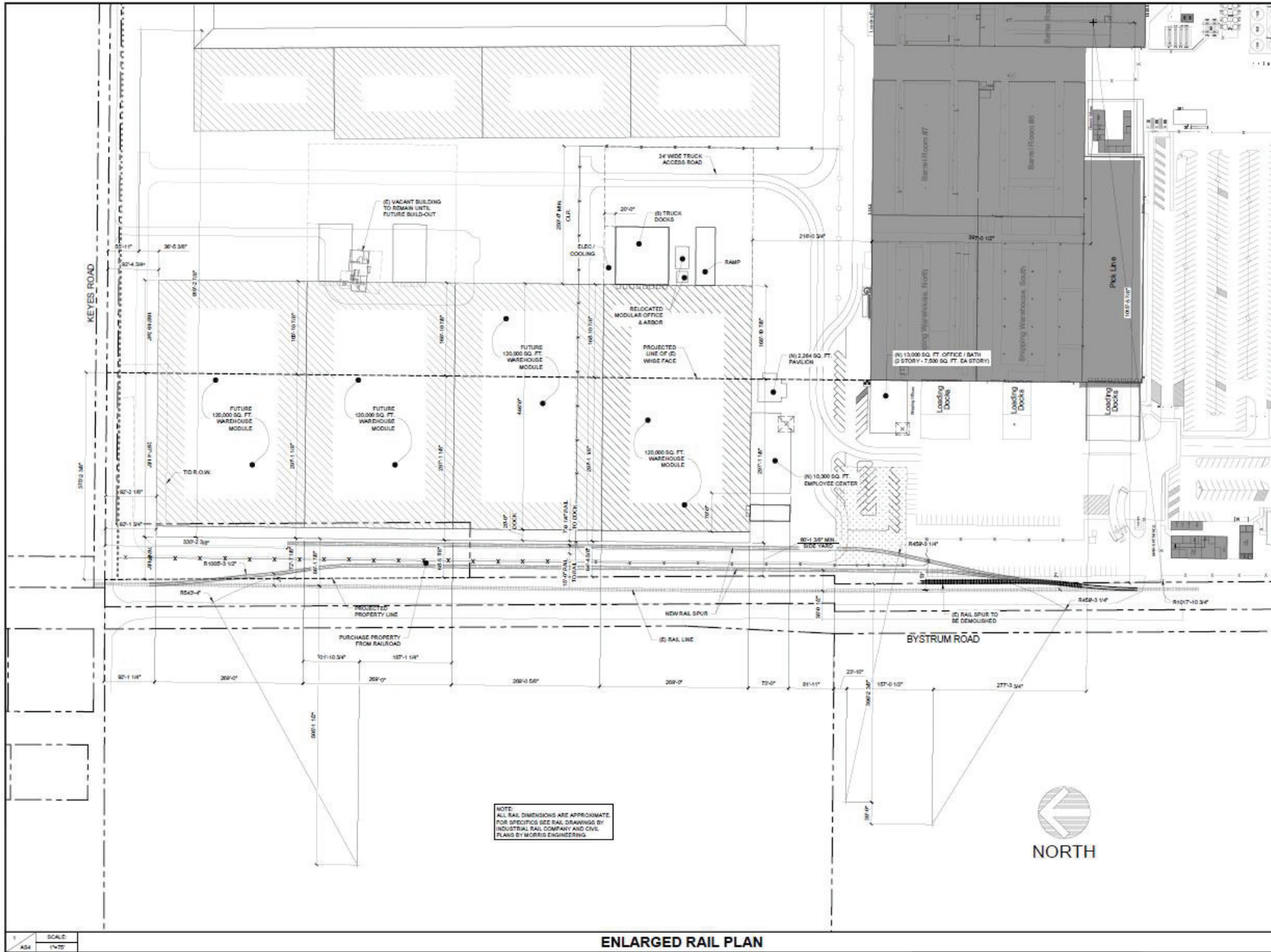
Conversely, traffic on eastbound Keyes Road is lower in the morning and steadily increases over these hours, surpassing the westbound flow rate after about 1:00 p.m. Therefore, the 8 to 16 vehicle eastbound queue estimated for the 10:00 hour would be expected to increase and reach 16 to 32 vehicles by 1:00 p.m., similar to the westbound direction.

As noted in the project application, the statement has been made that *the train engineers will be as courteous as possible, by pulling clear of Keyes Road throughout this process if any large backups are seen.* Should this occur, it is likely that the loaded car pickup could be accomplished in two steps, with the northbound train first clearing Keyes Road and permitting traffic to clear, then followed by backing across Keyes Road to secure the loaded cars and then proceeding north.

**TABLE 12
PROJECTED VEHICLE QUEUES AT KEYES ROAD TRAIN CROSSING**

| Direction | Vehicle Queue (# cars) | | |
|----------------------|--|--|-------------------|
| | Existing Conditions 10/4/16 observation NB Train | With Proposed Project, Product Pick-up, NB Train | |
| | | 4 Minute Duration | 8 Minute Duration |
| Eastbound Keyes Road | 2 | 8 | 16 |
| Westbound Keyes Road | 4 | 16 | 32 |

Rail Car Equivalent Truck Traffic. As previously discussed, the proposed project includes shipping of product both by truck and rail car. Rail shipments are estimated at five (5) cars per day, 4 days per week. The equivalent truck traffic volume is discussed here, should rail shipments not be available. Information provided by the applicant indicates that the shipping capacity of one rail car is equivalent to three (3) trucks. As such, five rail cars per day would be the equivalent of 15 trucks, or 30 truck trips per day to and from the site. As presented in this analysis, with the inclusion of rail service, the proposed project is expected to result in an additional 25 trucks serving the site or 50 daily truck trips generated by the site. Therefore, in the absence of rail service, the additional truck traffic would increase by approximately 60%. Associated traffic impacts would be expected to be proportionately less than that identified for the proposed project. As the traffic impacts associated with the proposed project have been projected to be relatively minor, this additional truck traffic would not be expected to have a significant impact.



ENLARGED RAIL PLAN

| DATE | NO. | REVISIONS | BY |
|---------|-----|------------------------------|----|
| 7/17/15 | | PRELIM. CLIENT REVIEW | CB |
| 8/22/15 | | PRELIM. WAREHOUSE FLOOR PLAN | CB |

ENGINEERING INCORPORATED
STRUCTURAL DESIGN AND CIVIL ENGINEERING
400 Downey Ave. Modesto, CA 95354
Tel: 209.486.8000 Fax: 209.482.0060
Email: CBE@CBCEngineering.com

ANDERSON-LTRIN INC.
Professional Engineer
Phone: 209.643.4111 • Fax: 209.643.5267
1285 Veterans Circle • PO. Box 1328 • Turlock, CA 95381

REGISTERED PROFESSIONAL ENGINEER
C. A. R. BALLINGER
No. 42718
CIVIL
STATE OF CALIFORNIA
DATE: 2/22/11

ENLARGED RAIL PLAN
CENTRAL VALLEY DISTRIBUTION CENTER
FOR: BRONCO WINERY
800 KEYES ROAD
CERES, CA 95307

JOB #: 4129-4-15
DATE: 7/14/15
SCALE: SHOWN
APPROVED: CB
DRAWN: HJ
SHEET:

AS4

SUMMARY AND CONCLUSIONS

This report summarizes analysis of the traffic impacts associated with development of the proposed Bronco Winery Distribution Center. The project consists of development of additional warehouse and building space at the existing winery facility located south of Keyes Road and east of Bystrum Road. The project includes development of a rail spur connection to permit shipment of product by rail as well as by truck. The analysis focuses on impacts to the Keyes Road / Bystrum Road intersection immediately adjacent to the U.P.R.R. crossing of Keyes Road. The main access to the existing winery is located on Bystrum Road approximately 2,000 feet south of Keyes Road. A second project access is located on Keyes Road 3/4 of a mile east of Bystrum Road.

Traffic operations have been quantified relative to "in season" conditions which include trucking and employee operations associated with grape harvest and crush in addition to typical shipping and receiving winery operations. Additionally, although the proposed winery expansion would be realized over a number of years, this study assumes build out of the proposed project to quantify resulting "Existing plus Project" traffic operating conditions.

Project Description

Build out of the entire project will include construction of eight warehouses totaling 629,500 sf, 4 office buildings totaling 101,000 sf and 2 assembly buildings totaling 12,600 sf immediately north of the company's existing winery facilities. The proposed Phase 1 portion of the project will consist of one 120,000 sf warehouse building and the railroad spur lines. The railroad spur lines will extend for approximately 1,400' immediately east of the existing U.P.R.R. line between Keyes Road and the project main access. The two existing access gates will continue to serve the expanded project site.

Project proponents expect that the project will not increase the capacity of the site for wine making. As a result, the project is not expected to see an increase in the number of trucks bringing grapes to the site during Crush. However, the project involves creation of an appreciable amount of wine storage to accommodate wine produced on-site or to accommodate wine created or bottled elsewhere and trucked to this site for bottling and/or storage prior to eventual shipment. As such, the project would involve some additional employee trips to and from the site by automobile, as well as wine deliveries and shipments by truck and rail.

Existing Traffic Conditions

To determine existing traffic volumes and obtain more information about traffic conditions in the study area, information regarding daily, a.m. and p.m. peak hour traffic volumes was assembled. New weekday intersection and roadway counts were conducted on October 4, 2016. Intersection counts were performed from 7:00 - 9:00 a.m. and 4:00 - 6:00 p.m. at the two study intersections. Daily 24 hour roadway counts were also conducted on four roadway segments. These included:

- Keyes Road west of Bystrum Road
- Keyes Road east of the East Truck Access to Bronco Winery
- Bystrum Road south of Keyes Road
- East Truck Access road south of Keyes Road

All intersection and roadway counts were conducted in 15 minute increments and included separate truck classification counts.

The study area intersections currently operate within acceptable standards. Satisfactory level of service “A” to “C” operations are currently experienced at each of the study intersections in the a.m. and p.m. peak traffic hours. These calculations consider the peak hour percentage of truck traffic at each approach to the intersections.

Existing peak hour volumes at the side street stop sign controlled study intersections do not warrant installation of a traffic signal. Existing side street volumes are below the minimum volume threshold required to meet the peak hour signal warrant. The a.m. peak hour volumes at the Keyes Road / Bystrum Road intersection meet the AASHTO guideline criteria for consideration of left turn channelization. However, this threshold is only met for the one morning hour and review of hourly roadway volumes throughout the balance of the day indicates that these threshold volumes would not be met during any other hours of the day.

Project Characteristics

Project Employee Traffic. The winery operation currently has 486 in season employees, 389 of which are employed in wholesale / production and operate under three shifts. Build out of the proposed facility expansion is projected to result in modest employee increases, with total employees increasing to 516 persons, an increase of 30 employees or 6%. As occurs today, employees will access the facility via the Bystrum Road entrance.

To quantify this employee increase in terms of traffic volumes, automobile traffic at the Bystrum Road / Keyes Road intersection associated with the existing winery operations has been increased by this same 6%. Inbound and outbound patterns, as well as the directional distribution of employee trips has been assumed to be the same as existing employee traffic. Table 8 displays this employee trip generation information. Existing employee traffic volumes are based upon gate counts at the winery main access. An additional 60 daily employee trips are projected to be generated by the site with the proposed project.

Project Truck Traffic. The proposed project will generate additional truck traffic. In season truck traffic generated by the site consists of shipping trucks, tanker trucks, grape trucks, pomace trucks and various delivery trucks. Shipping, tanker, pomace and delivery trucks utilize the Bystrum Road main access, while grape trucks utilize the easterly access during the season. Development of the project is projected to result in an increase in shipping truck traffic, while other truck traffic is projected to remain at existing levels. An additional 25 shipping trucks are projected to exit and enter the site with build out of the proposed expansion project. The

additional truck traffic is expected to have similar travel patterns to existing truck traffic generated by the site, with regards to both the distribution of traffic to Keyes Road as well as arrival and departure times to and from the site. An additional 50 daily truck trips are projected to be generated by the site with the proposed project.

Rail Car Traffic. Construction of the proposed railroad spur lines will permit shipping of product via rail. Rail car volume is projected at five cars per day, four days a week, resulting in one train trip to and from the site four days a week. Rail cars will be delivered and picked up as part of the existing train schedule serving this rail route.

Existing Plus Project Traffic Volumes and Levels of Service

Traffic generated by build out of the proposed winery expansion project will have a very minor effect on current intersection operations. No changes to current operating levels of service are projected and any increases in delay are projected to be very minor. Satisfactory operating levels of service are projected to continue. The minor increases in peak hour traffic will not warrant signalization of the study intersections. While project traffic will not measurably effect the need for left turn channelization at the Keyes Road / Bystrum Road intersection, to address current concerns the intersection will be improved as part of the project. This considers build out of the expansion project. As previously noted, the initial Phase 1 project consists of one 120,000 sf warehouse building.

Railroad Spur Line Operation

The proposed railroad spur line will be located immediately east of the existing U.P.R.R. line along the west border of the site and will extend for approximately 1,400' between Keyes Road and the winery main entrance to the south. Two parallel spur lines will be constructed to facilitate train car pick-up and drop off and to minimize potential delays to traffic on Keyes Road. Projected operations are as follows: 1) The train will originate from the north and pass the southerly spur switch, 2) Train will back into the easterly spur to pick up loaded cars and transfer them to the westerly spur, 3) Train then moves back to the easterly spur to drop off empty cars, 4) Train proceeds south to Turlock were it turns around and then proceeds north picking up loaded cars along the service route, 5) At the winery, the train will back into the westerly spur line at the northerly spur switch to pick up loaded cars, then proceeds north.

With respect to current train activity, the addition of the winery spur lines will not significantly increase delays to Keyes Road for the southbound train trip. The train crossing duration may increase somewhat, as the train will be slowing in order to stop and back-up into the southerly spur line switch south of Keyes Road. As previously discussed, this existing southbound crossing time was observed to be 40 seconds and this would be expected to increase somewhat due to the train slowing as it crosses Keyes Road.

The northbound trip for picking up loaded cars will require the train to stop just north of Keyes Road and then back into the westerly spur line at the northerly spur switch, pick up loaded cars, and then proceed north. The time to cross Keyes Road and perform this maneuver is estimated at four (4) to eight (8) minutes by U.P.R.R. personnel. The current northbound train crossing

duration was observed at 54 seconds, and this resulted in observed vehicle queues at the crossing on eastbound and westbound Keyes Road of two (2) vehicles and four (4) vehicles, respectively. Extrapolating this information out for the additional three minutes of delay associated with the shorter duration estimate, it would be expected that vehicle queues of 8 vehicles and 16 vehicles would form on eastbound and westbound Keyes Road, respectively. For the longer eight minute duration estimate, vehicle queues of 16 and 32 vehicles would be expected on eastbound and westbound Keyes Road. Following departure of the train, it is estimated that a 32 vehicle queue would require another 70 seconds to disperse.

As noted in the project application, the statement has been made that *the train engineers will be as courteous as possible, by pulling clear of Keyes Road throughout this process if any large backups are seen.* Should this occur, it is likely that the loaded car pickup could be accomplished in two steps, with the northbound train first clearing Keyes Road and permitting traffic to clear, then followed by backing across Keyes Road to secure the loaded cars and then proceeding north.

APPENDIX

**EXISTING
LEVEL OF SERVICE**

**EXISTING PLUS PROJECT
LEVELS OF SERVICE**

TRAFFIC COUNTS

HCM Unsignalized Intersection Capacity Analysis
 3: Keyes Rd & Bystrum

Ex AM
 11/4/2016

| | → | ↘ | ↙ | ← | ↖ | ↗ |
|-----------------------------------|------|-------|------|----------------------|------|------|
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↘ | | | ↖ | ↗ | |
| Sign Control | Free | | | Free | Stop | |
| Grade | 0% | | | 0% | 0% | |
| Volume (veh/h) | 145 | 29 | 50 | 287 | 27 | 18 |
| Peak Hour Factor | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 |
| Hourly flow rate (vph) | 156 | 31 | 54 | 309 | 29 | 19 |
| Pedestrians | | | | | | |
| Lane Width (ft) | | | | | | |
| Walking Speed (ft/s) | | | | | | |
| Percent Blockage | | | | | | |
| Right turn flare (veh) | | | | | | |
| Median type | | | | | None | |
| Median storage veh | | | | | | |
| Upstream signal (ft) | | | | | | |
| pX, platoon unblocked | | | | | | |
| vC, conflicting volume | | | 187 | | 588 | 172 |
| vC1, stage 1 conf vol | | | | | | |
| vC2, stage 2 conf vol | | | | | | |
| vCu, unblocked vol | | | 187 | | 588 | 172 |
| tC, single (s) | | | 4.1 | | 6.6 | 6.5 |
| tC, 2 stage (s) | | | | | | |
| tF (s) | | | 2.2 | | 3.7 | 3.6 |
| p0 queue free % | | | 96 | | 93 | 98 |
| cM capacity (veh/h) | | | 1387 | | 423 | 798 |
| Direction, Lane # | EB 1 | WB 1 | NB 1 | | | |
| Volume Total | 187 | 362 | 48 | | | |
| Volume Left | 0 | 54 | 29 | | | |
| Volume Right | 31 | 0 | 19 | | | |
| cSH | 1700 | 1387 | 521 | | | |
| Volume to Capacity | 0.11 | 0.04 | 0.09 | | | |
| Queue Length 95th (ft) | 0 | 3 | 8 | | | |
| Control Delay (s) | 0.0 | 1.5 | 12.6 | | | |
| Lane LOS | | A | B | | | |
| Approach Delay (s) | 0.0 | 1.5 | 12.6 | | | |
| Approach LOS | | | B | | | |
| Intersection Summary | | | | | | |
| Average Delay | | | 1.9 | | | |
| Intersection Capacity Utilization | | 40.6% | | ICU Level of Service | | A |
| Analysis Period (min) | | | 15 | | | |

HCM Unsignalized Intersection Capacity Analysis
 5: Keyes Rd & East Access

Ex AM
 11/4/2016

| | → | ↘ | ↙ | ← | ↖ | ↗ |
|-----------------------------------|------|-------|------|----------------------|------|------|
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↗ | | | ↖ | ↘ | |
| Sign Control | Free | | | Free | Stop | |
| Grade | 0% | | | 0% | 0% | |
| Volume (veh/h) | 168 | 3 | 0 | 383 | 2 | 0 |
| Peak Hour Factor | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 |
| Hourly flow rate (vph) | 181 | 3 | 0 | 412 | 2 | 0 |
| Pedestrians | | | | | | |
| Lane Width (ft) | | | | | | |
| Walking Speed (ft/s) | | | | | | |
| Percent Blockage | | | | | | |
| Right turn flare (veh) | | | | | | |
| Median type | | | | | None | |
| Median storage veh | | | | | | |
| Upstream signal (ft) | | | | | | |
| pX, platoon unblocked | | | | | | |
| vC, conflicting volume | | | 184 | | 594 | 182 |
| vC1, stage 1 conf vol | | | | | | |
| vC2, stage 2 conf vol | | | | | | |
| vCu, unblocked vol | | | 184 | | 594 | 182 |
| tC, single (s) | | | 5.1 | | 7.4 | 7.2 |
| tC, 2 stage (s) | | | | | | |
| tF (s) | | | 3.1 | | 4.4 | 4.2 |
| p0 queue free % | | | 100 | | 99 | 100 |
| cM capacity (veh/h) | | | 968 | | 339 | 661 |
| Direction, Lane # | EB 1 | WB 1 | NB 1 | | | |
| Volume Total | 184 | 412 | 2 | | | |
| Volume Left | 0 | 0 | 2 | | | |
| Volume Right | 3 | 0 | 0 | | | |
| cSH | 1700 | 968 | 339 | | | |
| Volume to Capacity | 0.11 | 0.00 | 0.01 | | | |
| Queue Length 95th (ft) | 0 | 0 | 0 | | | |
| Control Delay (s) | 0.0 | 0.0 | 15.7 | | | |
| Lane LOS | | | C | | | |
| Approach Delay (s) | 0.0 | 0.0 | 15.7 | | | |
| Approach LOS | | | C | | | |
| Intersection Summary | | | | | | |
| Average Delay | | | 0.1 | | | |
| Intersection Capacity Utilization | | 30.2% | | ICU Level of Service | A | |
| Analysis Period (min) | | 15 | | | | |

HCM Unsignalized Intersection Capacity Analysis
 3: Keyes Rd & Bystrum

Ex PM
 11/4/2016

| | → | ↘ | ↙ | ← | ↖ | ↗ |
|-----------------------------------|------|------|-------|----------------------|------|------|
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↶ | | | ↷ | ↶ | ↷ |
| Sign Control | Free | | | Free | Stop | |
| Grade | 0% | | | 0% | 0% | |
| Volume (veh/h) | 424 | 7 | 9 | 168 | 19 | 42 |
| Peak Hour Factor | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 |
| Hourly flow rate (vph) | 456 | 8 | 10 | 181 | 20 | 45 |
| Pedestrians | | | | | | |
| Lane Width (ft) | | | | | | |
| Walking Speed (ft/s) | | | | | | |
| Percent Blockage | | | | | | |
| Right turn flare (veh) | | | | | | |
| Median type | | | | | None | |
| Median storage (veh) | | | | | | |
| Upstream signal (ft) | | | | | | |
| pX, platoon unblocked | | | | | | |
| vC, conflicting volume | | | 463 | | 660 | 460 |
| vC1, stage 1 conf vol | | | | | | |
| vC2, stage 2 conf vol | | | | | | |
| vCu, unblocked vol | | | 463 | | 660 | 460 |
| tC, single (s) | | | 4.2 | | 6.8 | 6.4 |
| tC, 2 stage (s) | | | | | | |
| tF (s) | | | 2.3 | | 3.9 | 3.5 |
| p0 queue free % | | | 99 | | 94 | 92 |
| cM capacity (veh/h) | | | 1052 | | 368 | 572 |
| Direction, Lane # | EB 1 | WB 1 | NB 1 | | | |
| Volume Total | 463 | 190 | 66 | | | |
| Volume Left | 0 | 10 | 20 | | | |
| Volume Right | 8 | 0 | 45 | | | |
| cSH | 1700 | 1052 | 488 | | | |
| Volume to Capacity | 0.27 | 0.01 | 0.13 | | | |
| Queue Length 95th (ft) | 0 | 1 | 12 | | | |
| Control Delay (s) | 0.0 | 0.5 | 13.5 | | | |
| Lane LOS | | A | B | | | |
| Approach Delay (s) | 0.0 | 0.5 | 13.5 | | | |
| Approach LOS | | | B | | | |
| Intersection Summary | | | | | | |
| Average Delay | | | 1.4 | | | |
| Intersection Capacity Utilization | | | 33.0% | ICU Level of Service | | A |
| Analysis Period (min) | | | 15 | | | |

HCM Unsignalized Intersection Capacity Analysis
 5: Keyes Rd & East Access

Ex PM
 11/4/2016

| | → | ↘ | ↙ | ← | ↖ | ↗ |
|-----------------------------------|------|------|-------|----------------------|------|------|
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↗ | | | ↖ | ↗ | |
| Sign Control | Free | | | Free | Stop | |
| Grade | 0% | | | 0% | 0% | |
| Volume (veh/h) | 494 | 2 | 2 | 178 | 0 | 4 |
| Peak Hour Factor | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 |
| Hourly flow rate (vph) | 531 | 2 | 2 | 191 | 0 | 4 |
| Pedestrians | | | | | | |
| Lane Width (ft) | | | | | | |
| Walking Speed (ft/s) | | | | | | |
| Percent Blockage | | | | | | |
| Right turn flare (veh) | | | | | | |
| Median type | | | | | None | |
| Median storage (veh) | | | | | | |
| Upstream signal (ft) | | | | | | |
| pX, platoon unblocked | | | | | | |
| vC, conflicting volume | | | 533 | | 728 | 532 |
| vC1, stage 1 conf vol | | | | | | |
| vC2, stage 2 conf vol | | | | | | |
| vCu, unblocked vol | | | 533 | | 728 | 532 |
| tC, single (s) | | | 5.1 | | 7.4 | 7.2 |
| tC, 2 stage (s) | | | | | | |
| tF (s) | | | 3.1 | | 4.4 | 4.2 |
| p0 queue free % | | | 100 | | 100 | 99 |
| cM capacity (veh/h) | | | 680 | | 276 | 397 |
| Direction, Lane # | EB 1 | WB 1 | NB 1 | | | |
| Volume Total | 533 | 194 | 4 | | | |
| Volume Left | 0 | 2 | 0 | | | |
| Volume Right | 2 | 0 | 4 | | | |
| cSH | 1700 | 680 | 397 | | | |
| Volume to Capacity | 0.31 | 0.00 | 0.01 | | | |
| Queue Length 95th (ft) | 0 | 0 | 1 | | | |
| Control Delay (s) | 0.0 | 0.2 | 14.2 | | | |
| Lane LOS | | A | B | | | |
| Approach Delay (s) | 0.0 | 0.2 | 14.2 | | | |
| Approach LOS | | | B | | | |
| Intersection Summary | | | | | | |
| Average Delay | | | 0.1 | | | |
| Intersection Capacity Utilization | | | 36.1% | ICU Level of Service | | A |
| Analysis Period (min) | | | 15 | | | |

HCM Unsignalized Intersection Capacity Analysis
 3: Keyes Rd & Bystrum

Ex AM + Project buildout
 11/4/2016

| | → | ↘ | ↙ | ← | ↖ | ↗ |
|-----------------------------------|------|------|-------|----------------------|------|------|
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↗ | | | ↖ | ↗ | |
| Sign Control | Free | | | Free | Stop | |
| Grade | 0% | | | 0% | 0% | |
| Volume (veh/h) | 145 | 31 | 52 | 287 | 30 | 20 |
| Peak Hour Factor | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 |
| Hourly flow rate (vph) | 156 | 33 | 56 | 309 | 32 | 22 |
| Pedestrians | | | | | | |
| Lane Width (ft) | | | | | | |
| Walking Speed (ft/s) | | | | | | |
| Percent Blockage | | | | | | |
| Right turn flare (veh) | | | | | | |
| Median type | | | | | None | |
| Median storage (veh) | | | | | | |
| Upstream signal (ft) | | | | | | |
| pX, platoon unblocked | | | | | | |
| vC, conflicting volume | | | 189 | | 593 | 173 |
| vC1, stage 1 conf vol | | | | | | |
| vC2, stage 2 conf vol | | | | | | |
| vCu, unblocked vol | | | 189 | | 593 | 173 |
| tC, single (s) | | | 4.1 | | 6.6 | 6.5 |
| tC, 2 stage (s) | | | | | | |
| tF (s) | | | 2.2 | | 3.7 | 3.6 |
| p0 queue free % | | | 96 | | 92 | 97 |
| cM capacity (veh/h) | | | 1385 | | 419 | 797 |
| Direction, Lane # | EB 1 | WB 1 | NB 1 | | | |
| Volume Total | 189 | 365 | 54 | | | |
| Volume Left | 0 | 56 | 32 | | | |
| Volume Right | 33 | 0 | 22 | | | |
| cSH | 1700 | 1385 | 517 | | | |
| Volume to Capacity | 0.11 | 0.04 | 0.10 | | | |
| Queue Length 95th (ft) | 0 | 3 | 9 | | | |
| Control Delay (s) | 0.0 | 1.5 | 12.8 | | | |
| Lane LOS | | A | B | | | |
| Approach Delay (s) | 0.0 | 1.5 | 12.8 | | | |
| Approach LOS | | | B | | | |
| Intersection Summary | | | | | | |
| Average Delay | | | 2.0 | | | |
| Intersection Capacity Utilization | | | 40.8% | ICU Level of Service | | A |
| Analysis Period (min) | | | 15 | | | |

HCM Unsignalized Intersection Capacity Analysis
 5: Keyes Rd & East Access

Ex AM + Project buildout
 11/4/2016

| | → | ↘ | ↙ | ← | ↖ | ↗ |
|-----------------------------------|------|------|-------|----------------------|------|------|
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↑ | | | ↑ | ↑ | |
| Sign Control | Free | | | Free | Stop | |
| Grade | 0% | | | 0% | 0% | |
| Volume (veh/h) | 170 | 3 | 0 | 385 | 2 | 0 |
| Peak Hour Factor | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 |
| Hourly flow rate (vph) | 183 | 3 | 0 | 414 | 2 | 0 |
| Pedestrians | | | | | | |
| Lane Width (ft) | | | | | | |
| Walking Speed (ft/s) | | | | | | |
| Percent Blockage | | | | | | |
| Right turn flare (veh) | | | | | | |
| Median type | | | | | None | |
| Median storage veh | | | | | | |
| Upstream signal (ft) | | | | | | |
| pX, platoon unblocked | | | | | | |
| vC, conflicting volume | | | 186 | | 598 | 184 |
| vC1, stage 1 conf vol | | | | | | |
| vC2, stage 2 conf vol | | | | | | |
| vCu, unblocked vol | | | 186 | | 598 | 184 |
| tC, single (s) | | | 5.1 | | 7.4 | 7.2 |
| tC, 2 stage (s) | | | | | | |
| tF (s) | | | 3.1 | | 4.4 | 4.2 |
| p0 queue free % | | | 100 | | 99 | 100 |
| cM capacity (veh/h) | | | 966 | | 337 | 659 |
| Direction, Lane # | EB 1 | WB 1 | NB 1 | | | |
| Volume Total | 186 | 414 | 2 | | | |
| Volume Left | 0 | 0 | 2 | | | |
| Volume Right | 3 | 0 | 0 | | | |
| cSH | 1700 | 966 | 337 | | | |
| Volume to Capacity | 0.11 | 0.00 | 0.01 | | | |
| Queue Length 95th (ft) | 0 | 0 | 0 | | | |
| Control Delay (s) | 0.0 | 0.0 | 15.7 | | | |
| Lane LOS | | | C | | | |
| Approach Delay (s) | 0.0 | 0.0 | 15.7 | | | |
| Approach LOS | | | C | | | |
| Intersection Summary | | | | | | |
| Average Delay | | | 0.1 | | | |
| Intersection Capacity Utilization | | | 30.3% | ICU Level of Service | | A |
| Analysis Period (min) | | | 15 | | | |

HCM Unsignalized Intersection Capacity Analysis
 3: Keyes Rd & Bystrum

Ex PM + Project buildout
 11/4/2016

| | → | ↘ | ↙ | ← | ↖ | ↗ |
|-----------------------------------|------|-------|------|----------------------|------|------|
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↖ | | | ↖ | ↗ | |
| Sign Control | Free | | | Free | Stop | |
| Grade | 0% | | | 0% | 0% | |
| Volume (veh/h) | 424 | 10 | 9 | 168 | 21 | 47 |
| Peak Hour Factor | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 |
| Hourly flow rate (vph) | 456 | 11 | 10 | 181 | 23 | 51 |
| Pedestrians | | | | | | |
| Lane Width (ft) | | | | | | |
| Walking Speed (ft/s) | | | | | | |
| Percent Blockage | | | | | | |
| Right turn flare (veh) | | | | | | |
| Median type | | | | | None | |
| Median storage veh | | | | | | |
| Upstream signal (ft) | | | | | | |
| pX, platoon unblocked | | | | | | |
| vC, conflicting volume | | | 467 | | 661 | 461 |
| vC1, stage 1 conf vol | | | | | | |
| vC2, stage 2 conf vol | | | | | | |
| vCu, unblocked vol | | | 467 | | 661 | 461 |
| tC, single (s) | | | 4.2 | | 6.8 | 6.4 |
| tC, 2 stage (s) | | | | | | |
| tF (s) | | | 2.3 | | 3.9 | 3.5 |
| p0 queue free % | | | 99 | | 94 | 91 |
| cM capacity (veh/h) | | | 1049 | | 367 | 570 |
| Direction, Lane # | EB 1 | WB 1 | NB 1 | | | |
| Volume Total | 467 | 190 | 73 | | | |
| Volume Left | 0 | 10 | 23 | | | |
| Volume Right | 11 | 0 | 51 | | | |
| cSH | 1700 | 1049 | 487 | | | |
| Volume to Capacity | 0.27 | 0.01 | 0.15 | | | |
| Queue Length 95th (ft) | 0 | 1 | 13 | | | |
| Control Delay (s) | 0.0 | 0.5 | 13.7 | | | |
| Lane LOS | | A | B | | | |
| Approach Delay (s) | 0.0 | 0.5 | 13.7 | | | |
| Approach LOS | | | B | | | |
| Intersection Summary | | | | | | |
| Average Delay | | | 1.5 | | | |
| Intersection Capacity Utilization | | 33.6% | | ICU Level of Service | A | |
| Analysis Period (min) | | 15 | | | | |

HCM Unsignalized Intersection Capacity Analysis
 5: Keyes Rd & East Access

Ex PM + Project buildout
 11/4/2016

| | → | ↘ | ↙ | ← | ↖ | ↗ |
|-----------------------------------|------|------|-------|----------------------|------|------|
| Movement | EBT | EBR | WBL | WBT | NBL | NBR |
| Lane Configurations | ↗ | | | ↖ | ↘ | |
| Sign Control | Free | | | Free | Stop | |
| Grade | 0% | | | 0% | 0% | |
| Volume (veh/h) | 499 | 2 | 2 | 178 | 0 | 4 |
| Peak Hour Factor | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 | 0.93 |
| Hourly flow rate (vph) | 537 | 2 | 2 | 191 | 0 | 4 |
| Pedestrians | | | | | | |
| Lane Width (ft) | | | | | | |
| Walking Speed (ft/s) | | | | | | |
| Percent Blockage | | | | | | |
| Right turn flare (veh) | | | | | | |
| Median type | | | | | None | |
| Median storage (veh) | | | | | | |
| Upstream signal (ft) | | | | | | |
| pX, platoon unblocked | | | | | | |
| vC, conflicting volume | | | 539 | | 733 | 538 |
| vC1, stage 1 conf vol | | | | | | |
| vC2, stage 2 conf vol | | | | | | |
| vCu, unblocked vol | | | 539 | | 733 | 538 |
| tC, single (s) | | | 5.1 | | 7.4 | 7.2 |
| tC, 2 stage (s) | | | | | | |
| tF (s) | | | 3.1 | | 4.4 | 4.2 |
| p0 queue free % | | | 100 | | 100 | 99 |
| cM capacity (veh/h) | | | 677 | | 274 | 394 |
| Direction, Lane # | EB 1 | WB 1 | NB 1 | | | |
| Volume Total | 539 | 194 | 4 | | | |
| Volume Left | 0 | 2 | 0 | | | |
| Volume Right | 2 | 0 | 4 | | | |
| cSH | 1700 | 677 | 394 | | | |
| Volume to Capacity | 0.32 | 0.00 | 0.01 | | | |
| Queue Length 95th (ft) | 0 | 0 | 1 | | | |
| Control Delay (s) | 0.0 | 0.2 | 14.2 | | | |
| Lane LOS | | A | B | | | |
| Approach Delay (s) | 0.0 | 0.2 | 14.2 | | | |
| Approach LOS | | | B | | | |
| Intersection Summary | | | | | | |
| Average Delay | | | 0.1 | | | |
| Intersection Capacity Utilization | | | 36.4% | ICU Level of Service | | A |
| Analysis Period (min) | | | 15 | | | |

ALL TRAFFIC DATA

0480-01

County of Stanislaus
 All Vehicles & Uturns On Unshifted
 Bikes & Peds On Bank 1
 Heavy Trucks On Bank 2

(916) 771-8700

orders@atdtraffic.com

File Name : 16-7710-001 Bystrum Rd & Keyes Rd
 Date : 10/4/2016

Unshifted Count = All Vehicles & Uturns

| START TIME | Bystrum Rd Southbound | | | | | Keyes Rd Westbound | | | | | Bystrum Rd Northbound | | | | | Keyes Rd Eastbound | | | | | Total | Uturns Total |
|--------------------|-----------------------|----------|----------|----------|-----------|--------------------|------------|----------|----------|------------|-----------------------|----------|------------|----------|------------|--------------------|-------------|-----------|----------|-------------|-------------|--------------|
| | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | | |
| 7:00 | 0 | 0 | 0 | 0 | 0 | 8 | 36 | 0 | 0 | 44 | 1 | 0 | 3 | 0 | 4 | 0 | 30 | 5 | 0 | 35 | 83 | 0 |
| 7:15 | 0 | 0 | 0 | 0 | 0 | 12 | 67 | 0 | 0 | 79 | 10 | 0 | 5 | 0 | 15 | 0 | 40 | 9 | 0 | 49 | 143 | 0 |
| 7:30 | 0 | 0 | 0 | 0 | 0 | 11 | 70 | 0 | 0 | 81 | 4 | 0 | 3 | 0 | 7 | 0 | 40 | 4 | 0 | 44 | 132 | 0 |
| 7:45 | 0 | 0 | 0 | 0 | 0 | 14 | 79 | 0 | 0 | 93 | 6 | 0 | 4 | 0 | 10 | 0 | 36 | 10 | 0 | 46 | 149 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 45 | 252 | 0 | 0 | 297 | 21 | 0 | 15 | 0 | 36 | 0 | 146 | 28 | 0 | 174 | 507 | 0 |
| 8:00 | 0 | 0 | 0 | 0 | 0 | 13 | 71 | 0 | 0 | 84 | 7 | 0 | 6 | 0 | 13 | 0 | 29 | 6 | 0 | 35 | 132 | 0 |
| 8:15 | 0 | 0 | 0 | 0 | 0 | 13 | 62 | 0 | 0 | 75 | 0 | 0 | 3 | 0 | 3 | 0 | 26 | 6 | 0 | 32 | 110 | 0 |
| 8:30 | 0 | 0 | 0 | 0 | 0 | 3 | 40 | 0 | 0 | 43 | 5 | 0 | 5 | 0 | 10 | 0 | 38 | 5 | 0 | 43 | 96 | 0 |
| 8:45 | 0 | 0 | 0 | 0 | 0 | 6 | 35 | 0 | 0 | 41 | 0 | 0 | 2 | 0 | 2 | 0 | 30 | 4 | 0 | 34 | 77 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 35 | 208 | 0 | 0 | 243 | 12 | 0 | 16 | 0 | 28 | 0 | 123 | 21 | 0 | 144 | 415 | 0 |
| 16:00 | 0 | 0 | 0 | 0 | 0 | 2 | 43 | 0 | 0 | 45 | 8 | 0 | 21 | 0 | 29 | 0 | 87 | 0 | 0 | 87 | 161 | 0 |
| 16:15 | 0 | 0 | 0 | 0 | 0 | 4 | 47 | 0 | 0 | 51 | 2 | 0 | 7 | 0 | 9 | 0 | 81 | 3 | 0 | 84 | 144 | 0 |
| 16:30 | 0 | 0 | 0 | 0 | 0 | 1 | 34 | 0 | 0 | 35 | 5 | 0 | 7 | 0 | 12 | 0 | 108 | 2 | 0 | 110 | 157 | 0 |
| 16:45 | 0 | 0 | 0 | 0 | 0 | 1 | 50 | 0 | 0 | 51 | 5 | 0 | 2 | 0 | 7 | 0 | 96 | 0 | 0 | 96 | 154 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 8 | 174 | 0 | 0 | 182 | 20 | 0 | 37 | 0 | 57 | 0 | 372 | 5 | 0 | 377 | 616 | 0 |
| 17:00 | 0 | 0 | 0 | 0 | 0 | 2 | 45 | 0 | 0 | 47 | 6 | 0 | 20 | 0 | 26 | 0 | 107 | 0 | 0 | 107 | 180 | 0 |
| 17:15 | 0 | 0 | 0 | 0 | 0 | 5 | 39 | 0 | 0 | 44 | 3 | 0 | 13 | 0 | 16 | 0 | 113 | 5 | 0 | 118 | 178 | 0 |
| 17:30 | 0 | 0 | 0 | 0 | 0 | 10 | 34 | 0 | 0 | 44 | 5 | 0 | 8 | 0 | 13 | 0 | 93 | 8 | 0 | 101 | 158 | 0 |
| 17:45 | 0 | 0 | 0 | 0 | 0 | 7 | 35 | 0 | 0 | 42 | 1 | 0 | 5 | 0 | 6 | 0 | 105 | 5 | 0 | 110 | 158 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 24 | 153 | 0 | 0 | 177 | 15 | 0 | 46 | 0 | 61 | 0 | 418 | 18 | 0 | 436 | 674 | 0 |
| Grand Total | 0 | 0 | 0 | 0 | 0 | 112 | 787 | 0 | 0 | 899 | 68 | 0 | 114 | 0 | 182 | 0 | 1059 | 72 | 0 | 1131 | 2212 | 0 |
| Apprch % | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% | 12.5% | 87.5% | 0.0% | 0.0% | 40.6% | 37.4% | 0.0% | 62.6% | 0.0% | 8.2% | 0.0% | 93.6% | 6.4% | 0.0% | 51.1% | 100.0% | 0 |
| Total % | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% | 5.1% | 35.6% | 0.0% | 0.0% | 40.6% | 3.1% | 0.0% | 5.2% | 0.0% | 8.2% | 0.0% | 47.9% | 3.3% | 0.0% | 51.1% | 100.0% | 0 |

123

| AM PEAK HOUR | Bystrum Rd Southbound | | | | | Keyes Rd Westbound | | | | | Bystrum Rd Northbound | | | | | Keyes Rd Eastbound | | | | | Total |
|---|-----------------------|------|-------|--------|-----------|--------------------|-------|-------|--------|-----------|-----------------------|------|-------|--------|-----------|--------------------|-------|-------|--------|-----------|--------|
| START TIME | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | |
| Peak Hour Analysis From 07:15 to 08:15 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour For Entire Intersection Begins at 07:15 | | | | | | | | | | | | | | | | | | | | | |
| 7:15 | 0 | 0 | 0 | 0 | 0 | 12 | 67 | 0 | 0 | 79 | 10 | 0 | 5 | 0 | 15 | 0 | 40 | 9 | 0 | 49 | 143 |
| 7:30 | 0 | 0 | 0 | 0 | 0 | 11 | 70 | 0 | 0 | 81 | 4 | 0 | 3 | 0 | 7 | 0 | 40 | 4 | 0 | 44 | 132 |
| 7:45 | 0 | 0 | 0 | 0 | 0 | 14 | 79 | 0 | 0 | 93 | 6 | 0 | 4 | 0 | 10 | 0 | 36 | 10 | 0 | 46 | 149 |
| 8:00 | 0 | 0 | 0 | 0 | 0 | 13 | 71 | 0 | 0 | 84 | 7 | 0 | 6 | 0 | 13 | 0 | 29 | 6 | 0 | 35 | 132 |
| Total Volume | 0 | 0 | 0 | 0 | 0 | 50 | 287 | 0 | 0 | 337 | 27 | 0 | 18 | 0 | 45 | 0 | 145 | 29 | 0 | 174 | 556 |
| % App Total | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% | 14.8% | 85.2% | 0.0% | 0.0% | 40.6% | 60.0% | 0.0% | 40.0% | 0.0% | 8.2% | 0.0% | 83.3% | 16.7% | 0.0% | 51.1% | 100.0% |
| PHF | .000 | .000 | .000 | .000 | .000 | .893 | .908 | .000 | .000 | .906 | .675 | .000 | .750 | .000 | .750 | .000 | .906 | .725 | .000 | .888 | .933 |

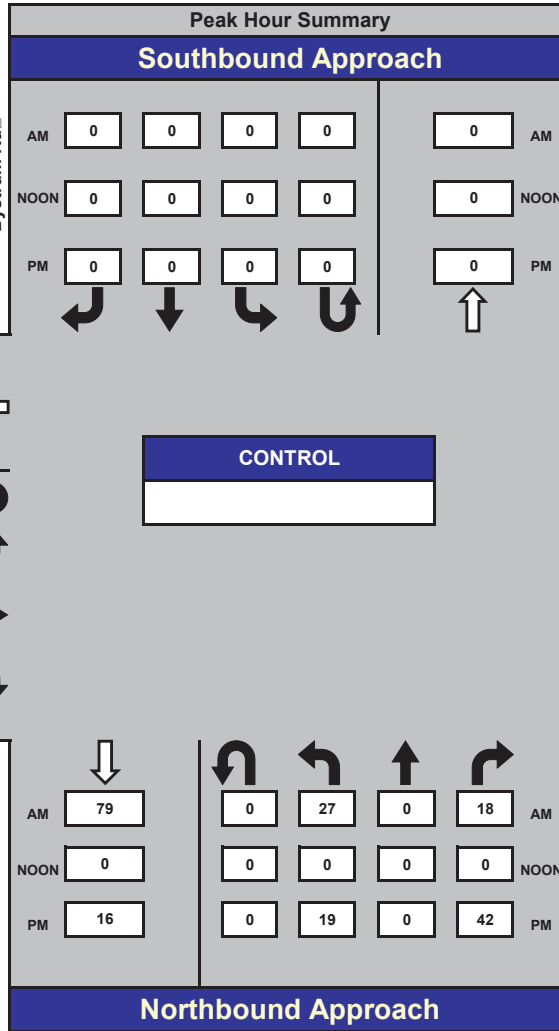
| PM PEAK HOUR | Bystrum Rd Southbound | | | | | Keyes Rd Westbound | | | | | Bystrum Rd Northbound | | | | | Keyes Rd Eastbound | | | | | Total |
|---|-----------------------|------|-------|--------|-----------|--------------------|-------|-------|--------|-----------|-----------------------|------|-------|--------|-----------|--------------------|-------|-------|--------|-----------|--------|
| START TIME | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | |
| Peak Hour Analysis From 16:30 to 17:30 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour For Entire Intersection Begins at 16:30 | | | | | | | | | | | | | | | | | | | | | |
| 16:30 | 0 | 0 | 0 | 0 | 0 | 1 | 34 | 0 | 0 | 35 | 5 | 0 | 7 | 0 | 12 | 0 | 108 | 2 | 0 | 110 | 157 |
| 16:45 | 0 | 0 | 0 | 0 | 0 | 1 | 50 | 0 | 0 | 51 | 5 | 0 | 2 | 0 | 7 | 0 | 96 | 0 | 0 | 96 | 154 |
| 17:00 | 0 | 0 | 0 | 0 | 0 | 2 | 45 | 0 | 0 | 47 | 6 | 0 | 20 | 0 | 26 | 0 | 107 | 0 | 0 | 107 | 180 |
| 17:15 | 0 | 0 | 0 | 0 | 0 | 5 | 39 | 0 | 0 | 44 | 3 | 0 | 13 | 0 | 16 | 0 | 113 | 5 | 0 | 118 | 178 |
| Total Volume | 0 | 0 | 0 | 0 | 0 | 9 | 168 | 0 | 0 | 177 | 19 | 0 | 42 | 0 | 61 | 0 | 424 | 7 | 0 | 431 | 669 |
| % App Total | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% | 5.1% | 94.9% | 0.0% | 0.0% | 40.6% | 31.1% | 0.0% | 68.9% | 0.0% | 8.2% | 0.0% | 98.4% | 1.6% | 0.0% | 51.1% | 100.0% |
| PHF | .000 | .000 | .000 | .000 | .000 | .450 | .840 | .000 | .000 | .868 | .792 | .000 | .525 | .000 | .587 | .000 | .938 | .350 | .000 | .913 | .929 |

Bystrum Rd & Keyes Rd

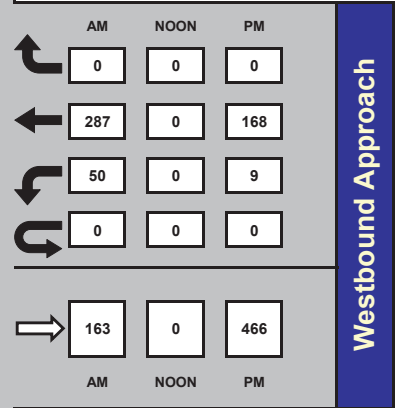
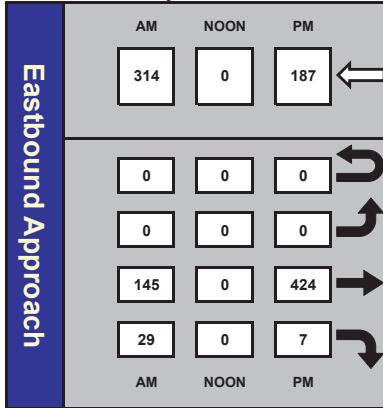
Date: 10/4/2016

Day: Tuesday

Project #: 16-7710-001

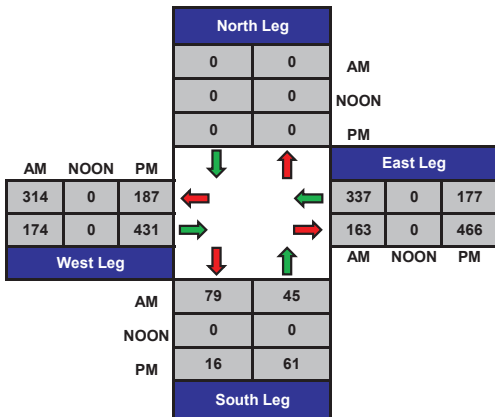


| | |
|----------------|---------------|
| AM Peak Hour | 07:15 - 08:15 |
| NOON Peak Hour | |
| PM Peak Hour | 16:30 - 17:30 |

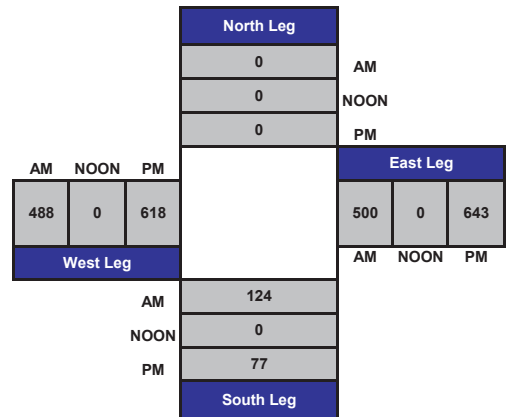


| Count Periods | Start | End |
|---------------|---------|---------|
| AM | 7:00 AM | 9:00 AM |
| NOON | NONE | NONE |
| PM | 4:00 PM | 6:00 PM |

Total Ins & Outs



Total Volume Per Leg



ALL TRAFFIC DATA

County of Stanislaus
 All Vehicles & Turns On Unshifted
 Bikes & Peds On Bank 1
 Heavy Trucks On Bank 2

(916) 771-8700
orders@atdtraffic.com

File Name : 16-7710-001 Bystrum Rd & Keyes Rd
 Date : 10/4/2016

Bank 2 Count = Heavy Trucks

| START TIME | Bystrum Rd Southbound | | | | | Keyes Rd Westbound | | | | | Bystrum Rd Northbound | | | | | Keyes Rd Eastbound | | | | | Total | Peds Total |
|-------------|-----------------------|------|-------|------|-----------|--------------------|-------|-------|------|-----------|-----------------------|------|-------|------|-----------|--------------------|-------|-------|------|-----------|--------|------------|
| | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | | |
| 7:00 | 0 | 0 | 0 | 0 | 0 | 1 | 4 | 0 | 0 | 5 | 1 | 0 | 0 | 0 | 1 | 0 | 2 | 2 | 0 | 4 | 10 | 0 |
| 7:15 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | 0 | 0 | 8 | 3 | 0 | 3 | 0 | 6 | 0 | 8 | 1 | 0 | 9 | 23 | 0 |
| 7:30 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 6 | 1 | 0 | 2 | 0 | 3 | 0 | 4 | 1 | 0 | 5 | 14 | 0 |
| 7:45 | 0 | 0 | 0 | 0 | 0 | 0 | 10 | 0 | 0 | 10 | 1 | 0 | 1 | 0 | 2 | 0 | 7 | 1 | 0 | 8 | 20 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 1 | 28 | 0 | 0 | 29 | 6 | 0 | 6 | 0 | 12 | 0 | 21 | 5 | 0 | 26 | 67 | 0 |
| 8:00 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 7 | 1 | 0 | 0 | 0 | 1 | 0 | 4 | 1 | 0 | 5 | 13 | 0 |
| 8:15 | 0 | 0 | 0 | 0 | 0 | 1 | 10 | 0 | 0 | 11 | 0 | 0 | 1 | 0 | 1 | 0 | 5 | 2 | 0 | 7 | 19 | 0 |
| 8:30 | 0 | 0 | 0 | 0 | 0 | 1 | 2 | 0 | 0 | 3 | 3 | 0 | 1 | 0 | 4 | 0 | 3 | 2 | 0 | 5 | 12 | 0 |
| 8:45 | 0 | 0 | 0 | 0 | 0 | 1 | 5 | 0 | 0 | 6 | 0 | 0 | 1 | 0 | 1 | 0 | 6 | 2 | 0 | 8 | 15 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 3 | 24 | 0 | 0 | 27 | 4 | 0 | 3 | 0 | 7 | 0 | 18 | 7 | 0 | 25 | 59 | 0 |
| 16:00 | 0 | 0 | 0 | 0 | 0 | 2 | 6 | 0 | 0 | 8 | 1 | 0 | 2 | 0 | 3 | 0 | 4 | 0 | 0 | 4 | 15 | 0 |
| 16:15 | 0 | 0 | 0 | 0 | 0 | 1 | 4 | 0 | 0 | 5 | 1 | 0 | 2 | 0 | 3 | 0 | 2 | 3 | 0 | 5 | 13 | 0 |
| 16:30 | 0 | 0 | 0 | 0 | 0 | 1 | 2 | 0 | 0 | 3 | 3 | 0 | 0 | 0 | 3 | 0 | 12 | 2 | 0 | 14 | 20 | 0 |
| 16:45 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 6 | 2 | 0 | 1 | 0 | 3 | 0 | 7 | 0 | 0 | 7 | 16 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 4 | 18 | 0 | 0 | 22 | 7 | 0 | 5 | 0 | 12 | 0 | 25 | 5 | 0 | 30 | 64 | 0 |
| 17:00 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 7 | 2 | 0 | 2 | 0 | 4 | 0 | 7 | 0 | 0 | 7 | 18 | 0 |
| 17:15 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 2 | 1 | 0 | 4 | 0 | 5 | 0 | 8 | 1 | 0 | 9 | 16 | 0 |
| 17:30 | 0 | 0 | 0 | 0 | 0 | 1 | 4 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 5 | 10 | 0 |
| 17:45 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 | 0 | 0 | 1 | 0 | 1 | 0 | 7 | 1 | 0 | 8 | 12 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 1 | 16 | 0 | 0 | 17 | 3 | 0 | 7 | 0 | 10 | 0 | 27 | 2 | 0 | 29 | 56 | 0 |
| Grand Total | 0 | 0 | 0 | 0 | 0 | 9 | 86 | 0 | 0 | 95 | 20 | 0 | 21 | 0 | 41 | 0 | 91 | 19 | 0 | 110 | 246 | 0 |
| Apprch % | 0.0% | 0.0% | 0.0% | | | 9.5% | 90.5% | 0.0% | | 38.6% | 48.8% | 0.0% | 51.2% | | 16.7% | 0.0% | 82.7% | 17.3% | | 44.7% | 100.0% | |
| Total % | 0.0% | 0.0% | 0.0% | | | 3.7% | 35.0% | 0.0% | | 38.6% | 8.1% | 0.0% | 8.5% | | 16.7% | 0.0% | 37.0% | 7.7% | | 44.7% | 100.0% | |

125

| AM PEAK HOUR | Bystrum Rd Southbound | | | | | Keyes Rd Westbound | | | | | Bystrum Rd Northbound | | | | | Keyes Rd Eastbound | | | | | Total |
|---|-----------------------|------|-------|------|-----------|--------------------|--------|-------|------|-----------|-----------------------|------|-------|------|-----------|--------------------|-------|-------|------|-----------|--------|
| START TIME | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | Total |
| Peak Hour Analysis From 07:15 to 08:15 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour For Entire Intersection Begins at 07:15 | | | | | | | | | | | | | | | | | | | | | |
| 7:15 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | 0 | 0 | 8 | 3 | 0 | 3 | 0 | 6 | 0 | 8 | 1 | 0 | 9 | 23 |
| 7:30 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 6 | 1 | 0 | 2 | 0 | 3 | 0 | 4 | 1 | 0 | 5 | 14 |
| 7:45 | 0 | 0 | 0 | 0 | 0 | 0 | 10 | 0 | 0 | 10 | 1 | 0 | 1 | 0 | 2 | 0 | 7 | 1 | 0 | 8 | 20 |
| 8:00 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 7 | 1 | 0 | 0 | 0 | 1 | 0 | 4 | 1 | 0 | 5 | 13 |
| Total Volume | 0 | 0 | 0 | 0 | 0 | 0 | 31 | 0 | 0 | 31 | 6 | 0 | 6 | 0 | 12 | 0 | 23 | 4 | 0 | 27 | 70 |
| % App Total | 0.0% | 0.0% | 0.0% | | | 0.0% | 100.0% | 0.0% | | 38.6% | 50.0% | 0.0% | 50.0% | | 16.7% | 0.0% | 85.2% | 14.8% | | 44.7% | 100.0% |
| PHF | .000 | .000 | .000 | | | .000 | .775 | .000 | | .775 | .500 | .000 | .500 | | .500 | .000 | .719 | 1.000 | | .750 | .761 |

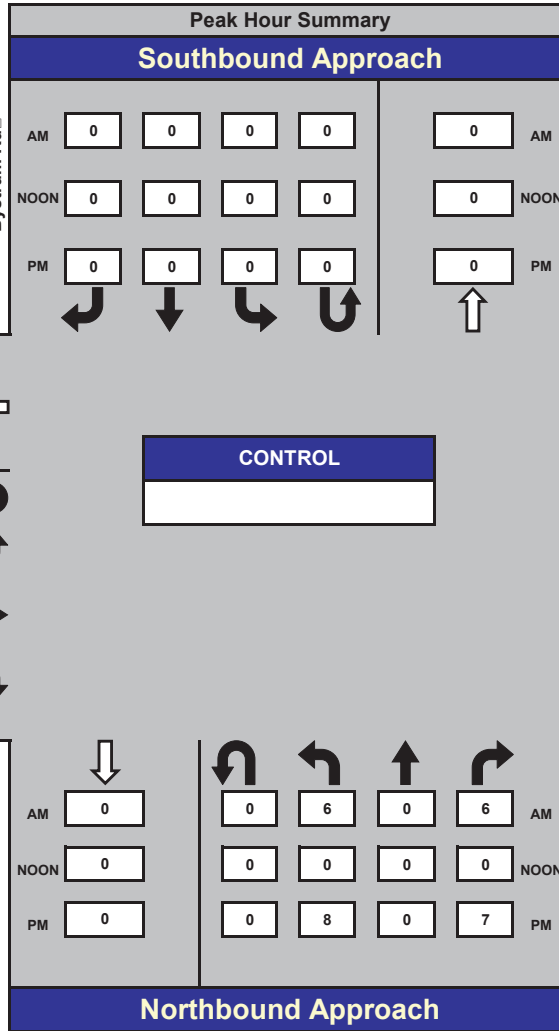
| PM PEAK HOUR | Bystrum Rd Southbound | | | | | Keyes Rd Westbound | | | | | Bystrum Rd Northbound | | | | | Keyes Rd Eastbound | | | | | Total |
|---|-----------------------|------|-------|------|-----------|--------------------|-------|-------|------|-----------|-----------------------|------|-------|------|-----------|--------------------|-------|-------|------|-----------|--------|
| START TIME | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | Total |
| Peak Hour Analysis From 16:30 to 17:30 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour For Entire Intersection Begins at 16:30 | | | | | | | | | | | | | | | | | | | | | |
| 16:30 | 0 | 0 | 0 | 0 | 0 | 1 | 2 | 0 | 0 | 3 | 3 | 0 | 0 | 0 | 3 | 0 | 12 | 2 | 0 | 14 | 20 |
| 16:45 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 6 | 2 | 0 | 1 | 0 | 3 | 0 | 7 | 0 | 0 | 7 | 16 |
| 17:00 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 7 | 2 | 0 | 2 | 0 | 4 | 0 | 7 | 0 | 0 | 7 | 18 |
| 17:15 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 2 | 1 | 0 | 4 | 0 | 5 | 0 | 8 | 1 | 0 | 9 | 16 |
| Total Volume | 0 | 0 | 0 | 0 | 0 | 1 | 17 | 0 | 0 | 18 | 8 | 0 | 7 | 0 | 15 | 0 | 34 | 3 | 0 | 37 | 70 |
| % App Total | 0.0% | 0.0% | 0.0% | | | 5.6% | 94.4% | 0.0% | | 38.6% | 53.3% | 0.0% | 46.7% | | 16.7% | 0.0% | 91.9% | 8.1% | | 44.7% | 100.0% |
| PHF | .000 | .000 | .000 | | | .250 | .607 | .000 | | .643 | .667 | .000 | .438 | | .750 | .000 | .708 | .375 | | .661 | .875 |

Bystrum Rd & Keyes Rd

Date: 10/4/2016

Day: Tuesday

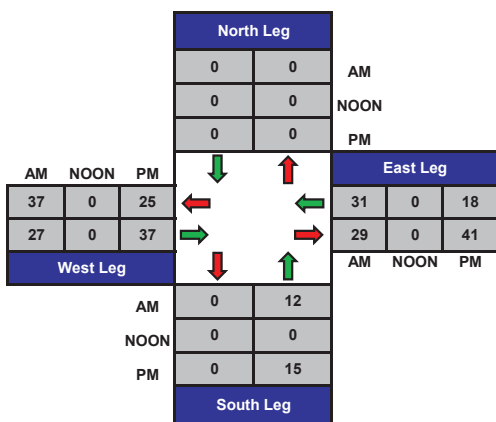
Project #: 16-7710-001



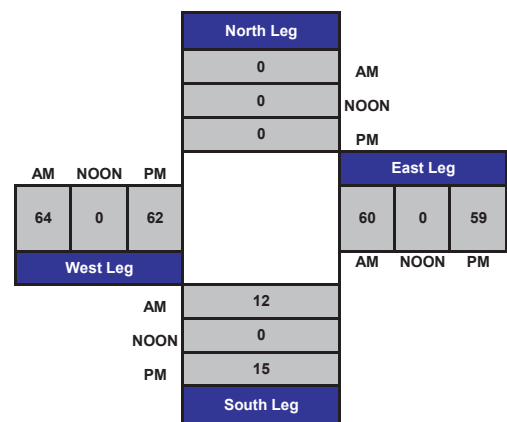
| | |
|----------------|---------------|
| AM Peak Hour | 07:15 - 08:15 |
| NOON Peak Hour | |
| PM Peak Hour | 16:30 - 17:30 |

| Count Periods | Start | End |
|---------------|---------|---------|
| AM | 7:00 AM | 9:00 AM |
| NOON | NONE | NONE |
| PM | 4:00 PM | 6:00 PM |

Total Ins & Outs



Total Volume Per Leg



ALL TRAFFIC DATA

0480-01

County of Stanislaus
 All Vehicles & Uturns On Unshifted
 Bikes & Peds On Bank 1
 Heavy Trucks On Bank 2

(916) 771-8700
orders@atdtraffic.com

File Name : 16-7710-002 East Entrance to Bronco Winery & Keyes Rd
 Date : 10/4/2016

Unshifted Count = All Vehicles & Uturns

| START TIME | East Entrance to Bronco Winery Southbound | | | | | Keyes Rd Westbound | | | | | East Entrance to Bronco Winery Northbound | | | | | Keyes Rd Eastbound | | | | | Total | Uturns Total |
|-------------|---|------|-------|--------|-----------|--------------------|-------|-------|--------|-----------|---|------|-------|--------|-----------|--------------------|-------|-------|--------|-----------|--------|--------------|
| | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | | |
| 7:00 | 0 | 0 | 0 | 0 | 0 | 0 | 54 | 0 | 0 | 54 | 0 | 0 | 0 | 0 | 0 | 0 | 31 | 0 | 0 | 31 | 85 | 0 |
| 7:15 | 0 | 0 | 0 | 0 | 0 | 0 | 73 | 0 | 0 | 73 | 0 | 0 | 0 | 0 | 0 | 0 | 35 | 1 | 0 | 36 | 109 | 0 |
| 7:30 | 0 | 0 | 0 | 0 | 0 | 0 | 104 | 0 | 0 | 104 | 0 | 0 | 0 | 0 | 0 | 0 | 53 | 1 | 0 | 54 | 158 | 0 |
| 7:45 | 0 | 0 | 0 | 0 | 0 | 0 | 109 | 0 | 0 | 109 | 0 | 0 | 0 | 0 | 0 | 0 | 42 | 0 | 0 | 42 | 151 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 0 | 340 | 0 | 0 | 340 | 0 | 0 | 0 | 0 | 0 | 0 | 161 | 2 | 0 | 163 | 503 | 0 |
| 8:00 | 0 | 0 | 0 | 0 | 0 | 0 | 97 | 0 | 0 | 97 | 2 | 0 | 0 | 0 | 2 | 0 | 38 | 1 | 0 | 39 | 138 | 0 |
| 8:15 | 0 | 0 | 0 | 0 | 0 | 1 | 71 | 0 | 0 | 72 | 0 | 0 | 0 | 0 | 0 | 0 | 26 | 0 | 0 | 26 | 98 | 0 |
| 8:30 | 0 | 0 | 0 | 0 | 0 | 0 | 53 | 0 | 0 | 53 | 0 | 0 | 0 | 0 | 0 | 0 | 45 | 0 | 0 | 45 | 98 | 0 |
| 8:45 | 0 | 0 | 0 | 0 | 0 | 0 | 41 | 0 | 0 | 41 | 0 | 0 | 0 | 0 | 0 | 0 | 35 | 0 | 0 | 35 | 76 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 1 | 262 | 0 | 0 | 263 | 2 | 0 | 0 | 0 | 2 | 0 | 144 | 1 | 0 | 145 | 410 | 0 |
| 16:00 | 0 | 0 | 0 | 0 | 0 | 0 | 49 | 0 | 0 | 49 | 0 | 0 | 0 | 0 | 0 | 0 | 120 | 0 | 0 | 120 | 169 | 0 |
| 16:15 | 0 | 0 | 0 | 0 | 0 | 0 | 50 | 0 | 0 | 50 | 0 | 0 | 0 | 0 | 0 | 0 | 94 | 0 | 0 | 94 | 144 | 0 |
| 16:30 | 0 | 0 | 0 | 0 | 0 | 1 | 38 | 0 | 0 | 39 | 0 | 0 | 0 | 0 | 0 | 0 | 130 | 0 | 0 | 130 | 169 | 0 |
| 16:45 | 0 | 0 | 0 | 0 | 0 | 0 | 49 | 0 | 0 | 49 | 0 | 0 | 1 | 0 | 1 | 0 | 108 | 1 | 0 | 109 | 159 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 1 | 186 | 0 | 0 | 187 | 0 | 0 | 1 | 0 | 1 | 0 | 452 | 1 | 0 | 453 | 641 | 0 |
| 17:00 | 0 | 0 | 0 | 0 | 0 | 1 | 48 | 0 | 0 | 49 | 0 | 0 | 1 | 0 | 1 | 0 | 127 | 0 | 0 | 127 | 177 | 0 |
| 17:15 | 0 | 0 | 0 | 0 | 0 | 0 | 43 | 0 | 0 | 43 | 0 | 0 | 2 | 0 | 2 | 0 | 129 | 1 | 0 | 130 | 175 | 0 |
| 17:30 | 0 | 0 | 0 | 0 | 0 | 0 | 43 | 0 | 0 | 43 | 0 | 0 | 1 | 0 | 1 | 0 | 104 | 1 | 0 | 105 | 149 | 0 |
| 17:45 | 0 | 0 | 0 | 0 | 0 | 0 | 44 | 0 | 0 | 44 | 0 | 0 | 1 | 0 | 1 | 0 | 121 | 0 | 0 | 121 | 166 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 1 | 178 | 0 | 0 | 179 | 0 | 0 | 5 | 0 | 5 | 0 | 481 | 2 | 0 | 483 | 667 | 0 |
| Grand Total | 0 | 0 | 0 | 0 | 0 | 3 | 966 | 0 | 0 | 969 | 2 | 0 | 6 | 0 | 8 | 0 | 1238 | 6 | 0 | 1244 | 2221 | 0 |
| Apprch % | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% | 0.3% | 99.7% | 0.0% | 0.0% | 43.6% | 25.0% | 0.0% | 75.0% | 0.0% | 0.4% | 0.0% | 99.5% | 0.5% | 0.0% | 56.0% | 100.0% | |
| Total % | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% | 0.1% | 43.5% | 0.0% | 0.0% | 43.6% | 0.1% | 0.0% | 0.3% | 0.0% | 0.4% | 0.0% | 55.7% | 0.3% | 0.0% | 56.0% | 100.0% | |

127

| AM PEAK HOUR | East Entrance to Bronco Winery Southbound | | | | | Keyes Rd Westbound | | | | | East Entrance to Bronco Winery Northbound | | | | | Keyes Rd Eastbound | | | | | Total |
|---|---|------|-------|--------|-----------|--------------------|--------|-------|--------|-----------|---|------|-------|--------|-----------|--------------------|-------|-------|--------|-----------|--------|
| | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | |
| Peak Hour Analysis From 07:15 to 08:15 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour For Entire Intersection Begins at 07:15 | | | | | | | | | | | | | | | | | | | | | |
| 7:15 | 0 | 0 | 0 | 0 | 0 | 0 | 73 | 0 | 0 | 73 | 0 | 0 | 0 | 0 | 0 | 0 | 35 | 1 | 0 | 36 | 109 |
| 7:30 | 0 | 0 | 0 | 0 | 0 | 0 | 104 | 0 | 0 | 104 | 0 | 0 | 0 | 0 | 0 | 0 | 53 | 1 | 0 | 54 | 158 |
| 7:45 | 0 | 0 | 0 | 0 | 0 | 0 | 109 | 0 | 0 | 109 | 0 | 0 | 0 | 0 | 0 | 0 | 42 | 0 | 0 | 42 | 151 |
| 8:00 | 0 | 0 | 0 | 0 | 0 | 0 | 97 | 0 | 0 | 97 | 2 | 0 | 0 | 0 | 2 | 0 | 38 | 1 | 0 | 39 | 138 |
| Total Volume | 0 | 0 | 0 | 0 | 0 | 0 | 383 | 0 | 0 | 383 | 2 | 0 | 0 | 0 | 2 | 0 | 168 | 3 | 0 | 171 | 556 |
| % App Total | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% | 100.0% | 0.0% | 0.0% | 43.6% | 100.0% | 0.0% | 0.0% | 0.0% | 0.4% | 0.0% | 98.2% | 1.8% | 0.0% | 56.0% | 100.0% |
| PHF | .000 | .000 | .000 | .000 | .000 | .000 | .878 | .000 | .000 | .878 | .250 | .000 | .000 | .000 | .250 | .000 | .792 | .750 | .000 | .792 | .880 |

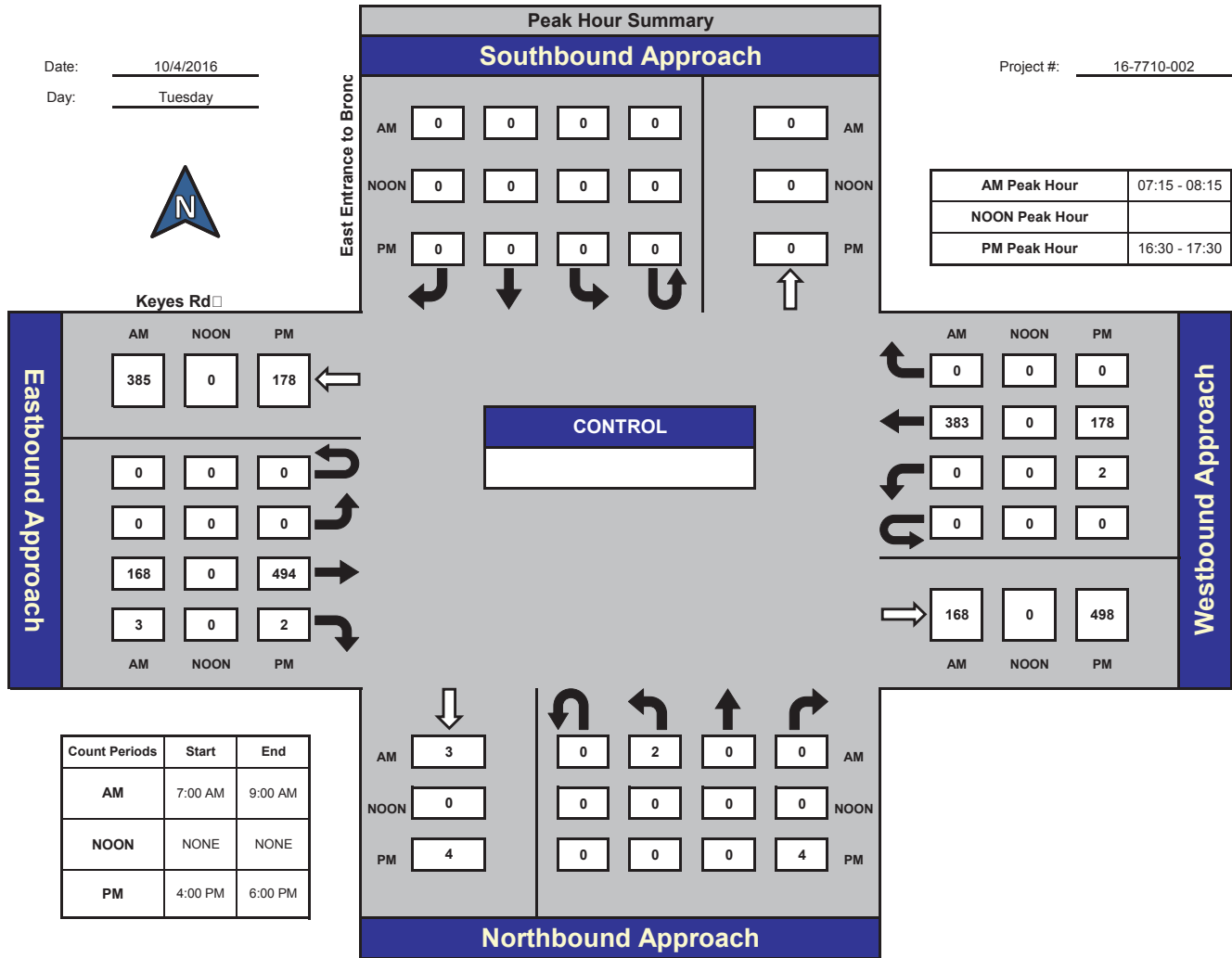
| PM PEAK HOUR | East Entrance to Bronco Winery Southbound | | | | | Keyes Rd Westbound | | | | | East Entrance to Bronco Winery Northbound | | | | | Keyes Rd Eastbound | | | | | Total |
|---|---|------|-------|--------|-----------|--------------------|-------|-------|--------|-----------|---|------|--------|--------|-----------|--------------------|-------|-------|--------|-----------|--------|
| | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | LEFT | THRU | RIGHT | UTURNS | APP.TOTAL | |
| Peak Hour Analysis From 16:30 to 17:30 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour For Entire Intersection Begins at 16:30 | | | | | | | | | | | | | | | | | | | | | |
| 16:30 | 0 | 0 | 0 | 0 | 0 | 1 | 38 | 0 | 0 | 39 | 0 | 0 | 0 | 0 | 0 | 0 | 130 | 0 | 0 | 130 | 169 |
| 16:45 | 0 | 0 | 0 | 0 | 0 | 0 | 49 | 0 | 0 | 49 | 0 | 0 | 1 | 0 | 1 | 0 | 108 | 1 | 0 | 109 | 159 |
| 17:00 | 0 | 0 | 0 | 0 | 0 | 0 | 48 | 0 | 0 | 48 | 0 | 0 | 1 | 0 | 1 | 0 | 127 | 0 | 0 | 127 | 177 |
| 17:15 | 0 | 0 | 0 | 0 | 0 | 0 | 43 | 0 | 0 | 43 | 0 | 0 | 2 | 0 | 2 | 0 | 129 | 1 | 0 | 130 | 175 |
| Total Volume | 0 | 0 | 0 | 0 | 0 | 2 | 178 | 0 | 0 | 180 | 0 | 0 | 4 | 0 | 4 | 0 | 494 | 2 | 0 | 496 | 680 |
| % App Total | 0.0% | 0.0% | 0.0% | 0.0% | 0.0% | 1.1% | 98.9% | 0.0% | 0.0% | 43.6% | 0.0% | 0.0% | 100.0% | 0.0% | 0.4% | 0.0% | 99.6% | 0.4% | 0.0% | 56.0% | 100.0% |
| PHF | .000 | .000 | .000 | .000 | .000 | .500 | .908 | .000 | .000 | .918 | .000 | .000 | .500 | .000 | .500 | .000 | .950 | .500 | .000 | .954 | .960 |

East Entrance to Bronco Winery & Keyes Rd

Date: 10/4/2016

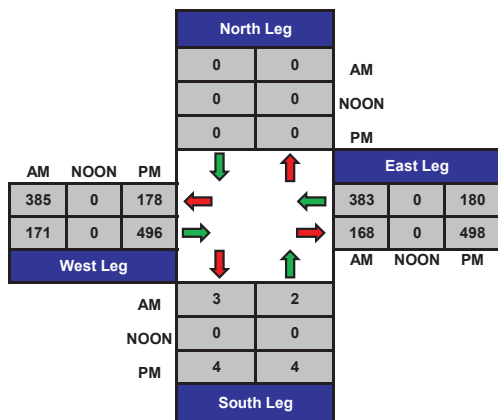
Day: Tuesday

Project #: 16-7710-002

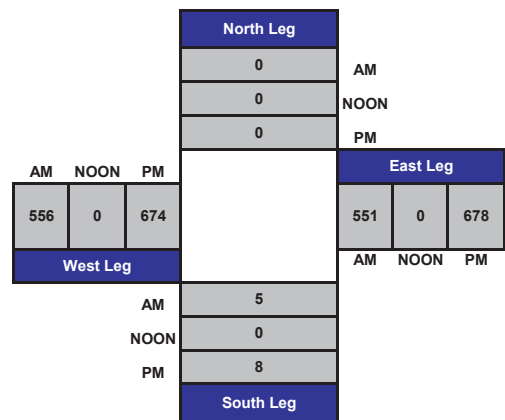


| Count Periods | Start | End |
|---------------|---------|---------|
| AM | 7:00 AM | 9:00 AM |
| NOON | NONE | NONE |
| PM | 4:00 PM | 6:00 PM |

Total Ins & Outs



Total Volume Per Leg



ALL TRAFFIC DATA

County of Stanislaus
 All Vehicles & Turns On Unshifted
 Bikes & Peds On Bank 1
 Heavy Trucks On Bank 2

(916) 771-8700
orders@aldtraffic.com

File Name : 16-7710-002 East Entrance to Bronco Winery & Keyes Rd
 Date : 10/4/2016

Bank 2 Count = Heavy Trucks

| START TIME | East Entrance to Bronco Winery Southbound | | | | | Keyes Rd Westbound | | | | | East Entrance to Bronco Winery Northbound | | | | | Keyes Rd Eastbound | | | | | Total | Peds Total |
|-------------|---|------|-------|------|-----------|--------------------|-------|-------|------|-----------|---|------|-------|------|-----------|--------------------|-------|-------|------|-----------|--------|------------|
| | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | | |
| 7:00 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 2 | 8 | 0 |
| 7:15 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 1 | 0 | 6 | 12 | 0 |
| 7:30 | 0 | 0 | 0 | 0 | 0 | 0 | 10 | 0 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 1 | 0 | 8 | 18 | 0 |
| 7:45 | 0 | 0 | 0 | 0 | 0 | 0 | 9 | 0 | 0 | 9 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | 0 | 0 | 8 | 17 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 0 | 31 | 0 | 0 | 31 | 0 | 0 | 0 | 0 | 0 | 0 | 22 | 2 | 0 | 24 | 55 | 0 |
| 8:00 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 5 | 2 | 0 | 0 | 0 | 2 | 0 | 6 | 0 | 0 | 6 | 13 | 0 |
| 8:15 | 0 | 0 | 0 | 0 | 0 | 0 | 9 | 0 | 0 | 9 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 7 | 16 | 0 |
| 8:30 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 6 | 10 | 0 |
| 8:45 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | 0 | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 7 | 15 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 0 | 26 | 0 | 0 | 26 | 2 | 0 | 0 | 0 | 2 | 0 | 26 | 0 | 0 | 26 | 54 | 0 |
| 16:00 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 5 | 12 | 0 |
| 16:15 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 5 | 10 | 0 |
| 16:30 | 0 | 0 | 0 | 0 | 0 | 1 | 4 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 13 | 0 | 0 | 13 | 18 | 0 |
| 16:45 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 5 | 0 | 0 | 1 | 0 | 1 | 0 | 7 | 1 | 0 | 8 | 14 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 1 | 21 | 0 | 0 | 22 | 0 | 0 | 1 | 0 | 1 | 0 | 30 | 1 | 0 | 31 | 54 | 0 |
| 17:00 | 0 | 0 | 0 | 0 | 0 | 1 | 6 | 0 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 9 | 0 | 0 | 9 | 16 | 0 |
| 17:15 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 | 0 | 0 | 2 | 0 | 2 | 0 | 8 | 1 | 0 | 9 | 14 | 0 |
| 17:30 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 5 | 0 | 0 | 1 | 0 | 1 | 0 | 6 | 1 | 0 | 7 | 13 | 0 |
| 17:45 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 | 0 | 0 | 1 | 0 | 1 | 0 | 6 | 0 | 0 | 6 | 10 | 0 |
| Total | 0 | 0 | 0 | 0 | 0 | 1 | 17 | 0 | 0 | 18 | 0 | 0 | 4 | 0 | 4 | 0 | 29 | 2 | 0 | 31 | 53 | 0 |
| Grand Total | 0 | 0 | 0 | 0 | 0 | 2 | 95 | 0 | 0 | 97 | 2 | 0 | 5 | 0 | 7 | 0 | 107 | 5 | 0 | 112 | 216 | 0 |
| Apprch % | 0.0% | 0.0% | 0.0% | | | 2.1% | 97.9% | 0.0% | | 44.9% | 28.6% | 0.0% | 71.4% | | | 0.0% | 95.5% | 4.5% | | | | |
| Total % | 0.0% | 0.0% | 0.0% | | 0.0% | 0.9% | 44.0% | 0.0% | | | 0.9% | 0.0% | 2.3% | | 3.2% | 0.0% | 49.5% | 2.3% | | 51.9% | 100.0% | |

129

| AM PEAK HOUR | East Entrance to Bronco Winery Southbound | | | | | Keyes Rd Westbound | | | | | East Entrance to Bronco Winery Northbound | | | | | Keyes Rd Eastbound | | | | | Total |
|---|---|------|-------|------|-----------|--------------------|--------|-------|------|-----------|---|------|-------|------|-----------|--------------------|-------|-------|------|-----------|-------|
| START TIME | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | Total |
| Peak Hour Analysis From 07:15 to 08:15 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour For Entire Intersection Begins at 07:15 | | | | | | | | | | | | | | | | | | | | | |
| 7:15 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 6 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 1 | 0 | 6 | 12 |
| 7:30 | 0 | 0 | 0 | 0 | 0 | 0 | 10 | 0 | 0 | 10 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 1 | 0 | 8 | 18 |
| 7:45 | 0 | 0 | 0 | 0 | 0 | 0 | 9 | 0 | 0 | 9 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | 0 | 0 | 8 | 17 |
| 8:00 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 5 | 2 | 0 | 0 | 0 | 2 | 0 | 6 | 0 | 0 | 6 | 13 |
| Total Volume | 0 | 0 | 0 | 0 | 0 | 0 | 30 | 0 | 0 | 30 | 2 | 0 | 0 | 0 | 2 | 0 | 26 | 2 | 0 | 28 | 60 |
| % App Total | 0.0% | 0.0% | 0.0% | | | 0.0% | 100.0% | 0.0% | | | 100.0% | 0.0% | 0.0% | | | 0.0% | 92.9% | 7.1% | | | |
| PHF | .000 | .000 | .000 | | .000 | .000 | .750 | .000 | | .750 | .250 | .000 | .000 | | .250 | .000 | .813 | .500 | | .875 | .833 |

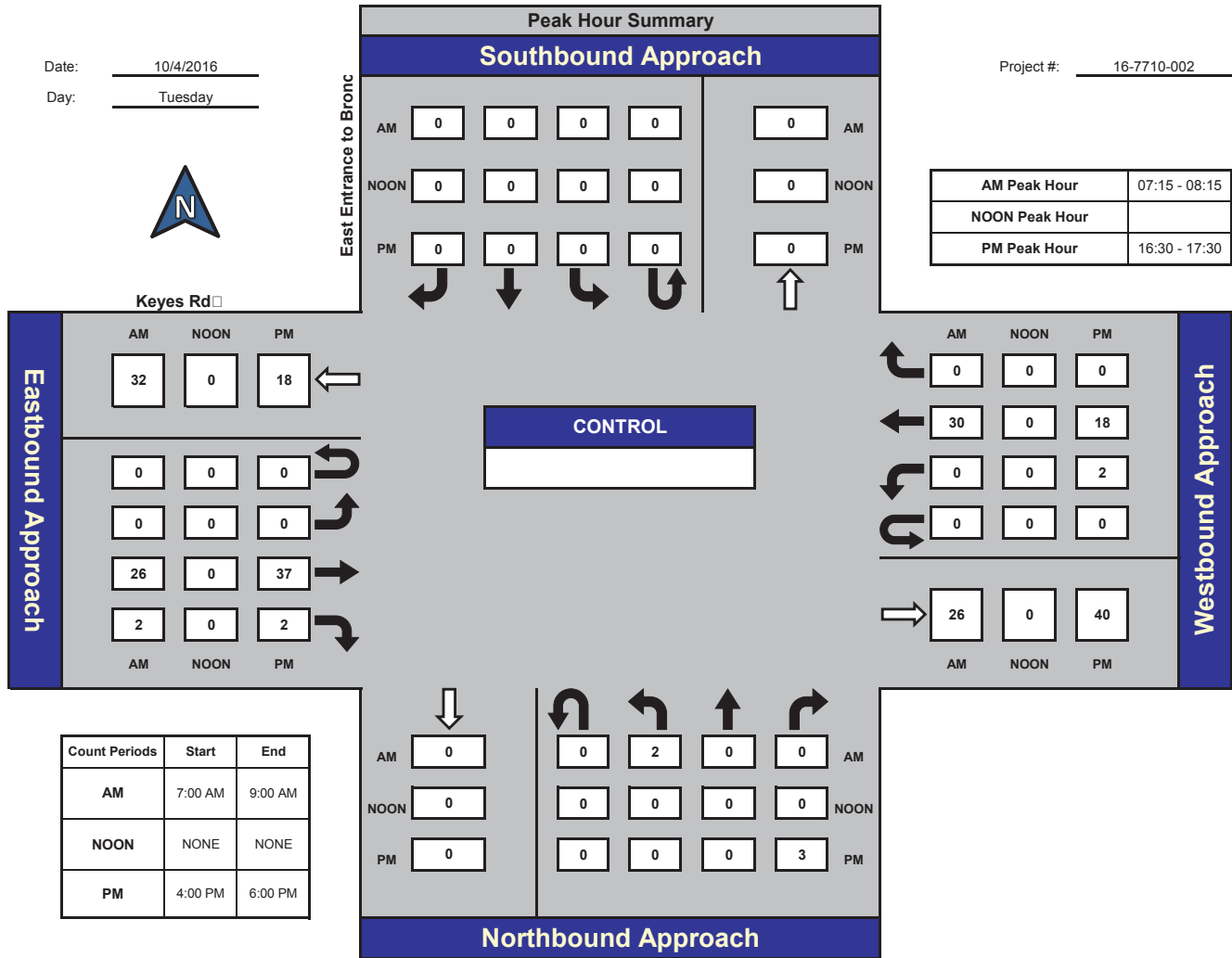
| PM PEAK HOUR | East Entrance to Bronco Winery Southbound | | | | | Keyes Rd Westbound | | | | | East Entrance to Bronco Winery Northbound | | | | | Keyes Rd Eastbound | | | | | Total |
|---|---|------|-------|------|-----------|--------------------|-------|-------|------|-----------|---|------|--------|------|-----------|--------------------|-------|-------|------|-----------|-------|
| START TIME | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | LEFT | THRU | RIGHT | PEDS | APP.TOTAL | Total |
| Peak Hour Analysis From 16:30 to 17:30 | | | | | | | | | | | | | | | | | | | | | |
| Peak Hour For Entire Intersection Begins at 16:30 | | | | | | | | | | | | | | | | | | | | | |
| 16:30 | 0 | 0 | 0 | 0 | 0 | 1 | 4 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 13 | 0 | 0 | 13 | 18 |
| 16:45 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 5 | 0 | 0 | 1 | 0 | 1 | 0 | 7 | 1 | 0 | 8 | 14 |
| 17:00 | 0 | 0 | 0 | 0 | 0 | 1 | 6 | 0 | 0 | 7 | 0 | 0 | 0 | 0 | 0 | 0 | 9 | 0 | 0 | 9 | 16 |
| 17:15 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 | 0 | 0 | 2 | 0 | 2 | 0 | 8 | 1 | 0 | 9 | 14 |
| Total Volume | 0 | 0 | 0 | 0 | 0 | 2 | 18 | 0 | 0 | 20 | 0 | 0 | 3 | 0 | 3 | 0 | 37 | 2 | 0 | 39 | 62 |
| % App Total | 0.0% | 0.0% | 0.0% | | | 10.0% | 90.0% | 0.0% | | | 0.0% | 0.0% | 100.0% | | | 0.0% | 94.9% | 5.1% | | | |
| PHF | .000 | .000 | .000 | | .000 | .500 | .750 | .000 | | .714 | .000 | .000 | .375 | | .375 | .000 | .712 | .500 | | .750 | .861 |

East Entrance to Bronco Winery & Keyes Rd

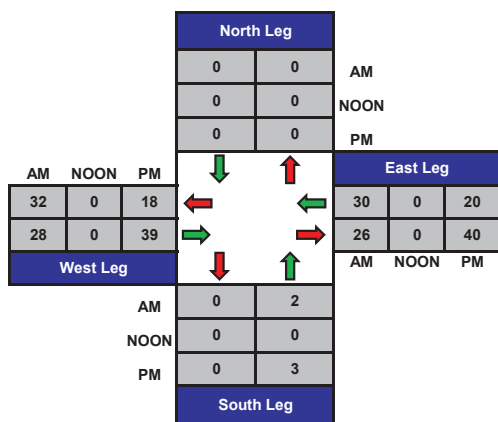
Date: 10/4/2016

Day: Tuesday

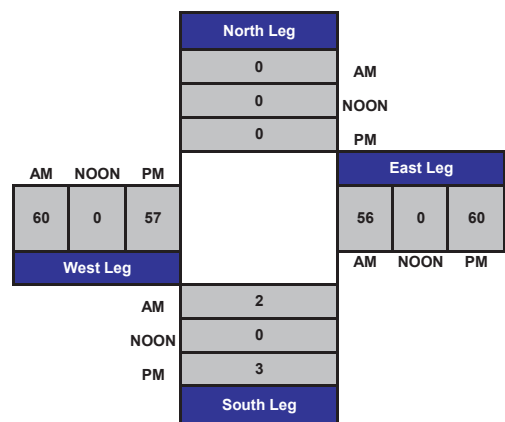
Project #: 16-7710-002



Total Ins & Outs



Total Volume Per Leg



CLASSIFICATION

Bystrum Rd S/O Keyes Rd

Day: Tuesday
Date: 10/4/2016

County: Stanislaus
Project #: CA16_7709_004

Summary

| Time | # 1 | # 2 | # 3 | # 4 | # 5 | # 6 | # 7 | # 8 | # 9 | # 10 | # 11 | # 12 | # 13 | Total |
|---------------|----------|------------|------------|-----------|------------|----------|-----|-----------|------------|------|-----------|----------|------|-------------|
| 0:00 AM | 0 | 21 | 2 | 0 | 1 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 26 |
| 1:00 | 0 | 5 | 1 | 0 | 1 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 9 |
| 2:00 | 0 | 15 | 1 | 1 | 0 | 0 | 0 | 1 | 1 | 0 | 0 | 0 | 0 | 19 |
| 3:00 | 1 | 13 | 1 | 0 | 5 | 0 | 0 | 1 | 1 | 0 | 0 | 0 | 0 | 22 |
| 4:00 | 0 | 10 | 6 | 0 | 11 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 32 |
| 5:00 | 0 | 49 | 8 | 0 | 20 | 0 | 0 | 0 | 9 | 0 | 0 | 0 | 0 | 86 |
| 6:00 | 0 | 73 | 20 | 1 | 8 | 1 | 0 | 1 | 5 | 0 | 2 | 0 | 0 | 111 |
| 7:00 | 0 | 65 | 16 | 0 | 14 | 0 | 0 | 0 | 12 | 0 | 1 | 2 | 0 | 110 |
| 8:00 | 0 | 54 | 6 | 2 | 13 | 0 | 0 | 2 | 10 | 0 | 0 | 0 | 0 | 87 |
| 9:00 | 0 | 18 | 3 | 1 | 7 | 0 | 0 | 1 | 12 | 0 | 2 | 0 | 0 | 44 |
| 10:00 | 1 | 13 | 13 | 4 | 12 | 0 | 0 | 1 | 10 | 0 | 1 | 0 | 0 | 55 |
| 11:00 | 0 | 20 | 8 | 0 | 8 | 0 | 0 | 1 | 12 | 0 | 2 | 0 | 0 | 51 |
| 12:00 PM | 1 | 31 | 8 | 0 | 10 | 1 | 0 | 1 | 12 | 0 | 1 | 2 | 0 | 67 |
| 13:00 | 0 | 50 | 14 | 1 | 11 | 3 | 0 | 1 | 12 | 0 | 0 | 1 | 0 | 93 |
| 14:00 | 0 | 88 | 24 | 5 | 14 | 0 | 0 | 0 | 12 | 0 | 0 | 1 | 0 | 144 |
| 15:00 | 1 | 62 | 16 | 3 | 13 | 0 | 0 | 0 | 5 | 0 | 0 | 1 | 0 | 101 |
| 16:00 | 0 | 36 | 10 | 0 | 11 | 0 | 0 | 2 | 15 | 0 | 2 | 0 | 0 | 76 |
| 17:00 | 0 | 75 | 13 | 1 | 6 | 0 | 0 | 0 | 8 | 0 | 1 | 0 | 0 | 104 |
| 18:00 | 0 | 45 | 9 | 0 | 0 | 1 | 0 | 1 | 7 | 0 | 0 | 0 | 0 | 63 |
| 19:00 | 0 | 18 | 4 | 1 | 3 | 0 | 0 | 0 | 8 | 0 | 0 | 0 | 0 | 34 |
| 20:00 | 0 | 10 | 2 | 0 | 1 | 0 | 0 | 0 | 6 | 0 | 0 | 0 | 0 | 19 |
| 21:00 | 0 | 18 | 3 | 0 | 2 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 24 |
| 22:00 | 0 | 33 | 5 | 0 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 40 |
| 23:00 | 0 | 57 | 1 | 0 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 60 |
| Totals | 4 | 879 | 194 | 20 | 173 | 6 | | 13 | 168 | | 13 | 7 | | 1477 |
| % of Totals | 0% | 60% | 13% | 1% | 12% | 0% | | 1% | 11% | | 1% | 0% | | 100% |

131

| | | | | | | | | | | | | | | | | |
|---------------------------------|-------|---------------|-------|-------|-------|------------------|---|-------|---------------|--------|-------|-------------------------|-----|--------|---|--|
| AM Volumes | 2 | 356 | 85 | 9 | 100 | 1 | 0 | 8 | 80 | 0 | 9 | 2 | 0 | 652 | | |
| % AM | 0% | 24% | 6% | 1% | 7% | 0% | | 1% | 5% | | 1% | 0% | | 44% | | |
| AM Peak Hour | 10:00 | 6:00 | 6:00 | 10:00 | 5:00 | 6:00 | | 8:00 | 7:00 | | 6:00 | 7:00 | | 6:00 | | |
| Volume | 1 | 73 | 20 | 4 | 20 | 1 | | 2 | 12 | | 2 | 2 | | 111 | | |
| PM Volumes | 2 | 523 | 109 | 11 | 73 | 5 | 0 | 5 | 88 | 0 | 4 | 5 | 0 | 825 | | |
| % PM | 0% | 35% | 7% | 1% | 5% | 0% | | 0% | 6% | | 0% | 0% | | 56% | | |
| PM Peak Hour | 12:00 | 14:00 | 14:00 | 14:00 | 14:00 | 13:00 | | 16:00 | 16:00 | | 16:00 | 12:00 | | 14:00 | | |
| Volume | 1 | 88 | 24 | 5 | 14 | 3 | | 2 | 15 | | 2 | 2 | | 144 | | |
| Directional Peak Periods | | AM 7-9 | | | | NOON 12-2 | | | PM 4-6 | | | Off Peak Volumes | | | | |
| All Classes | | Volume | | | % | Volume | | | % | Volume | | | % | Volume | | |
| | | 197 | ↔ | | 13% | 160 | ↔ | | 11% | 180 | ↔ | | 12% | 940 | ↔ | |

| Classification Definitions | | | | |
|-------------------------------|-------------------------------|----------------------------|-----------------------------|----------------------------|
| 1 Motorcycles | 4 Buses | 7 >=4-Axle Single Units | 10 >=6-Axle Single Trailers | 13 >=7-Axle Multi-Trailers |
| 2 Passenger Cars | 5 2-Axle, 6-Tire Single Units | 8 <=4-Axle Single Trailers | 11 <=5-Axle Multi-Trailers | |
| 3 2-Axle, 4-Tire Single Units | 6 3-Axle Single Units | 9 5-Axle Single Trailers | 12 6-Axle Multi-Trailers | |

CLASSIFICATION

Bystrum Rd S/O Keyes Rd

Day: Tuesday
Date: 10/4/2016

County: Stanislaus
Project #: CA16_7709_004s

South Bound

| Time | # 1 | # 2 | # 3 | # 4 | # 5 | # 6 | # 7 | # 8 | # 9 | # 10 | # 11 | # 12 | # 13 | Total |
|--------------------|-----------|------------|------------|-----------|------------|-----------|-----|-----------|------------|------|-----------|-----------|------|-------------|
| 0:00 AM | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 |
| 1:00 | 0 | 2 | 1 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 |
| 2:00 | 0 | 9 | 1 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 11 |
| 3:00 | 1 | 11 | 0 | 0 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 14 |
| 4:00 | 0 | 9 | 4 | 0 | 7 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 24 |
| 5:00 | 0 | 48 | 7 | 0 | 15 | 0 | 0 | 0 | 7 | 0 | 0 | 0 | 0 | 77 |
| 6:00 | 0 | 32 | 11 | 1 | 7 | 0 | 0 | 0 | 3 | 0 | 2 | 0 | 0 | 56 |
| 7:00 | 0 | 46 | 11 | 0 | 11 | 0 | 0 | 0 | 5 | 0 | 0 | 1 | 0 | 74 |
| 8:00 | 0 | 36 | 4 | 1 | 10 | 0 | 0 | 2 | 6 | 0 | 0 | 0 | 0 | 59 |
| 9:00 | 0 | 12 | 1 | 1 | 6 | 0 | 0 | 1 | 5 | 0 | 0 | 0 | 0 | 26 |
| 10:00 | 1 | 5 | 6 | 3 | 9 | 0 | 0 | 1 | 3 | 0 | 1 | 0 | 0 | 29 |
| 11:00 | 0 | 7 | 1 | 0 | 5 | 0 | 0 | 0 | 7 | 0 | 1 | 0 | 0 | 21 |
| 12:00 PM | 1 | 11 | 4 | 0 | 9 | 1 | 0 | 1 | 6 | 0 | 0 | 1 | 0 | 34 |
| 13:00 | 0 | 39 | 7 | 1 | 8 | 0 | 0 | 1 | 8 | 0 | 0 | 0 | 0 | 64 |
| 14:00 | 0 | 38 | 10 | 5 | 6 | 0 | 0 | 0 | 6 | 0 | 0 | 1 | 0 | 66 |
| 15:00 | 0 | 14 | 5 | 2 | 11 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 35 |
| 16:00 | 0 | 0 | 2 | 0 | 4 | 0 | 0 | 2 | 6 | 0 | 1 | 0 | 0 | 15 |
| 17:00 | 0 | 29 | 6 | 1 | 5 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 42 |
| 18:00 | 0 | 4 | 1 | 0 | 0 | 1 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 10 |
| 19:00 | 0 | 4 | 2 | 1 | 1 | 0 | 0 | 0 | 5 | 0 | 0 | 0 | 0 | 13 |
| 20:00 | 0 | 5 | 2 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 8 |
| 21:00 | 0 | 15 | 3 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 20 |
| 22:00 | 0 | 16 | 3 | 0 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 21 |
| 23:00 | 0 | 19 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 20 |
| Totals | 3 | 411 | 92 | 16 | 122 | 2 | | 8 | 82 | | 5 | 3 | | 744 |
| % of Totals | 0% | 55% | 12% | 2% | 16% | 0% | | 1% | 11% | | 1% | 0% | | 100% |

132

| | | | | | | | | | | | | | | |
|--------------|-------|-------|-------|-------|-------|-------|---|-------|-------|---|-------|-------|---|-------|
| AM Volumes | 2 | 217 | 47 | 6 | 73 | 0 | 0 | 4 | 42 | 0 | 4 | 1 | 0 | 396 |
| % AM | 0% | 29% | 6% | 1% | 10% | | | 1% | 6% | | 1% | 0% | | 53% |
| AM Peak Hour | 10:00 | 5:00 | 6:00 | 10:00 | 5:00 | | | 8:00 | 5:00 | | 6:00 | 7:00 | | 5:00 |
| Volume | 1 | 48 | 11 | 3 | 15 | | | 2 | 7 | | 2 | 1 | | 77 |
| PM Volumes | 1 | 194 | 45 | 10 | 49 | 2 | 0 | 4 | 40 | 0 | 1 | 2 | 0 | 348 |
| % PM | 0% | 26% | 6% | 1% | 7% | 0% | | 1% | 5% | | 0% | 0% | | 47% |
| PM Peak Hour | 12:00 | 13:00 | 14:00 | 14:00 | 15:00 | 12:00 | | 16:00 | 13:00 | | 16:00 | 12:00 | | 14:00 |
| Volume | 1 | 39 | 10 | 5 | 11 | 1 | | 2 | 8 | | 1 | 1 | | 66 |

| Directional Peak Periods All Classes | AM 7-9 | | NOON 12-2 | | PM 4-6 | | Off Peak Volumes | |
|---|--------|-----|-----------|-----|--------|----|------------------|-----|
| | Volume | % | Volume | % | Volume | % | Volume | % |
| | 133 | 18% | 98 | 13% | 57 | 8% | 456 | 61% |

| Classification Definitions | | | | |
|-------------------------------|-------------------------------|----------------------------|-----------------------------|----------------------------|
| 1 Motorcycles | 4 Buses | 7 >=4-Axle Single Units | 10 >=6-Axle Single Trailers | 13 >=7-Axle Multi-Trailers |
| 2 Passenger Cars | 5 2-Axle, 6-Tire Single Units | 8 <=4-Axle Single Trailers | 11 <=5-Axle Multi-Trailers | |
| 3 2-Axle, 4-Tire Single Units | 6 3-Axle Single Units | 9 5-Axle Single Trailers | 12 6-Axle Multi-Trailers | |

CLASSIFICATION

Bystrum Rd S/O Keyes Rd

Day: Tuesday
Date: 10/4/2016

County: Stanislaus
Project #: CA16_7709_004n

North Bound

| Time | # 1 | # 2 | # 3 | # 4 | # 5 | # 6 | # 7 | # 8 | # 9 | # 10 | # 11 | # 12 | # 13 | Total |
|--------------------|-----------|------------|------------|-----------|-----------|-----------|-----|-----------|------------|------|-----------|-----------|------|-------------|
| 0:00 AM | 0 | 21 | 2 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 0 | 0 | 25 |
| 1:00 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 5 |
| 2:00 | 0 | 6 | 0 | 1 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 8 |
| 3:00 | 0 | 2 | 1 | 0 | 4 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 8 |
| 4:00 | 0 | 1 | 2 | 0 | 4 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 8 |
| 5:00 | 0 | 1 | 1 | 0 | 5 | 0 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 9 |
| 6:00 | 0 | 41 | 9 | 0 | 1 | 1 | 0 | 1 | 2 | 0 | 0 | 0 | 0 | 55 |
| 7:00 | 0 | 19 | 5 | 0 | 3 | 0 | 0 | 0 | 7 | 0 | 1 | 1 | 0 | 36 |
| 8:00 | 0 | 18 | 2 | 1 | 3 | 0 | 0 | 0 | 4 | 0 | 0 | 0 | 0 | 28 |
| 9:00 | 0 | 6 | 2 | 0 | 1 | 0 | 0 | 0 | 7 | 0 | 2 | 0 | 0 | 18 |
| 10:00 | 0 | 8 | 7 | 1 | 3 | 0 | 0 | 0 | 7 | 0 | 0 | 0 | 0 | 26 |
| 11:00 | 0 | 13 | 7 | 0 | 3 | 0 | 0 | 1 | 5 | 0 | 1 | 0 | 0 | 30 |
| 12:00 PM | 0 | 20 | 4 | 0 | 1 | 0 | 0 | 0 | 6 | 0 | 1 | 1 | 0 | 33 |
| 13:00 | 0 | 11 | 7 | 0 | 3 | 3 | 0 | 0 | 4 | 0 | 0 | 1 | 0 | 29 |
| 14:00 | 0 | 50 | 14 | 0 | 8 | 0 | 0 | 0 | 6 | 0 | 0 | 0 | 0 | 78 |
| 15:00 | 1 | 48 | 11 | 1 | 2 | 0 | 0 | 0 | 2 | 0 | 0 | 1 | 0 | 66 |
| 16:00 | 0 | 36 | 8 | 0 | 7 | 0 | 0 | 0 | 9 | 0 | 1 | 0 | 0 | 61 |
| 17:00 | 0 | 46 | 7 | 0 | 1 | 0 | 0 | 0 | 7 | 0 | 1 | 0 | 0 | 62 |
| 18:00 | 0 | 41 | 8 | 0 | 0 | 0 | 0 | 1 | 3 | 0 | 0 | 0 | 0 | 53 |
| 19:00 | 0 | 14 | 2 | 0 | 2 | 0 | 0 | 0 | 3 | 0 | 0 | 0 | 0 | 21 |
| 20:00 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 0 | 0 | 11 |
| 21:00 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 4 |
| 22:00 | 0 | 17 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 19 |
| 23:00 | 0 | 38 | 1 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 0 | 0 | 40 |
| Totals | 1 | 468 | 102 | 4 | 51 | 4 | | 5 | 86 | | 8 | 4 | | 733 |
| % of Totals | 0% | 64% | 14% | 1% | 7% | 1% | | 1% | 12% | | 1% | 1% | | 100% |

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| | | | | | | | | | | | | | | | | |
|---------------------------------|-------|---------------|-------|-------|-------|------------------|---|-------|---------------|--------|-------|-------------------------|-----|--------|---|--|
| AM Volumes | 0 | 139 | 38 | 3 | 27 | 1 | 0 | 4 | 38 | 0 | 5 | 1 | 0 | 256 | | |
| % AM | | 19% | 5% | 0% | 4% | 0% | | 1% | 5% | | 1% | 0% | | 35% | | |
| AM Peak Hour | | 6:00 | 6:00 | 2:00 | 5:00 | 6:00 | | 2:00 | 7:00 | | 9:00 | 7:00 | | 6:00 | | |
| Volume | | 41 | 9 | 1 | 5 | 1 | | 1 | 7 | | 2 | 1 | | 55 | | |
| PM Volumes | 1 | 329 | 64 | 1 | 24 | 3 | 0 | 1 | 48 | 0 | 3 | 3 | 0 | 477 | | |
| % PM | 0% | 45% | 9% | 0% | 3% | 0% | | 0% | 7% | | 0% | 0% | | 65% | | |
| PM Peak Hour | 15:00 | 14:00 | 14:00 | 15:00 | 14:00 | 13:00 | | 18:00 | 16:00 | | 12:00 | 12:00 | | 14:00 | | |
| Volume | 1 | 50 | 14 | 1 | 8 | 3 | | 1 | 9 | | 1 | 1 | | 78 | | |
| Directional Peak Periods | | AM 7-9 | | | | NOON 12-2 | | | PM 4-6 | | | Off Peak Volumes | | | | |
| All Classes | | Volume | | | % | Volume | | | % | Volume | | | % | Volume | | |
| | | 64 | ↔ | | 9% | 62 | ↔ | | 8% | 123 | ↔ | | 17% | 484 | ↔ | |

| Classification Definitions | | | | |
|-------------------------------|-------------------------------|----------------------------|-----------------------------|----------------------------|
| 1 Motorcycles | 4 Buses | 7 >=4-Axle Single Units | 10 >=6-Axle Single Trailers | 13 >=7-Axle Multi-Trailers |
| 2 Passenger Cars | 5 2-Axle, 6-Tire Single Units | 8 <=4-Axle Single Trailers | 11 <=5-Axle Multi-Trailers | |
| 3 2-Axle, 4-Tire Single Units | 6 3-Axle Single Units | 9 5-Axle Single Trailers | 12 6-Axle Multi-Trailers | |

VOLUME
 Bystrom Rd S/O Keyes Rd

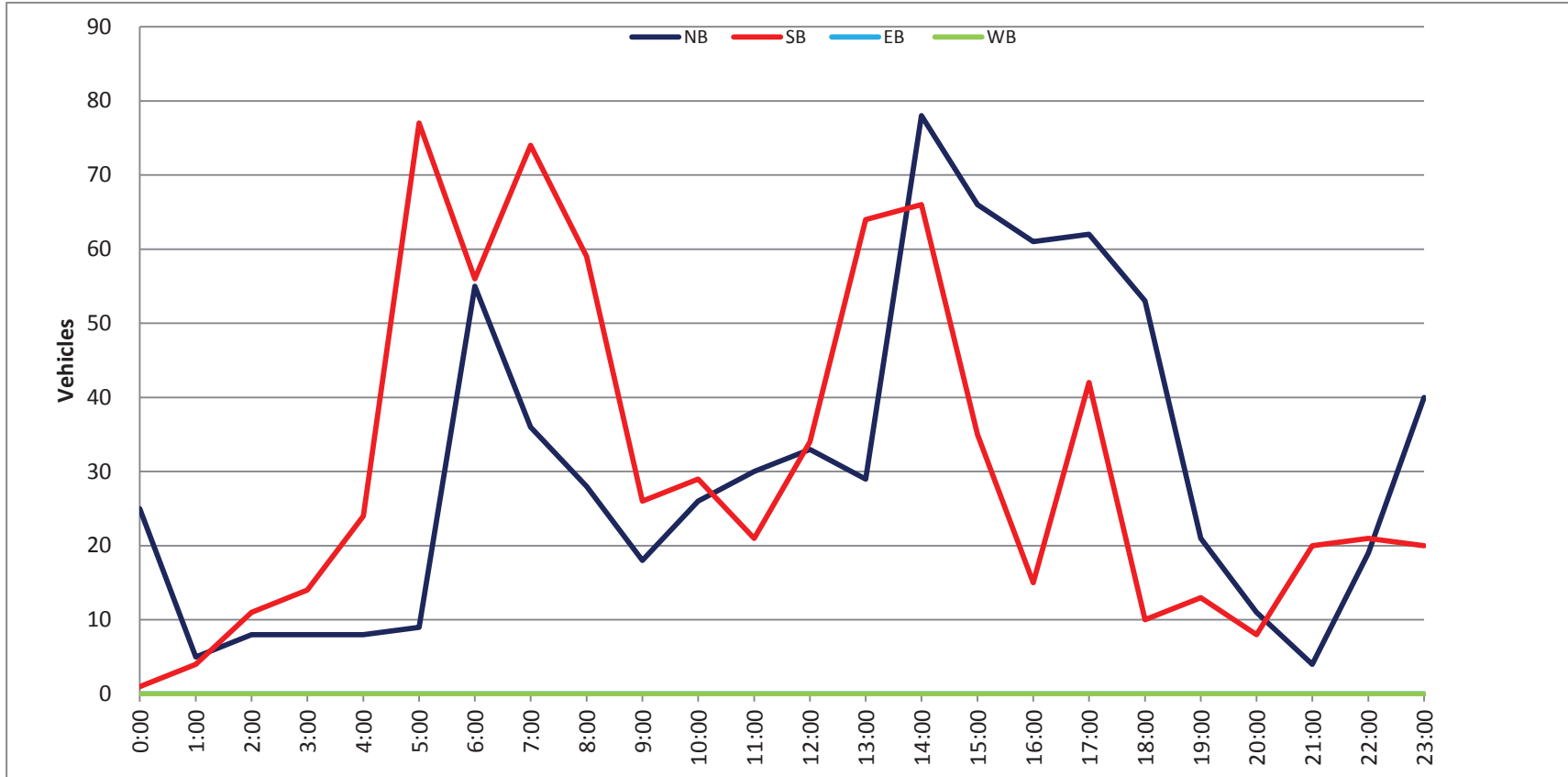
Day: Tuesday
 Date: 10/4/2016

County: Stanislaus
 Project #: CA16_7709_004

| DAILY TOTALS | | | | | NB | SB | EB | WB | Total | | | | |
|----------------|-------|-------|----|----|-------|----------------|-------|-------|-------|----|-------|----|-----|
| | | | | | 733 | 744 | 0 | 0 | 1,477 | | | | |
| AM Period | NB | SB | EB | WB | TOTAL | PM Period | NB | SB | EB | WB | TOTAL | | |
| 0:00 | 19 | 0 | 0 | 0 | 19 | 12:00 | 9 | 8 | 0 | 0 | 17 | | |
| 0:15 | 3 | 1 | 0 | 0 | 4 | 12:15 | 9 | 8 | 0 | 0 | 17 | | |
| 0:30 | 3 | 0 | 0 | 0 | 3 | 12:30 | 9 | 8 | 0 | 0 | 17 | | |
| 0:45 | 0 | 25 | 0 | 1 | 0 | 12:45 | 6 | 33 | 10 | 34 | 16 | 67 | |
| 1:00 | 3 | 0 | 0 | 0 | 3 | 13:00 | 6 | 11 | 0 | 0 | 17 | | |
| 1:15 | 0 | 0 | 0 | 0 | 0 | 13:15 | 8 | 14 | 0 | 0 | 22 | | |
| 1:30 | 2 | 1 | 0 | 0 | 3 | 13:30 | 8 | 20 | 0 | 0 | 28 | | |
| 1:45 | 0 | 5 | 3 | 4 | 0 | 13:45 | 7 | 29 | 19 | 64 | 0 | 26 | 93 |
| 2:00 | 4 | 1 | 0 | 0 | 5 | 14:00 | 12 | 9 | 0 | 0 | 21 | | |
| 2:15 | 2 | 3 | 0 | 0 | 5 | 14:15 | 12 | 15 | 0 | 0 | 27 | | |
| 2:30 | 1 | 2 | 0 | 0 | 3 | 14:30 | 41 | 18 | 0 | 0 | 59 | | |
| 2:45 | 1 | 8 | 5 | 11 | 0 | 14:45 | 13 | 78 | 24 | 66 | 0 | 37 | 144 |
| 3:00 | 1 | 2 | 0 | 0 | 3 | 15:00 | 19 | 10 | 0 | 0 | 29 | | |
| 3:15 | 3 | 3 | 0 | 0 | 6 | 15:15 | 14 | 11 | 0 | 0 | 25 | | |
| 3:30 | 3 | 3 | 0 | 0 | 6 | 15:30 | 22 | 6 | 0 | 0 | 28 | | |
| 3:45 | 1 | 8 | 6 | 14 | 0 | 15:45 | 11 | 66 | 8 | 35 | 0 | 19 | 101 |
| 4:00 | 1 | 2 | 0 | 0 | 3 | 16:00 | 28 | 4 | 0 | 0 | 32 | | |
| 4:15 | 3 | 5 | 0 | 0 | 8 | 16:15 | 12 | 7 | 0 | 0 | 19 | | |
| 4:30 | 2 | 6 | 0 | 0 | 8 | 16:30 | 11 | 3 | 0 | 0 | 14 | | |
| 4:45 | 2 | 8 | 11 | 24 | 0 | 16:45 | 10 | 61 | 1 | 15 | 0 | 11 | 76 |
| 5:00 | 3 | 3 | 0 | 0 | 6 | 17:00 | 27 | 2 | 0 | 0 | 29 | | |
| 5:15 | 2 | 11 | 0 | 0 | 13 | 17:15 | 15 | 8 | 0 | 0 | 23 | | |
| 5:30 | 3 | 18 | 0 | 0 | 21 | 17:30 | 13 | 20 | 0 | 0 | 33 | | |
| 5:45 | 1 | 9 | 45 | 77 | 0 | 17:45 | 7 | 62 | 12 | 42 | 0 | 19 | 104 |
| 6:00 | 4 | 9 | 0 | 0 | 13 | 18:00 | 9 | 3 | 0 | 0 | 12 | | |
| 6:15 | 7 | 8 | 0 | 0 | 15 | 18:15 | 8 | 2 | 0 | 0 | 10 | | |
| 6:30 | 32 | 16 | 0 | 0 | 48 | 18:30 | 32 | 3 | 0 | 0 | 35 | | |
| 6:45 | 12 | 55 | 23 | 56 | 0 | 18:45 | 4 | 53 | 2 | 10 | 0 | 6 | 63 |
| 7:00 | 3 | 14 | 0 | 0 | 17 | 19:00 | 8 | 4 | 0 | 0 | 12 | | |
| 7:15 | 15 | 21 | 0 | 0 | 36 | 19:15 | 4 | 3 | 0 | 0 | 7 | | |
| 7:30 | 8 | 16 | 0 | 0 | 24 | 19:30 | 5 | 5 | 0 | 0 | 10 | | |
| 7:45 | 10 | 36 | 23 | 74 | 0 | 19:45 | 4 | 21 | 1 | 13 | 0 | 5 | 34 |
| 8:00 | 12 | 20 | 0 | 0 | 32 | 20:00 | 4 | 3 | 0 | 0 | 7 | | |
| 8:15 | 3 | 18 | 0 | 0 | 21 | 20:15 | 2 | 1 | 0 | 0 | 3 | | |
| 8:30 | 11 | 10 | 0 | 0 | 21 | 20:30 | 4 | 3 | 0 | 0 | 7 | | |
| 8:45 | 2 | 28 | 11 | 59 | 0 | 20:45 | 1 | 11 | 1 | 8 | 0 | 2 | 19 |
| 9:00 | 5 | 3 | 0 | 0 | 8 | 21:00 | 1 | 2 | 0 | 0 | 3 | | |
| 9:15 | 5 | 7 | 0 | 0 | 12 | 21:15 | 0 | 1 | 0 | 0 | 1 | | |
| 9:30 | 5 | 6 | 0 | 0 | 11 | 21:30 | 1 | 6 | 0 | 0 | 7 | | |
| 9:45 | 3 | 18 | 10 | 26 | 0 | 21:45 | 2 | 4 | 11 | 20 | 0 | 13 | 24 |
| 10:00 | 9 | 7 | 0 | 0 | 16 | 22:00 | 1 | 3 | 0 | 0 | 4 | | |
| 10:15 | 4 | 6 | 0 | 0 | 10 | 22:15 | 2 | 2 | 0 | 0 | 4 | | |
| 10:30 | 8 | 2 | 0 | 0 | 10 | 22:30 | 10 | 11 | 0 | 0 | 21 | | |
| 10:45 | 5 | 26 | 14 | 29 | 0 | 22:45 | 6 | 19 | 5 | 21 | 0 | 11 | 40 |
| 11:00 | 10 | 3 | 0 | 0 | 13 | 23:00 | 3 | 4 | 0 | 0 | 7 | | |
| 11:15 | 4 | 5 | 0 | 0 | 9 | 23:15 | 5 | 10 | 0 | 0 | 15 | | |
| 11:30 | 11 | 9 | 0 | 0 | 20 | 23:30 | 28 | 2 | 0 | 0 | 30 | | |
| 11:45 | 5 | 30 | 4 | 21 | 0 | 23:45 | 4 | 40 | 4 | 20 | 0 | 8 | 60 |
| TOTALS | 256 | 396 | | | 652 | TOTALS | 477 | 348 | | | 825 | | |
| SPLIT % | 39.3% | 60.7% | | | 44.1% | SPLIT % | 57.8% | 42.2% | | | 55.9% | | |

| DAILY TOTALS | | | | | NB | SB | EB | WB | Total | | |
|-----------------|-------|-------|-------|-------|-------|-----------------|-------|-------|-------|-------|-------|
| | | | | | 733 | 744 | 0 | 0 | 1,477 | | |
| AM Peak Hour | 6:30 | 5:15 | | | 6:30 | PM Peak Hour | 15:00 | 14:45 | 14:45 | | |
| AM Pk Volume | 62 | 83 | | | 136 | PM Pk Volume | 87 | 67 | 152 | | |
| Pk Hr Factor | 0.484 | 0.461 | | | 0.708 | Pk Hr Factor | 0.750 | 0.531 | 0.804 | | |
| 7 - 9 Volume | 64 | 133 | 0 | 0 | 197 | 4 - 6 Volume | 123 | 57 | 0 | 0 | 180 |
| 7 - 9 Peak Hour | 7:15 | 7:15 | | | 7:15 | 4 - 6 Peak Hour | 16:45 | 17:00 | | | 17:00 |
| 7 - 9 Pk Volume | 45 | 80 | 0 | 0 | 125 | 4 - 6 Pk Volume | 65 | 42 | 0 | 0 | 104 |
| Pk Hr Factor | 0.750 | 0.870 | 0.000 | 0.000 | 0.868 | Pk Hr Factor | 0.602 | 0.525 | 0.000 | 0.000 | 0.788 |

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CLASSIFICATION

East Entrance to Bronco Winery S/O Keyes Rd

Day: Tuesday
Date: 10/4/2016

County: Stanislaus
Project #: CA16_7709_003

Summary

| Time | # 1 | # 2 | # 3 | # 4 | # 5 | # 6 | # 7 | # 8 | # 9 | # 10 | # 11 | # 12 | # 13 | Total |
|---------------|-----|-----------|-----|-----|-----|-----|-----|-----|-----|------|------------|-----------|------|------------|
| 0:00 AM | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 7 |
| 1:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 9 | 0 | 0 | 9 |
| 2:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 11 | 0 | 0 | 11 |
| 3:00 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 13 | 0 | 0 | 14 |
| 4:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 4 |
| 5:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 7 |
| 6:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 |
| 7:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 |
| 8:00 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 4 |
| 9:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 1 | 0 | 5 |
| 10:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 9 | 3 | 0 | 12 |
| 11:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | 3 | 0 | 11 |
| 12:00 PM | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 4 |
| 13:00 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 13 | 0 | 14 |
| 14:00 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 12 | 0 | 15 |
| 15:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 2 | 0 | 3 |
| 16:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 |
| 17:00 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 8 |
| 18:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 2 | 0 | 8 |
| 19:00 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 5 |
| 20:00 | 0 | 4 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 9 |
| 21:00 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 10 | 0 | 0 | 11 |
| 22:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 10 | 1 | 0 | 11 |
| 23:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 7 |
| Totals | | 16 | | | | | | | | | 120 | 49 | | 185 |
| % of Totals | | 9% | | | | | | | | | 65% | 26% | | 100% |

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| | | | | | | | | | | | | | | | |
|---------------------------------|---|---------------|---|----|--------|------------------|-----|--------|---|---------------|--------|-------|-----|-------------------------|--|
| AM Volumes | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 66 | 18 | 0 | 87 | |
| % AM | | 2% | | | | | | | | | 36% | 10% | | 47% | |
| AM Peak Hour | | 8:00 | | | | | | | | | 3:00 | 5:00 | | 3:00 | |
| Volume | | 2 | | | | | | | | | 13 | 7 | | 14 | |
| PM Volumes | 0 | 13 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 54 | 31 | 0 | 98 | |
| % PM | | 7% | | | | | | | | | 29% | 17% | | 53% | |
| PM Peak Hour | | 20:00 | | | | | | | | | 21:00 | 13:00 | | 14:00 | |
| Volume | | 4 | | | | | | | | | 10 | 13 | | 15 | |
| Directional Peak Periods | | AM 7-9 | | | | NOON 12-2 | | | | PM 4-6 | | | | Off Peak Volumes | |
| All Classes | | Volume | | % | Volume | | % | Volume | | % | Volume | | % | | |
| | | 6 | ↔ | 3% | 18 | ↔ | 10% | 11 | ↔ | 6% | 150 | ↔ | 81% | | |

| Classification Definitions | | | | |
|-------------------------------|-------------------------------|----------------------------|-----------------------------|----------------------------|
| 1 Motorcycles | 4 Buses | 7 >=4-Axle Single Units | 10 >=6-Axle Single Trailers | 13 >=7-Axle Multi-Trailers |
| 2 Passenger Cars | 5 2-Axle, 6-Tire Single Units | 8 <=4-Axle Single Trailers | 11 <=5-Axle Multi-Trailers | |
| 3 2-Axle, 4-Tire Single Units | 6 3-Axle Single Units | 9 5-Axle Single Trailers | 12 6-Axle Multi-Trailers | |

CLASSIFICATION

East Entrance to Bronco Winery S/O Keyes Rd

Day: Tuesday
Date: 10/4/2016

County: Stanislaus
Project #: CA16_7709_003s

South Bound

| Time | # 1 | # 2 | # 3 | # 4 | # 5 | # 6 | # 7 | # 8 | # 9 | # 10 | # 11 | # 12 | # 13 | Total |
|--------------------|-----|-----|-----|-----|-----|-----|-----|-----|-----|------|------|------|------|-------|
| 0:00 AM | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 |
| 1:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 0 | 5 |
| 2:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 8 | 0 | 0 | 8 |
| 3:00 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 5 |
| 4:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 |
| 5:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 |
| 6:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| 7:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 |
| 8:00 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 |
| 9:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 1 | 0 | 4 |
| 10:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 1 | 0 | 5 |
| 11:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 2 | 0 | 6 |
| 12:00 PM | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| 13:00 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 8 |
| 14:00 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 8 |
| 15:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 |
| 16:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 2 |
| 17:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 |
| 18:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 1 | 0 | 4 |
| 19:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 2 |
| 20:00 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 6 |
| 21:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 |
| 22:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 6 |
| 23:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 4 |
| Totals | | 8 | | | | | | | | | 61 | 23 | | 92 |
| % of Totals | | 9% | | | | | | | | | 66% | 25% | | 100% |

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| | | | | | | | | | | | | | | | | |
|---------------------------------|---|---------------|---|---|----|------------------|---|---|----|---------------|-------|-------|----|-------------------------|---|--|
| AM Volumes | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 34 | 8 | 0 | 45 | | |
| % AM | | 3% | | | | | | | | | 37% | 9% | | 49% | | |
| AM Peak Hour | | 8:00 | | | | | | | | | 2:00 | 5:00 | | 2:00 | | |
| Volume | | 2 | | | | | | | | | 8 | 2 | | 8 | | |
| PM Volumes | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 27 | 15 | 0 | 47 | | |
| % PM | | 5% | | | | | | | | | 29% | 16% | | 51% | | |
| PM Peak Hour | | 14:00 | | | | | | | | | 22:00 | 13:00 | | 13:00 | | |
| Volume | | 2 | | | | | | | | | 6 | 7 | | 8 | | |
| Directional Peak Periods | | AM 7-9 | | | | NOON 12-2 | | | | PM 4-6 | | | | Off Peak Volumes | | |
| All Classes | | Volume | | | % | Volume | | | % | Volume | | | % | Volume | | |
| | | 4 | ↔ | | 4% | 8 | ↔ | | 9% | 5 | ↔ | | 5% | 75 | ↔ | |

| Classification Definitions | | | | |
|-------------------------------|-------------------------------|----------------------------|-----------------------------|----------------------------|
| 1 Motorcycles | 4 Buses | 7 >=4-Axle Single Units | 10 >=6-Axle Single Trailers | 13 >=7-Axle Multi-Trailers |
| 2 Passenger Cars | 5 2-Axle, 6-Tire Single Units | 8 <=4-Axle Single Trailers | 11 <=5-Axle Multi-Trailers | |
| 3 2-Axle, 4-Tire Single Units | 6 3-Axle Single Units | 9 5-Axle Single Trailers | 12 6-Axle Multi-Trailers | |

CLASSIFICATION

East Entrance to Bronco Winery S/O Keyes Rd

Day: Tuesday
Date: 10/4/2016

County: Stanislaus
Project #: CA16_7709_003n

North Bound

| Time | # 1 | # 2 | # 3 | # 4 | # 5 | # 6 | # 7 | # 8 | # 9 | # 10 | # 11 | # 12 | # 13 | Total |
|--------------------|-----|-----|-----|-----|-----|-----|-----|-----|-----|------|------|------|------|-------|
| 0:00 AM | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 4 |
| 1:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 4 |
| 2:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 |
| 3:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 9 | 0 | 0 | 9 |
| 4:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 |
| 5:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 0 | 5 |
| 6:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 |
| 7:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |
| 8:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 |
| 9:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 |
| 10:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 5 | 2 | 0 | 7 |
| 11:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 1 | 0 | 5 |
| 12:00 PM | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 4 |
| 13:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 6 |
| 14:00 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 7 |
| 15:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 1 | 0 | 2 |
| 16:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 |
| 17:00 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 0 | 0 | 5 |
| 18:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 1 | 0 | 4 |
| 19:00 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 0 | 3 |
| 20:00 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 3 |
| 21:00 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 7 | 0 | 0 | 8 |
| 22:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 4 | 1 | 0 | 5 |
| 23:00 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 3 |
| Totals | | 8 | | | | | | | | | 59 | 26 | | 93 |
| % of Totals | | 9% | | | | | | | | | 63% | 28% | | 100% |

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| | | | | | | | | | | | | | | | | |
|---------------------------------|---|---------------|---|---|----|------------------|---|---|-----|---------------|-------|-------|----|-------------------------|---|-----|
| AM Volumes | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 32 | 10 | 0 | 42 | | |
| % AM | | | | | | | | | | | 34% | 11% | | 45% | | |
| AM Peak Hour | | | | | | | | | | | 3:00 | 5:00 | | 3:00 | | |
| Volume | | | | | | | | | | | 9 | 5 | | 9 | | |
| PM Volumes | 0 | 8 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 27 | 16 | 0 | 51 | | |
| % PM | | 9% | | | | | | | | | 29% | 17% | | 55% | | |
| PM Peak Hour | | 12:00 | | | | | | | | | 21:00 | 13:00 | | 21:00 | | |
| Volume | | 2 | | | | | | | | | 7 | 6 | | 8 | | |
| Directional Peak Periods | | AM 7-9 | | | | NOON 12-2 | | | | PM 4-6 | | | | Off Peak Volumes | | |
| All Classes | | Volume | | | % | Volume | | | % | Volume | | | % | Volume | % | |
| | | 2 | ↔ | | 2% | 10 | ↔ | | 11% | 6 | ↔ | | 6% | 75 | ↔ | 81% |

| Classification Definitions | | | | |
|-------------------------------|-------------------------------|----------------------------|-----------------------------|----------------------------|
| 1 Motorcycles | 4 Buses | 7 >=4-Axle Single Units | 10 >=6-Axle Single Trailers | 13 >=7-Axle Multi-Trailers |
| 2 Passenger Cars | 5 2-Axle, 6-Tire Single Units | 8 <=4-Axle Single Trailers | 11 <=5-Axle Multi-Trailers | |
| 3 2-Axle, 4-Tire Single Units | 6 3-Axle Single Units | 9 5-Axle Single Trailers | 12 6-Axle Multi-Trailers | |

VOLUME

East Entrance to Bronco Winery S/O Keys Rd

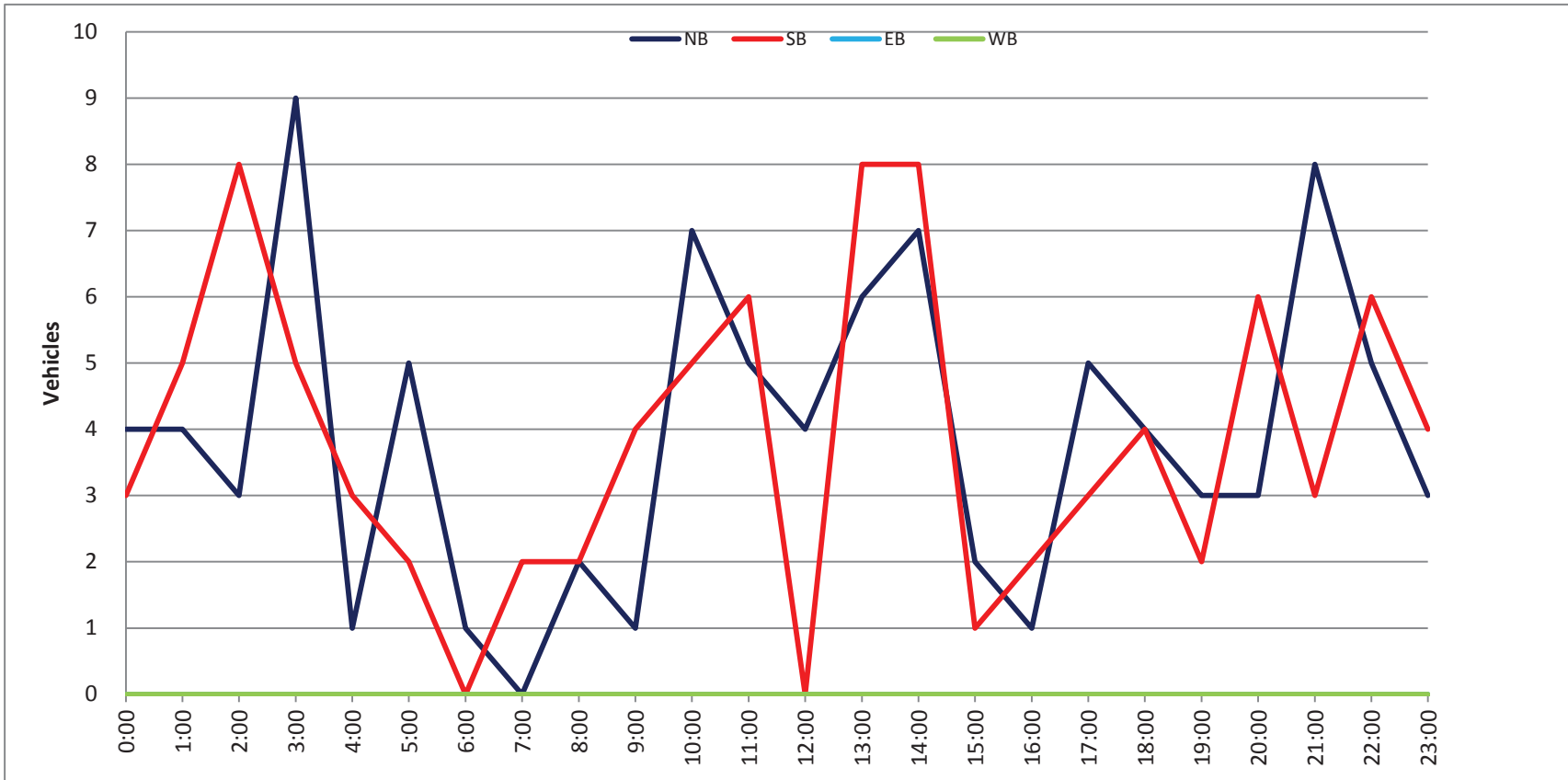
Day: Tuesday
 Date: 10/4/2016

County: Stanislaus
 Project #: CA16_7709_003

| DAILY TOTALS | | | | | NB | SB | EB | WB | Total | | |
|----------------|--------------|--------------|-------------|-------------|--------------|----------------|--------------|--------------|-------------|-------------|--------------|
| | | | | | 93 | 92 | 0 | 0 | 185 | | |
| AM Period | NB | SB | EB | WB | TOTAL | PM Period | NB | SB | EB | WB | TOTAL |
| 0:00 | 0 | 2 | 0 | 0 | 2 | 12:00 | 3 | 0 | 0 | 0 | 3 |
| 0:15 | 4 | 0 | 0 | 0 | 4 | 12:15 | 1 | 0 | 0 | 0 | 1 |
| 0:30 | 0 | 0 | 0 | 0 | 0 | 12:30 | 0 | 0 | 0 | 0 | 0 |
| 0:45 | 0 | 4 | 1 | 3 | 8 | 12:45 | 0 | 4 | 0 | 0 | 4 |
| 1:00 | 1 | 2 | 0 | 0 | 3 | 13:00 | 0 | 1 | 0 | 0 | 1 |
| 1:15 | 3 | 2 | 0 | 0 | 5 | 13:15 | 0 | 3 | 0 | 0 | 3 |
| 1:30 | 0 | 0 | 0 | 0 | 0 | 13:30 | 1 | 3 | 0 | 0 | 4 |
| 1:45 | 0 | 4 | 1 | 5 | 10 | 13:45 | 5 | 6 | 1 | 8 | 20 |
| 2:00 | 0 | 2 | 0 | 0 | 2 | 14:00 | 1 | 3 | 0 | 0 | 4 |
| 2:15 | 2 | 0 | 0 | 0 | 2 | 14:15 | 2 | 0 | 0 | 0 | 2 |
| 2:30 | 0 | 1 | 0 | 0 | 1 | 14:30 | 0 | 3 | 0 | 0 | 3 |
| 2:45 | 1 | 3 | 5 | 8 | 17 | 14:45 | 4 | 7 | 2 | 8 | 21 |
| 3:00 | 6 | 4 | 0 | 0 | 10 | 15:00 | 1 | 0 | 0 | 0 | 1 |
| 3:15 | 0 | 0 | 0 | 0 | 0 | 15:15 | 0 | 1 | 0 | 0 | 1 |
| 3:30 | 3 | 1 | 0 | 0 | 4 | 15:30 | 1 | 0 | 0 | 0 | 1 |
| 3:45 | 0 | 9 | 0 | 5 | 14 | 15:45 | 0 | 2 | 0 | 1 | 3 |
| 4:00 | 0 | 0 | 0 | 0 | 0 | 16:00 | 0 | 0 | 0 | 0 | 0 |
| 4:15 | 0 | 0 | 0 | 0 | 0 | 16:15 | 0 | 0 | 0 | 0 | 0 |
| 4:30 | 0 | 2 | 0 | 0 | 2 | 16:30 | 0 | 1 | 0 | 0 | 1 |
| 4:45 | 1 | 1 | 1 | 3 | 6 | 16:45 | 1 | 1 | 1 | 2 | 5 |
| 5:00 | 1 | 0 | 0 | 0 | 1 | 17:00 | 1 | 1 | 0 | 0 | 2 |
| 5:15 | 1 | 1 | 0 | 0 | 2 | 17:15 | 2 | 1 | 0 | 0 | 3 |
| 5:30 | 1 | 0 | 0 | 0 | 1 | 17:30 | 1 | 1 | 0 | 0 | 2 |
| 5:45 | 2 | 5 | 1 | 2 | 10 | 17:45 | 1 | 5 | 0 | 3 | 9 |
| 6:00 | 0 | 0 | 0 | 0 | 0 | 18:00 | 0 | 1 | 0 | 0 | 1 |
| 6:15 | 1 | 0 | 0 | 0 | 1 | 18:15 | 1 | 2 | 0 | 0 | 3 |
| 6:30 | 0 | 0 | 0 | 0 | 0 | 18:30 | 2 | 1 | 0 | 0 | 3 |
| 6:45 | 0 | 1 | 0 | 0 | 1 | 18:45 | 1 | 4 | 0 | 4 | 9 |
| 7:00 | 0 | 0 | 0 | 0 | 0 | 19:00 | 1 | 1 | 0 | 0 | 2 |
| 7:15 | 0 | 1 | 0 | 0 | 1 | 19:15 | 1 | 1 | 0 | 0 | 2 |
| 7:30 | 0 | 1 | 0 | 0 | 1 | 19:30 | 1 | 0 | 0 | 0 | 1 |
| 7:45 | 0 | 0 | 2 | 0 | 2 | 19:45 | 0 | 3 | 0 | 2 | 5 |
| 8:00 | 2 | 1 | 0 | 0 | 3 | 20:00 | 0 | 0 | 0 | 0 | 0 |
| 8:15 | 0 | 1 | 0 | 0 | 1 | 20:15 | 0 | 0 | 0 | 0 | 0 |
| 8:30 | 0 | 0 | 0 | 0 | 0 | 20:30 | 2 | 2 | 0 | 0 | 4 |
| 8:45 | 0 | 2 | 0 | 2 | 4 | 20:45 | 1 | 3 | 4 | 6 | 14 |
| 9:00 | 0 | 0 | 0 | 0 | 0 | 21:00 | 4 | 1 | 0 | 0 | 5 |
| 9:15 | 0 | 0 | 0 | 0 | 0 | 21:15 | 1 | 0 | 0 | 0 | 1 |
| 9:30 | 0 | 1 | 0 | 0 | 1 | 21:30 | 0 | 2 | 0 | 0 | 2 |
| 9:45 | 1 | 1 | 3 | 4 | 9 | 21:45 | 3 | 8 | 0 | 3 | 14 |
| 10:00 | 0 | 2 | 0 | 0 | 2 | 22:00 | 0 | 2 | 0 | 0 | 2 |
| 10:15 | 2 | 1 | 0 | 0 | 3 | 22:15 | 1 | 2 | 0 | 0 | 3 |
| 10:30 | 2 | 1 | 0 | 0 | 3 | 22:30 | 3 | 1 | 0 | 0 | 4 |
| 10:45 | 3 | 7 | 1 | 5 | 16 | 22:45 | 1 | 5 | 1 | 6 | 13 |
| 11:00 | 1 | 3 | 0 | 0 | 4 | 23:00 | 0 | 0 | 0 | 0 | 0 |
| 11:15 | 1 | 0 | 0 | 0 | 1 | 23:15 | 0 | 1 | 0 | 0 | 1 |
| 11:30 | 2 | 1 | 0 | 0 | 3 | 23:30 | 1 | 0 | 0 | 0 | 1 |
| 11:45 | 1 | 5 | 2 | 6 | 14 | 23:45 | 2 | 3 | 3 | 4 | 12 |
| TOTALS | 42 | 45 | 0 | 0 | 87 | TOTALS | 51 | 47 | 0 | 0 | 98 |
| SPLIT % | 48.3% | 51.7% | 0.0% | 0.0% | 47.0% | SPLIT % | 52.0% | 48.0% | 0.0% | 0.0% | 53.0% |

| DAILY TOTALS | | | | | NB | SB | EB | WB | Total |
|-----------------|-------|-------|-------|-------|-----------------|-------|-------|-------|-------|
| | | | | | 93 | 92 | 0 | 0 | 185 |
| AM Peak Hour | 2:45 | 2:15 | | 2:45 | PM Peak Hour | 14:00 | 13:45 | | 13:45 |
| AM Pk Volume | 10 | 10 | | 20 | PM Pk Volume | 9 | 10 | | 17 |
| Pk Hr Factor | 0.417 | 0.500 | | 0.500 | Pk Hr Factor | 0.438 | 0.583 | | 0.625 |
| 7 - 9 Volume | 2 | 4 | 0 | 6 | 4 - 6 Volume | 6 | 5 | 0 | 11 |
| 7 - 9 Peak Hour | 7:15 | 7:15 | | 7:15 | 4 - 6 Peak Hour | 16:45 | 16:30 | | 16:45 |
| 7 - 9 Pk Volume | 2 | 3 | 0 | 5 | 4 - 6 Pk Volume | 5 | 4 | 0 | 9 |
| Pk Hr Factor | 0.250 | 0.750 | 0.000 | 0.417 | Pk Hr Factor | 0.625 | 1.000 | 0.000 | 0.750 |

140



CLASSIFICATION

Keyes Rd W/O Bystrum Rd

Day: Tuesday
Date: 10/4/2016

County: Stanislaus
Project #: CA16_7709_001

Summary

| Time | # 1 | # 2 | # 3 | # 4 | # 5 | # 6 | # 7 | # 8 | # 9 | # 10 | # 11 | # 12 | # 13 | Total |
|--------------------|-----------|-------------|------------|-----------|------------|-----------|-----------|------------|------------|-----------|------------|-----------|------|-------------|
| 0:00 AM | 0 | 26 | 2 | 0 | 5 | 0 | 0 | 2 | 5 | 0 | 5 | 1 | 0 | 46 |
| 1:00 | 0 | 12 | 2 | 0 | 5 | 0 | 0 | 4 | 4 | 0 | 5 | 3 | 0 | 35 |
| 2:00 | 0 | 20 | 4 | 1 | 2 | 0 | 0 | 3 | 3 | 0 | 4 | 2 | 0 | 39 |
| 3:00 | 1 | 37 | 15 | 4 | 11 | 1 | 0 | 6 | 2 | 0 | 1 | 4 | 0 | 82 |
| 4:00 | 0 | 88 | 31 | 4 | 38 | 2 | 0 | 4 | 10 | 0 | 4 | 1 | 0 | 182 |
| 5:00 | 1 | 187 | 53 | 1 | 65 | 2 | 0 | 5 | 20 | 0 | 1 | 2 | 0 | 337 |
| 6:00 | 2 | 191 | 52 | 1 | 60 | 1 | 0 | 11 | 21 | 0 | 3 | 3 | 0 | 345 |
| 7:00 | 1 | 265 | 62 | 4 | 64 | 5 | 0 | 9 | 25 | 0 | 4 | 3 | 0 | 442 |
| 8:00 | 2 | 204 | 43 | 5 | 69 | 4 | 0 | 7 | 21 | 0 | 7 | 2 | 0 | 364 |
| 9:00 | 2 | 101 | 48 | 2 | 46 | 3 | 0 | 7 | 24 | 0 | 10 | 1 | 0 | 244 |
| 10:00 | 2 | 100 | 37 | 1 | 43 | 3 | 0 | 12 | 17 | 0 | 6 | 3 | 0 | 224 |
| 11:00 | 3 | 104 | 41 | 0 | 63 | 4 | 0 | 8 | 16 | 0 | 7 | 1 | 0 | 247 |
| 12:00 PM | 0 | 127 | 40 | 2 | 57 | 1 | 0 | 9 | 27 | 0 | 5 | 1 | 0 | 269 |
| 13:00 | 1 | 156 | 51 | 1 | 51 | 4 | 0 | 13 | 23 | 0 | 10 | 5 | 0 | 315 |
| 14:00 | 5 | 206 | 60 | 4 | 49 | 7 | 0 | 6 | 28 | 0 | 14 | 1 | 0 | 380 |
| 15:00 | 2 | 257 | 94 | 6 | 64 | 3 | 0 | 9 | 19 | 0 | 8 | 0 | 0 | 462 |
| 16:00 | 1 | 358 | 85 | 4 | 65 | 3 | 1 | 10 | 20 | 1 | 8 | 0 | 0 | 556 |
| 17:00 | 1 | 402 | 96 | 5 | 68 | 6 | 2 | 7 | 10 | 0 | 7 | 0 | 0 | 604 |
| 18:00 | 2 | 230 | 59 | 0 | 33 | 1 | 1 | 4 | 17 | 0 | 10 | 1 | 0 | 358 |
| 19:00 | 1 | 116 | 40 | 0 | 29 | 0 | 0 | 2 | 13 | 0 | 6 | 0 | 0 | 207 |
| 20:00 | 1 | 83 | 18 | 0 | 16 | 1 | 0 | 1 | 4 | 0 | 2 | 1 | 0 | 127 |
| 21:00 | 0 | 72 | 16 | 1 | 11 | 0 | 0 | 0 | 1 | 0 | 8 | 1 | 0 | 110 |
| 22:00 | 1 | 62 | 9 | 0 | 5 | 0 | 0 | 1 | 5 | 0 | 6 | 1 | 0 | 90 |
| 23:00 | 0 | 46 | 6 | 0 | 1 | 0 | 0 | 0 | 3 | 0 | 7 | 1 | 0 | 64 |
| Totals | 29 | 3450 | 964 | 46 | 920 | 51 | 4 | 140 | 338 | 1 | 148 | 38 | | 6129 |
| % of Totals | 0% | 56% | 16% | 1% | 15% | 1% | 0% | 2% | 6% | 0% | 2% | 1% | | 100% |

141

| | | | | | | | | | | | | | | | | |
|---------------------------------|-------|---------------|-------|-------|-------|------------------|-------|-------|---------------|--------|-------|-------------------------|-----|--------|-------|--|
| AM Volumes | 14 | 1335 | 390 | 23 | 471 | 25 | 0 | 78 | 168 | 0 | 57 | 26 | 0 | 2587 | | |
| % AM | 0% | 22% | 6% | 0% | 8% | 0% | | 1% | 3% | | 1% | 0% | | 42% | | |
| AM Peak Hour | 11:00 | 7:00 | 7:00 | 8:00 | 8:00 | 7:00 | | 10:00 | 7:00 | | 9:00 | 3:00 | | 7:00 | | |
| Volume | 3 | 265 | 62 | 5 | 69 | 5 | | 12 | 25 | | 10 | 4 | | 442 | | |
| PM Volumes | 15 | 2115 | 574 | 23 | 449 | 26 | 4 | 62 | 170 | 1 | 91 | 12 | 0 | 3542 | | |
| % PM | 0% | 35% | 9% | 0% | 7% | 0% | 0% | 1% | 3% | 0% | 1% | 0% | | 58% | | |
| PM Peak Hour | 14:00 | 17:00 | 17:00 | 15:00 | 17:00 | 14:00 | 17:00 | 13:00 | 14:00 | 16:00 | 14:00 | 13:00 | | 17:00 | | |
| Volume | 5 | 402 | 96 | 6 | 68 | 7 | 2 | 13 | 28 | 1 | 14 | 5 | | 604 | | |
| Directional Peak Periods | | AM 7-9 | | | | NOON 12-2 | | | PM 4-6 | | | Off Peak Volumes | | | | |
| All Classes | | Volume | | | % | Volume | | | % | Volume | | | % | Volume | | |
| | | 806 | ↔ | | 13% | 584 | ↔ | | 10% | 1160 | ↔ | | 19% | 3579 | ↔ 58% | |

| Classification Definitions | | | | |
|-------------------------------|-------------------------------|----------------------------|-----------------------------|----------------------------|
| 1 Motorcycles | 4 Buses | 7 >=4-Axle Single Units | 10 >=6-Axle Single Trailers | 13 >=7-Axle Multi-Trailers |
| 2 Passenger Cars | 5 2-Axle, 6-Tire Single Units | 8 <=4-Axle Single Trailers | 11 <=5-Axle Multi-Trailers | |
| 3 2-Axle, 4-Tire Single Units | 6 3-Axle Single Units | 9 5-Axle Single Trailers | 12 6-Axle Multi-Trailers | |

CLASSIFICATION

Keyes Rd W/O Bystrum Rd

Day: Tuesday
Date: 10/4/2016

County: Stanislaus
Project #: CA16_7709_001w

West Bound

142

| Time | # 1 | # 2 | # 3 | # 4 | # 5 | # 6 | # 7 | # 8 | # 9 | # 10 | # 11 | # 12 | # 13 | Total |
|---------------|-----------|-------------|------------|-----------|------------|-----------|----------|------------|------------|------|-----------|-----------|------|-------------|
| 0:00 AM | 0 | 20 | 1 | 0 | 5 | 0 | 0 | 2 | 2 | 0 | 2 | 0 | 0 | 32 |
| 1:00 | 0 | 4 | 1 | 0 | 5 | 0 | 0 | 4 | 3 | 0 | 1 | 1 | 0 | 19 |
| 2:00 | 0 | 12 | 1 | 1 | 2 | 0 | 0 | 2 | 0 | 0 | 0 | 0 | 0 | 18 |
| 3:00 | 0 | 29 | 12 | 4 | 10 | 1 | 0 | 6 | 2 | 0 | 0 | 1 | 0 | 65 |
| 4:00 | 0 | 65 | 24 | 4 | 34 | 2 | 0 | 3 | 4 | 0 | 0 | 0 | 0 | 136 |
| 5:00 | 1 | 119 | 35 | 1 | 58 | 2 | 0 | 5 | 9 | 0 | 1 | 1 | 0 | 232 |
| 6:00 | 0 | 118 | 40 | 0 | 51 | 0 | 0 | 10 | 10 | 0 | 2 | 1 | 0 | 232 |
| 7:00 | 0 | 154 | 37 | 3 | 50 | 3 | 0 | 8 | 11 | 0 | 3 | 1 | 0 | 270 |
| 8:00 | 0 | 121 | 23 | 3 | 59 | 2 | 0 | 7 | 6 | 0 | 3 | 2 | 0 | 226 |
| 9:00 | 0 | 45 | 26 | 1 | 35 | 0 | 0 | 4 | 9 | 0 | 2 | 0 | 0 | 122 |
| 10:00 | 2 | 56 | 13 | 1 | 30 | 1 | 0 | 12 | 11 | 0 | 3 | 2 | 0 | 131 |
| 11:00 | 1 | 47 | 19 | 0 | 42 | 2 | 0 | 7 | 5 | 0 | 3 | 0 | 0 | 126 |
| 12:00 PM | 0 | 59 | 20 | 0 | 42 | 0 | 0 | 5 | 8 | 0 | 1 | 1 | 0 | 136 |
| 13:00 | 0 | 58 | 19 | 1 | 34 | 1 | 0 | 8 | 7 | 0 | 4 | 0 | 0 | 132 |
| 14:00 | 1 | 66 | 22 | 2 | 24 | 3 | 0 | 3 | 14 | 0 | 6 | 1 | 0 | 142 |
| 15:00 | 1 | 74 | 29 | 2 | 34 | 1 | 0 | 7 | 3 | 0 | 2 | 0 | 0 | 153 |
| 16:00 | 1 | 102 | 22 | 3 | 37 | 3 | 0 | 5 | 7 | 0 | 2 | 0 | 0 | 182 |
| 17:00 | 0 | 92 | 31 | 2 | 35 | 4 | 1 | 2 | 3 | 0 | 2 | 0 | 0 | 172 |
| 18:00 | 2 | 80 | 23 | 0 | 15 | 0 | 0 | 3 | 6 | 0 | 4 | 0 | 0 | 133 |
| 19:00 | 0 | 51 | 15 | 0 | 21 | 0 | 0 | 2 | 4 | 0 | 4 | 0 | 0 | 97 |
| 20:00 | 0 | 41 | 9 | 0 | 13 | 1 | 0 | 1 | 4 | 0 | 0 | 0 | 0 | 69 |
| 21:00 | 0 | 25 | 8 | 1 | 7 | 0 | 0 | 0 | 0 | 0 | 6 | 0 | 0 | 47 |
| 22:00 | 1 | 24 | 4 | 0 | 5 | 0 | 0 | 1 | 2 | 0 | 2 | 0 | 0 | 39 |
| 23:00 | 0 | 27 | 2 | 0 | 1 | 0 | 0 | 0 | 2 | 0 | 3 | 0 | 0 | 35 |
| Totals | 10 | 1489 | 436 | 29 | 649 | 26 | 1 | 107 | 132 | | 56 | 11 | | 2946 |
| % of Totals | 0% | 51% | 15% | 1% | 22% | 1% | 0% | 4% | 4% | | 2% | 0% | | 100% |

| | | | | | | | | | | | | | | | | | |
|---------------------------------|-------|---------------|-------|-------|-------|------------------|-------|-------|---------------|--------|-------|-------------------------|-----|--------|---|--|-----|
| AM Volumes | 4 | 790 | 232 | 18 | 381 | 13 | 0 | 70 | 72 | 0 | 20 | 9 | 0 | 1609 | | | |
| % AM | 0% | 27% | 8% | 1% | 13% | 0% | | 2% | 2% | | 1% | 0% | | 55% | | | |
| AM Peak Hour | 10:00 | 7:00 | 6:00 | 3:00 | 8:00 | 7:00 | | 10:00 | 7:00 | | 7:00 | 8:00 | | 7:00 | | | |
| Volume | 2 | 154 | 40 | 4 | 59 | 3 | | 12 | 11 | | 3 | 2 | | 270 | | | |
| PM Volumes | 6 | 699 | 204 | 11 | 268 | 13 | 1 | 37 | 60 | 0 | 36 | 2 | 0 | 1337 | | | |
| % PM | 0% | 24% | 7% | 0% | 9% | 0% | 0% | 1% | 2% | | 1% | 0% | | 45% | | | |
| PM Peak Hour | 18:00 | 16:00 | 17:00 | 16:00 | 12:00 | 17:00 | 17:00 | 13:00 | 14:00 | | 14:00 | 12:00 | | 16:00 | | | |
| Volume | 2 | 102 | 31 | 3 | 42 | 4 | 1 | 8 | 14 | | 6 | 1 | | 182 | | | |
| Directional Peak Periods | | AM 7-9 | | | | NOON 12-2 | | | PM 4-6 | | | Off Peak Volumes | | | | | |
| All Classes | | Volume | | | % | Volume | | | % | Volume | | | % | Volume | | | % |
| | | 496 | ↔ | | 17% | 268 | ↔ | | 9% | 354 | ↔ | | 12% | 1828 | ↔ | | 62% |

| Classification Definitions | | | | |
|-------------------------------|-------------------------------|----------------------------|-----------------------------|----------------------------|
| 1 Motorcycles | 4 Buses | 7 >=4-Axle Single Units | 10 >=6-Axle Single Trailers | 13 >=7-Axle Multi-Trailers |
| 2 Passenger Cars | 5 2-Axle, 6-Tire Single Units | 8 <=4-Axle Single Trailers | 11 <=5-Axle Multi-Trailers | |
| 3 2-Axle, 4-Tire Single Units | 6 3-Axle Single Units | 9 5-Axle Single Trailers | 12 6-Axle Multi-Trailers | |

CLASSIFICATION

Keyes Rd W/O Bystrum Rd

Day: Tuesday
Date: 10/4/2016

County: Stanislaus
Project #: CA16_7709_001e

East Bound

143

| Time | # 1 | # 2 | # 3 | # 4 | # 5 | # 6 | # 7 | # 8 | # 9 | # 10 | # 11 | # 12 | # 13 | Total |
|--------------------|-----------|-------------|------------|-----------|------------|-----------|-----------|-----------|------------|-----------|-----------|-----------|------|-------------|
| 0:00 AM | 0 | 6 | 1 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 1 | 0 | 14 |
| 1:00 | 0 | 8 | 1 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 4 | 2 | 0 | 16 |
| 2:00 | 0 | 8 | 3 | 0 | 0 | 0 | 0 | 1 | 3 | 0 | 4 | 2 | 0 | 21 |
| 3:00 | 1 | 8 | 3 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 1 | 3 | 0 | 17 |
| 4:00 | 0 | 23 | 7 | 0 | 4 | 0 | 0 | 1 | 6 | 0 | 4 | 1 | 0 | 46 |
| 5:00 | 0 | 68 | 18 | 0 | 7 | 0 | 0 | 0 | 11 | 0 | 0 | 1 | 0 | 105 |
| 6:00 | 2 | 73 | 12 | 1 | 9 | 1 | 0 | 1 | 11 | 0 | 1 | 2 | 0 | 113 |
| 7:00 | 1 | 111 | 25 | 1 | 14 | 2 | 0 | 1 | 14 | 0 | 1 | 2 | 0 | 172 |
| 8:00 | 2 | 83 | 20 | 2 | 10 | 2 | 0 | 0 | 15 | 0 | 4 | 0 | 0 | 138 |
| 9:00 | 2 | 56 | 22 | 1 | 11 | 3 | 0 | 3 | 15 | 0 | 8 | 1 | 0 | 122 |
| 10:00 | 0 | 44 | 24 | 0 | 13 | 2 | 0 | 0 | 6 | 0 | 3 | 1 | 0 | 93 |
| 11:00 | 2 | 57 | 22 | 0 | 21 | 2 | 0 | 1 | 11 | 0 | 4 | 1 | 0 | 121 |
| 12:00 PM | 0 | 68 | 20 | 2 | 15 | 1 | 0 | 4 | 19 | 0 | 4 | 0 | 0 | 133 |
| 13:00 | 1 | 98 | 32 | 0 | 17 | 3 | 0 | 5 | 16 | 0 | 6 | 5 | 0 | 183 |
| 14:00 | 4 | 140 | 38 | 2 | 25 | 4 | 0 | 3 | 14 | 0 | 8 | 0 | 0 | 238 |
| 15:00 | 1 | 183 | 65 | 4 | 30 | 2 | 0 | 2 | 16 | 0 | 6 | 0 | 0 | 309 |
| 16:00 | 0 | 256 | 63 | 1 | 28 | 0 | 1 | 5 | 13 | 1 | 6 | 0 | 0 | 374 |
| 17:00 | 1 | 310 | 65 | 3 | 33 | 2 | 1 | 5 | 7 | 0 | 5 | 0 | 0 | 432 |
| 18:00 | 0 | 150 | 36 | 0 | 18 | 1 | 1 | 1 | 11 | 0 | 6 | 1 | 0 | 225 |
| 19:00 | 1 | 65 | 25 | 0 | 8 | 0 | 0 | 0 | 9 | 0 | 2 | 0 | 0 | 110 |
| 20:00 | 1 | 42 | 9 | 0 | 3 | 0 | 0 | 0 | 0 | 0 | 2 | 1 | 0 | 58 |
| 21:00 | 0 | 47 | 8 | 0 | 4 | 0 | 0 | 0 | 1 | 0 | 2 | 1 | 0 | 63 |
| 22:00 | 0 | 38 | 5 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 4 | 1 | 0 | 51 |
| 23:00 | 0 | 19 | 4 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 4 | 1 | 0 | 29 |
| Totals | 19 | 1961 | 528 | 17 | 271 | 25 | 3 | 33 | 206 | 1 | 92 | 27 | | 3183 |
| % of Totals | 1% | 62% | 17% | 1% | 9% | 1% | 0% | 1% | 6% | 0% | 3% | 1% | | 100% |

| | | | | | | | | | | | | | | | | | |
|---------------------------------|-------|---------------|-------|-------|-------|------------------|-------|-------|---------------|--------|-------|-------------------------|-----|--------|---|--|-----|
| AM Volumes | 10 | 545 | 158 | 5 | 90 | 12 | 0 | 8 | 96 | 0 | 37 | 17 | 0 | 978 | | | |
| % AM | 0% | 17% | 5% | 0% | 3% | 0% | | 0% | 3% | | 1% | 1% | | 31% | | | |
| AM Peak Hour | 6:00 | 7:00 | 7:00 | 8:00 | 11:00 | 9:00 | | 9:00 | 8:00 | | 9:00 | 3:00 | | 7:00 | | | |
| Volume | 2 | 111 | 25 | 2 | 21 | 3 | | 3 | 15 | | 8 | 3 | | 172 | | | |
| PM Volumes | 9 | 1416 | 370 | 12 | 181 | 13 | 3 | 25 | 110 | 1 | 55 | 10 | 0 | 2205 | | | |
| % PM | 0% | 44% | 12% | 0% | 6% | 0% | 0% | 1% | 3% | 0% | 2% | 0% | | 69% | | | |
| PM Peak Hour | 14:00 | 17:00 | 15:00 | 15:00 | 17:00 | 14:00 | 16:00 | 13:00 | 12:00 | 16:00 | 14:00 | 13:00 | | 17:00 | | | |
| Volume | 4 | 310 | 65 | 4 | 33 | 4 | 1 | 5 | 19 | 1 | 8 | 5 | | 432 | | | |
| Directional Peak Periods | | AM 7-9 | | | | NOON 12-2 | | | PM 4-6 | | | Off Peak Volumes | | | | | |
| All Classes | | Volume | | | % | Volume | | | % | Volume | | | % | Volume | | | % |
| | | 310 | ↔ | | 10% | 316 | ↔ | | 10% | 806 | ↔ | | 25% | 1751 | ↔ | | 55% |

| Classification Definitions | | | | |
|-------------------------------|-------------------------------|----------------------------|-----------------------------|----------------------------|
| 1 Motorcycles | 4 Buses | 7 >=4-Axle Single Units | 10 >=6-Axle Single Trailers | 13 >=7-Axle Multi-Trailers |
| 2 Passenger Cars | 5 2-Axle, 6-Tire Single Units | 8 <=4-Axle Single Trailers | 11 <=5-Axle Multi-Trailers | |
| 3 2-Axle, 4-Tire Single Units | 6 3-Axle Single Units | 9 5-Axle Single Trailers | 12 6-Axle Multi-Trailers | |

VOLUME

Keyes Rd W/O Bystrum Rd

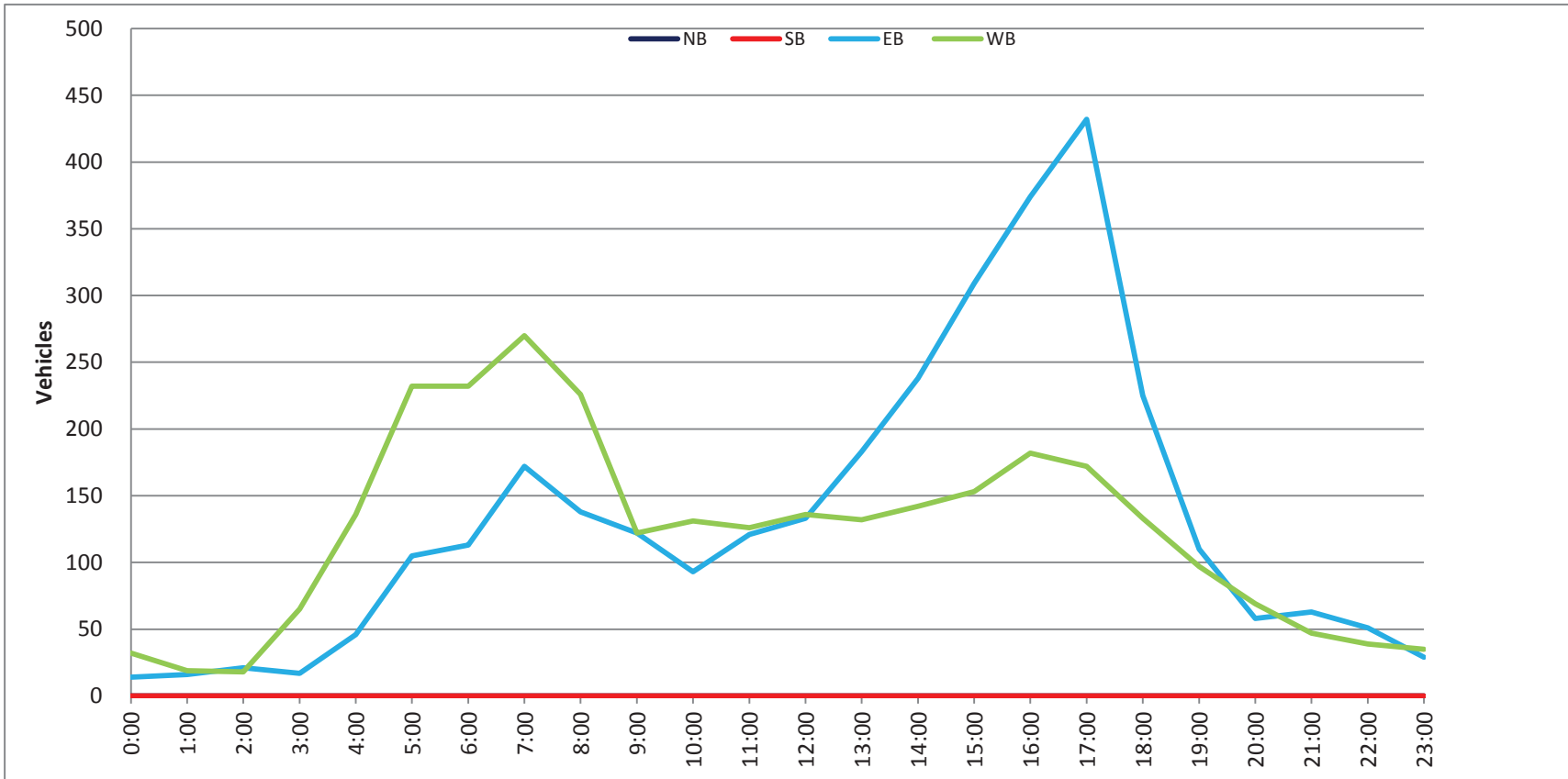
Day: Tuesday
 Date: 10/4/2016

County: Stanislaus
 Project #: CA16_7709_001

| DAILY TOTALS | | | | | NB | SB | EB | WB | Total | | |
|----------------|----|----|-------|-------|-------|----------------|-------|-------|-------|-------|-------|
| | | | | | 0 | 0 | 3,183 | 2,946 | 6,129 | | |
| AM Period | NB | SB | EB | WB | TOTAL | PM Period | NB | SB | EB | WB | TOTAL |
| 0:00 | 0 | 0 | 5 | 10 | 15 | 12:00 | 0 | 0 | 29 | 34 | 63 |
| 0:15 | 0 | 0 | 4 | 13 | 17 | 12:15 | 0 | 0 | 35 | 27 | 62 |
| 0:30 | 0 | 0 | 2 | 5 | 7 | 12:30 | 0 | 0 | 33 | 42 | 75 |
| 0:45 | 0 | 0 | 3 | 14 | 17 | 12:45 | 0 | 0 | 36 | 133 | 169 |
| 1:00 | 0 | 0 | 10 | 4 | 14 | 13:00 | 0 | 0 | 43 | 27 | 70 |
| 1:15 | 0 | 0 | 2 | 6 | 8 | 13:15 | 0 | 0 | 42 | 27 | 69 |
| 1:30 | 0 | 0 | 1 | 6 | 7 | 13:30 | 0 | 0 | 55 | 40 | 95 |
| 1:45 | 0 | 0 | 3 | 16 | 19 | 13:45 | 0 | 0 | 43 | 183 | 226 |
| 2:00 | 0 | 0 | 5 | 5 | 10 | 14:00 | 0 | 0 | 75 | 30 | 105 |
| 2:15 | 0 | 0 | 2 | 6 | 8 | 14:15 | 0 | 0 | 61 | 33 | 94 |
| 2:30 | 0 | 0 | 4 | 4 | 8 | 14:30 | 0 | 0 | 43 | 45 | 88 |
| 2:45 | 0 | 0 | 10 | 21 | 31 | 14:45 | 0 | 0 | 59 | 238 | 297 |
| 3:00 | 0 | 0 | 3 | 12 | 15 | 15:00 | 0 | 0 | 65 | 41 | 106 |
| 3:15 | 0 | 0 | 5 | 12 | 17 | 15:15 | 0 | 0 | 79 | 33 | 112 |
| 3:30 | 0 | 0 | 3 | 16 | 19 | 15:30 | 0 | 0 | 69 | 49 | 118 |
| 3:45 | 0 | 0 | 6 | 17 | 23 | 15:45 | 0 | 0 | 96 | 309 | 405 |
| 4:00 | 0 | 0 | 4 | 22 | 26 | 16:00 | 0 | 0 | 87 | 48 | 135 |
| 4:15 | 0 | 0 | 12 | 25 | 37 | 16:15 | 0 | 0 | 83 | 48 | 131 |
| 4:30 | 0 | 0 | 15 | 39 | 54 | 16:30 | 0 | 0 | 108 | 34 | 142 |
| 4:45 | 0 | 0 | 15 | 46 | 61 | 16:45 | 0 | 0 | 96 | 374 | 470 |
| 5:00 | 0 | 0 | 20 | 51 | 71 | 17:00 | 0 | 0 | 106 | 53 | 159 |
| 5:15 | 0 | 0 | 21 | 65 | 86 | 17:15 | 0 | 0 | 118 | 42 | 160 |
| 5:30 | 0 | 0 | 31 | 63 | 94 | 17:30 | 0 | 0 | 99 | 39 | 138 |
| 5:45 | 0 | 0 | 33 | 105 | 138 | 17:45 | 0 | 0 | 109 | 432 | 541 |
| 6:00 | 0 | 0 | 13 | 52 | 65 | 18:00 | 0 | 0 | 71 | 34 | 105 |
| 6:15 | 0 | 0 | 31 | 38 | 69 | 18:15 | 0 | 0 | 68 | 31 | 99 |
| 6:30 | 0 | 0 | 33 | 78 | 111 | 18:30 | 0 | 0 | 51 | 40 | 91 |
| 6:45 | 0 | 0 | 36 | 113 | 149 | 18:45 | 0 | 0 | 35 | 225 | 260 |
| 7:00 | 0 | 0 | 35 | 36 | 71 | 19:00 | 0 | 0 | 39 | 26 | 65 |
| 7:15 | 0 | 0 | 45 | 78 | 123 | 19:15 | 0 | 0 | 27 | 31 | 58 |
| 7:30 | 0 | 0 | 45 | 74 | 119 | 19:30 | 0 | 0 | 23 | 20 | 43 |
| 7:45 | 0 | 0 | 47 | 172 | 219 | 19:45 | 0 | 0 | 21 | 110 | 131 |
| 8:00 | 0 | 0 | 33 | 84 | 117 | 20:00 | 0 | 0 | 11 | 18 | 29 |
| 8:15 | 0 | 0 | 32 | 61 | 93 | 20:15 | 0 | 0 | 11 | 14 | 25 |
| 8:30 | 0 | 0 | 42 | 47 | 89 | 20:30 | 0 | 0 | 20 | 16 | 36 |
| 8:45 | 0 | 0 | 31 | 138 | 169 | 20:45 | 0 | 0 | 16 | 58 | 74 |
| 9:00 | 0 | 0 | 35 | 33 | 68 | 21:00 | 0 | 0 | 17 | 18 | 35 |
| 9:15 | 0 | 0 | 29 | 23 | 52 | 21:15 | 0 | 0 | 17 | 10 | 27 |
| 9:30 | 0 | 0 | 29 | 34 | 63 | 21:30 | 0 | 0 | 13 | 9 | 22 |
| 9:45 | 0 | 0 | 29 | 122 | 151 | 21:45 | 0 | 0 | 16 | 63 | 79 |
| 10:00 | 0 | 0 | 21 | 27 | 48 | 22:00 | 0 | 0 | 15 | 10 | 25 |
| 10:15 | 0 | 0 | 22 | 36 | 58 | 22:15 | 0 | 0 | 11 | 6 | 17 |
| 10:30 | 0 | 0 | 30 | 35 | 65 | 22:30 | 0 | 0 | 14 | 10 | 24 |
| 10:45 | 0 | 0 | 20 | 93 | 113 | 22:45 | 0 | 0 | 11 | 51 | 62 |
| 11:00 | 0 | 0 | 34 | 33 | 67 | 23:00 | 0 | 0 | 10 | 9 | 19 |
| 11:15 | 0 | 0 | 22 | 41 | 63 | 23:15 | 0 | 0 | 7 | 6 | 13 |
| 11:30 | 0 | 0 | 41 | 19 | 60 | 23:30 | 0 | 0 | 4 | 16 | 20 |
| 11:45 | 0 | 0 | 24 | 121 | 145 | 23:45 | 0 | 0 | 8 | 29 | 37 |
| TOTALS | | | 978 | 1609 | 2587 | TOTALS | | | 2205 | 1337 | 3542 |
| SPLIT % | | | 37.8% | 62.2% | 42.2% | SPLIT % | | | 62.3% | 37.7% | 57.8% |

| DAILY TOTALS | | | | | NB | SB | EB | WB | Total |
|-----------------|-------|-------|-------|-------|-----------------|-----------------|-------|-------|-------|
| | | | | | 0 | 0 | 3,183 | 2,946 | 6,129 |
| AM Peak Hour | | 7:00 | 7:15 | 7:15 | PM Peak Hour | | 17:30 | 16:45 | 17:00 |
| AM Pk Volume | | 172 | 318 | 488 | PM Pk Volume | | 347 | 187 | 609 |
| Pk Hr Factor | | 0.915 | 0.946 | 0.946 | Pk Hr Factor | | 0.796 | 0.877 | 0.944 |
| 7 - 9 Volume | 0 | 0 | 310 | 496 | 806 | 4 - 6 Volume | 0 | 0 | 806 |
| 7 - 9 Peak Hour | | 7:00 | 7:15 | 7:15 | 4 - 6 Peak Hour | | 17:00 | 16:15 | 16:30 |
| 7 - 9 Pk Volume | 0 | 0 | 172 | 318 | 488 | 4 - 6 Pk Volume | 0 | 0 | 432 |
| Pk Hr Factor | 0.000 | 0.000 | 0.915 | 0.946 | 0.946 | Pk Hr Factor | 0.000 | 0.000 | 0.915 |

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CLASSIFICATION

Keyes Rd E/O Entrance to Bronco Winery

Day: Tuesday
Date: 10/4/2016

County: Stanislaus
Project #: CA16_7709_002

Summary

| Time | # 1 | # 2 | # 3 | # 4 | # 5 | # 6 | # 7 | # 8 | # 9 | # 10 | # 11 | # 12 | # 13 | Total |
|--------------------|-----------|-------------|-------------|-----------|------------|-----------|-----------|------------|------------|-----------|------------|-----------|-----------|-------------|
| 0:00 AM | 0 | 24 | 6 | 0 | 4 | 0 | 0 | 1 | 6 | 0 | 10 | 1 | 0 | 52 |
| 1:00 | 0 | 18 | 1 | 0 | 3 | 1 | 0 | 3 | 2 | 0 | 2 | 4 | 0 | 34 |
| 2:00 | 1 | 24 | 5 | 0 | 3 | 0 | 0 | 2 | 3 | 0 | 10 | 3 | 0 | 51 |
| 3:00 | 0 | 56 | 13 | 0 | 6 | 1 | 0 | 4 | 2 | 0 | 7 | 5 | 0 | 94 |
| 4:00 | 1 | 131 | 25 | 0 | 11 | 1 | 0 | 6 | 11 | 0 | 2 | 1 | 0 | 189 |
| 5:00 | 9 | 230 | 56 | 0 | 24 | 2 | 0 | 6 | 14 | 0 | 3 | 2 | 2 | 348 |
| 6:00 | 1 | 237 | 65 | 1 | 23 | 1 | 0 | 11 | 19 | 0 | 3 | 2 | 0 | 363 |
| 7:00 | 2 | 343 | 78 | 1 | 31 | 7 | 0 | 6 | 21 | 1 | 6 | 2 | 0 | 498 |
| 8:00 | 1 | 267 | 80 | 2 | 15 | 3 | 0 | 10 | 19 | 0 | 5 | 1 | 1 | 404 |
| 9:00 | 1 | 143 | 59 | 1 | 15 | 3 | 0 | 14 | 20 | 0 | 11 | 1 | 0 | 268 |
| 10:00 | 3 | 131 | 41 | 0 | 28 | 3 | 0 | 8 | 15 | 0 | 13 | 3 | 0 | 245 |
| 11:00 | 3 | 127 | 48 | 0 | 33 | 3 | 0 | 9 | 16 | 0 | 11 | 1 | 0 | 251 |
| 12:00 PM | 0 | 159 | 49 | 2 | 26 | 2 | 0 | 11 | 22 | 0 | 10 | 0 | 0 | 281 |
| 13:00 | 3 | 198 | 59 | 0 | 23 | 6 | 0 | 9 | 22 | 1 | 11 | 7 | 0 | 339 |
| 14:00 | 6 | 269 | 70 | 2 | 26 | 4 | 0 | 8 | 21 | 0 | 14 | 1 | 0 | 421 |
| 15:00 | 3 | 316 | 103 | 5 | 55 | 4 | 0 | 8 | 22 | 0 | 4 | 0 | 0 | 520 |
| 16:00 | 1 | 438 | 105 | 1 | 49 | 1 | 1 | 9 | 20 | 0 | 6 | 0 | 0 | 631 |
| 17:00 | 0 | 475 | 97 | 3 | 59 | 2 | 2 | 11 | 10 | 0 | 5 | 0 | 0 | 664 |
| 18:00 | 1 | 269 | 53 | 0 | 22 | 2 | 1 | 5 | 13 | 0 | 2 | 2 | 0 | 370 |
| 19:00 | 3 | 131 | 38 | 1 | 19 | 0 | 0 | 3 | 12 | 0 | 5 | 0 | 0 | 212 |
| 20:00 | 0 | 102 | 19 | 0 | 7 | 1 | 0 | 1 | 0 | 0 | 9 | 1 | 0 | 140 |
| 21:00 | 0 | 81 | 14 | 0 | 7 | 0 | 0 | 0 | 2 | 0 | 6 | 3 | 0 | 113 |
| 22:00 | 0 | 65 | 7 | 0 | 2 | 0 | 0 | 1 | 4 | 0 | 2 | 1 | 0 | 82 |
| 23:00 | 0 | 69 | 8 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 3 | 3 | 0 | 86 |
| Totals | 39 | 4303 | 1099 | 19 | 491 | 47 | 4 | 146 | 299 | 2 | 160 | 44 | 3 | 6656 |
| % of Totals | 1% | 65% | 17% | 0% | 7% | 1% | 0% | 2% | 4% | 0% | 2% | 1% | 0% | 100% |

146

| | | | | | | | | | | | | | | | | | |
|---------------------------------|-------|-------|---------------|-------|-------|-------|------------------|-------|-------|-------|---------------|-------|------|-------------------------|--------|-------|--|
| AM Volumes | 22 | 1731 | 477 | 5 | 196 | 25 | 0 | 80 | 148 | 1 | 83 | 26 | 3 | 2797 | | | |
| % AM | 0% | 26% | 7% | 0% | 3% | 0% | | 1% | 2% | 0% | 1% | 0% | 0% | 42% | | | |
| AM Peak Hour | 5:00 | 7:00 | 8:00 | 8:00 | 11:00 | 7:00 | | 9:00 | 7:00 | 7:00 | 10:00 | 3:00 | 5:00 | 7:00 | | | |
| Volume | 9 | 343 | 80 | 2 | 33 | 7 | | 14 | 21 | 1 | 13 | 5 | 2 | 498 | | | |
| PM Volumes | 17 | 2572 | 622 | 14 | 295 | 22 | 4 | 66 | 151 | 1 | 77 | 18 | 0 | 3859 | | | |
| % PM | 0% | 39% | 9% | 0% | 4% | 0% | 0% | 1% | 2% | 0% | 1% | 0% | | 58% | | | |
| PM Peak Hour | 14:00 | 17:00 | 16:00 | 15:00 | 17:00 | 13:00 | 17:00 | 12:00 | 12:00 | 13:00 | 14:00 | 13:00 | | 17:00 | | | |
| Volume | 6 | 475 | 105 | 5 | 59 | 6 | 2 | 11 | 22 | 1 | 14 | 7 | | 664 | | | |
| Directional Peak Periods | | | AM 7-9 | | | | NOON 12-2 | | | | PM 4-6 | | | Off Peak Volumes | | | |
| All Classes | | | Volume | | | % | Volume | | | % | Volume | | | % | Volume | | |
| | | | 902 | ↔ | | 14% | 620 | ↔ | | 9% | 1295 | ↔ | | 19% | 3839 | ↔ 58% | |

| Classification Definitions | | | | |
|-------------------------------|-------------------------------|----------------------------|-----------------------------|----------------------------|
| 1 Motorcycles | 4 Buses | 7 >=4-Axle Single Units | 10 >=6-Axle Single Trailers | 13 >=7-Axle Multi-Trailers |
| 2 Passenger Cars | 5 2-Axle, 6-Tire Single Units | 8 <=4-Axle Single Trailers | 11 <=5-Axle Multi-Trailers | |
| 3 2-Axle, 4-Tire Single Units | 6 3-Axle Single Units | 9 5-Axle Single Trailers | 12 6-Axle Multi-Trailers | |

CLASSIFICATION

Keyes Rd E/O Entrance to Bronco Winery

Day: Tuesday
Date: 10/4/2016

County: Stanislaus
Project #: CA16_7709_002e

East Bound

| Time | # 1 | # 2 | # 3 | # 4 | # 5 | # 6 | # 7 | # 8 | # 9 | # 10 | # 11 | # 12 | # 13 | Total |
|--------------------|-----------|-------------|------------|-----------|------------|-----------|-----------|-----------|------------|------|-----------|-----------|------|-------------|
| 0:00 AM | 0 | 15 | 3 | 0 | 4 | 0 | 0 | 0 | 4 | 0 | 6 | 1 | 0 | 33 |
| 1:00 | 0 | 12 | 1 | 0 | 2 | 0 | 0 | 0 | 1 | 0 | 0 | 2 | 0 | 18 |
| 2:00 | 0 | 10 | 4 | 0 | 3 | 0 | 0 | 1 | 3 | 0 | 9 | 2 | 0 | 32 |
| 3:00 | 0 | 9 | 3 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 6 | 3 | 0 | 26 |
| 4:00 | 0 | 23 | 9 | 0 | 2 | 0 | 0 | 1 | 6 | 0 | 0 | 1 | 0 | 42 |
| 5:00 | 0 | 50 | 13 | 0 | 12 | 0 | 0 | 0 | 10 | 0 | 2 | 1 | 0 | 88 |
| 6:00 | 0 | 78 | 12 | 1 | 9 | 1 | 0 | 1 | 12 | 0 | 0 | 2 | 0 | 116 |
| 7:00 | 0 | 103 | 22 | 1 | 13 | 3 | 0 | 1 | 14 | 0 | 1 | 2 | 0 | 160 |
| 8:00 | 1 | 96 | 22 | 2 | 4 | 2 | 0 | 0 | 15 | 0 | 0 | 0 | 0 | 142 |
| 9:00 | 1 | 65 | 24 | 1 | 6 | 3 | 0 | 3 | 15 | 0 | 9 | 1 | 0 | 128 |
| 10:00 | 0 | 59 | 27 | 0 | 9 | 2 | 0 | 0 | 8 | 0 | 5 | 1 | 0 | 111 |
| 11:00 | 2 | 63 | 23 | 0 | 20 | 2 | 0 | 1 | 11 | 0 | 6 | 1 | 0 | 129 |
| 12:00 PM | 0 | 78 | 23 | 2 | 14 | 2 | 0 | 4 | 20 | 0 | 5 | 0 | 0 | 148 |
| 13:00 | 1 | 103 | 33 | 0 | 12 | 6 | 0 | 5 | 17 | 0 | 4 | 5 | 0 | 186 |
| 14:00 | 2 | 163 | 43 | 2 | 16 | 3 | 0 | 3 | 16 | 0 | 6 | 0 | 0 | 254 |
| 15:00 | 1 | 219 | 74 | 4 | 39 | 3 | 0 | 2 | 21 | 0 | 1 | 0 | 0 | 364 |
| 16:00 | 0 | 314 | 77 | 1 | 32 | 0 | 1 | 5 | 19 | 0 | 1 | 0 | 0 | 450 |
| 17:00 | 0 | 348 | 71 | 3 | 45 | 2 | 2 | 5 | 10 | 0 | 3 | 0 | 0 | 489 |
| 18:00 | 0 | 190 | 42 | 0 | 17 | 1 | 1 | 1 | 13 | 0 | 1 | 1 | 0 | 267 |
| 19:00 | 0 | 72 | 27 | 0 | 10 | 0 | 0 | 0 | 10 | 0 | 1 | 0 | 0 | 120 |
| 20:00 | 0 | 47 | 10 | 0 | 5 | 0 | 0 | 0 | 0 | 0 | 9 | 1 | 0 | 72 |
| 21:00 | 0 | 44 | 8 | 0 | 5 | 0 | 0 | 0 | 1 | 0 | 1 | 1 | 0 | 60 |
| 22:00 | 0 | 39 | 5 | 0 | 0 | 0 | 0 | 0 | 3 | 0 | 0 | 1 | 0 | 48 |
| 23:00 | 0 | 38 | 8 | 0 | 0 | 0 | 0 | 0 | 2 | 0 | 2 | 1 | 0 | 51 |
| Totals | 8 | 2238 | 584 | 17 | 284 | 30 | 4 | 33 | 231 | | 78 | 27 | | 3534 |
| % of Totals | 0% | 63% | 17% | 0% | 8% | 1% | 0% | 1% | 7% | | 2% | 1% | | 100% |

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| | | | | | | | | | | | | | | | | |
|---------------------------------|-------|---------------|-------|-------|-------|------------------|-------|-------|---------------|--------|-------|-------------------------|-----|--------|---|--|
| AM Volumes | 4 | 583 | 163 | 5 | 89 | 13 | 0 | 8 | 99 | 0 | 44 | 17 | 0 | 1025 | | |
| % AM | 0% | 16% | 5% | 0% | 3% | 0% | | 0% | 3% | | 1% | 0% | | 29% | | |
| AM Peak Hour | 11:00 | 7:00 | 10:00 | 8:00 | 11:00 | 7:00 | | 9:00 | 8:00 | | 2:00 | 3:00 | | 7:00 | | |
| Volume | 2 | 103 | 27 | 2 | 20 | 3 | | 3 | 15 | | 9 | 3 | | 160 | | |
| PM Volumes | 4 | 1655 | 421 | 12 | 195 | 17 | 4 | 25 | 132 | 0 | 34 | 10 | 0 | 2509 | | |
| % PM | 0% | 47% | 12% | 0% | 6% | 0% | 0% | 1% | 4% | | 1% | 0% | | 71% | | |
| PM Peak Hour | 14:00 | 17:00 | 16:00 | 15:00 | 17:00 | 13:00 | 17:00 | 13:00 | 15:00 | | 20:00 | 13:00 | | 17:00 | | |
| Volume | 2 | 348 | 77 | 4 | 45 | 6 | 2 | 5 | 21 | | 9 | 5 | | 489 | | |
| Directional Peak Periods | | AM 7-9 | | | | NOON 12-2 | | | PM 4-6 | | | Off Peak Volumes | | | | |
| All Classes | | Volume | | | % | Volume | | | % | Volume | | | % | Volume | | |
| | | 302 | ↔ | | 9% | 334 | ↔ | | 9% | 939 | ↔ | | 27% | 1959 | ↔ | |

| Classification Definitions | | | | |
|-------------------------------|-------------------------------|----------------------------|-----------------------------|----------------------------|
| 1 Motorcycles | 4 Buses | 7 >=4-Axle Single Units | 10 >=6-Axle Single Trailers | 13 >=7-Axle Multi-Trailers |
| 2 Passenger Cars | 5 2-Axle, 6-Tire Single Units | 8 <=4-Axle Single Trailers | 11 <=5-Axle Multi-Trailers | |
| 3 2-Axle, 4-Tire Single Units | 6 3-Axle Single Units | 9 5-Axle Single Trailers | 12 6-Axle Multi-Trailers | |

CLASSIFICATION

Keyes Rd E/O Entrance to Bronco Winery

Day: Tuesday
Date: 10/4/2016

County: Stanislaus
Project #: CA16_7709_002w

West Bound

| Time | # 1 | # 2 | # 3 | # 4 | # 5 | # 6 | # 7 | # 8 | # 9 | # 10 | # 11 | # 12 | # 13 | Total |
|--------------------|-----------|-------------|------------|-----------|------------|-----------|-----|------------|-----------|-----------|-----------|-----------|-----------|-------------|
| 0:00 AM | 0 | 9 | 3 | 0 | 0 | 0 | 0 | 1 | 2 | 0 | 4 | 0 | 0 | 19 |
| 1:00 | 0 | 6 | 0 | 0 | 1 | 1 | 0 | 3 | 1 | 0 | 2 | 2 | 0 | 16 |
| 2:00 | 1 | 14 | 1 | 0 | 0 | 0 | 0 | 1 | 0 | 0 | 1 | 1 | 0 | 19 |
| 3:00 | 0 | 47 | 10 | 0 | 1 | 1 | 0 | 4 | 2 | 0 | 1 | 2 | 0 | 68 |
| 4:00 | 1 | 108 | 16 | 0 | 9 | 1 | 0 | 5 | 5 | 0 | 2 | 0 | 0 | 147 |
| 5:00 | 9 | 180 | 43 | 0 | 12 | 2 | 0 | 6 | 4 | 0 | 1 | 1 | 2 | 260 |
| 6:00 | 1 | 159 | 53 | 0 | 14 | 0 | 0 | 10 | 7 | 0 | 3 | 0 | 0 | 247 |
| 7:00 | 2 | 240 | 56 | 0 | 18 | 4 | 0 | 5 | 7 | 1 | 5 | 0 | 0 | 338 |
| 8:00 | 0 | 171 | 58 | 0 | 11 | 1 | 0 | 10 | 4 | 0 | 5 | 1 | 1 | 262 |
| 9:00 | 0 | 78 | 35 | 0 | 9 | 0 | 0 | 11 | 5 | 0 | 2 | 0 | 0 | 140 |
| 10:00 | 3 | 72 | 14 | 0 | 19 | 1 | 0 | 8 | 7 | 0 | 8 | 2 | 0 | 134 |
| 11:00 | 1 | 64 | 25 | 0 | 13 | 1 | 0 | 8 | 5 | 0 | 5 | 0 | 0 | 122 |
| 12:00 PM | 0 | 81 | 26 | 0 | 12 | 0 | 0 | 7 | 2 | 0 | 5 | 0 | 0 | 133 |
| 13:00 | 2 | 95 | 26 | 0 | 11 | 0 | 0 | 4 | 5 | 1 | 7 | 2 | 0 | 153 |
| 14:00 | 4 | 106 | 27 | 0 | 10 | 1 | 0 | 5 | 5 | 0 | 8 | 1 | 0 | 167 |
| 15:00 | 2 | 97 | 29 | 1 | 16 | 1 | 0 | 6 | 1 | 0 | 3 | 0 | 0 | 156 |
| 16:00 | 1 | 124 | 28 | 0 | 17 | 1 | 0 | 4 | 1 | 0 | 5 | 0 | 0 | 181 |
| 17:00 | 0 | 127 | 26 | 0 | 14 | 0 | 0 | 6 | 0 | 0 | 2 | 0 | 0 | 175 |
| 18:00 | 1 | 79 | 11 | 0 | 5 | 1 | 0 | 4 | 0 | 0 | 1 | 1 | 0 | 103 |
| 19:00 | 3 | 59 | 11 | 1 | 9 | 0 | 0 | 3 | 2 | 0 | 4 | 0 | 0 | 92 |
| 20:00 | 0 | 55 | 9 | 0 | 2 | 1 | 0 | 1 | 0 | 0 | 0 | 0 | 0 | 68 |
| 21:00 | 0 | 37 | 6 | 0 | 2 | 0 | 0 | 0 | 1 | 0 | 5 | 2 | 0 | 53 |
| 22:00 | 0 | 26 | 2 | 0 | 2 | 0 | 0 | 1 | 1 | 0 | 2 | 0 | 0 | 34 |
| 23:00 | 0 | 31 | 0 | 0 | 0 | 0 | 0 | 0 | 1 | 0 | 1 | 2 | 0 | 35 |
| Totals | 31 | 2065 | 515 | 2 | 207 | 17 | | 113 | 68 | 2 | 82 | 17 | 3 | 3122 |
| % of Totals | 1% | 66% | 16% | 0% | 7% | 1% | | 4% | 2% | 0% | 3% | 1% | 0% | 100% |

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| | | | | | | | | | | | | | | | | |
|---------------------------------|-------|---------------|-------|-------|-------|------------------|---|-------|---------------|--------|-------|-------------------------|------|--------|-------|--|
| AM Volumes | 18 | 1148 | 314 | 0 | 107 | 12 | 0 | 72 | 49 | 1 | 39 | 9 | 3 | 1772 | | |
| % AM | 1% | 37% | 10% | | 3% | 0% | | 2% | 2% | 0% | 1% | 0% | 0% | 57% | | |
| AM Peak Hour | 5:00 | 7:00 | 8:00 | | 10:00 | 7:00 | | 9:00 | 6:00 | 7:00 | 10:00 | 1:00 | 5:00 | 7:00 | | |
| Volume | 9 | 240 | 58 | | 19 | 4 | | 11 | 7 | 1 | 8 | 2 | 2 | 338 | | |
| PM Volumes | 13 | 917 | 201 | 2 | 100 | 5 | 0 | 41 | 19 | 1 | 43 | 8 | 0 | 1350 | | |
| % PM | 0% | 29% | 6% | 0% | 3% | 0% | | 1% | 1% | 0% | 1% | 0% | | 43% | | |
| PM Peak Hour | 14:00 | 17:00 | 15:00 | 15:00 | 16:00 | 14:00 | | 12:00 | 13:00 | 13:00 | 14:00 | 13:00 | | 16:00 | | |
| Volume | 4 | 127 | 29 | 1 | 17 | 1 | | 7 | 5 | 1 | 8 | 2 | | 181 | | |
| Directional Peak Periods | | AM 7-9 | | | | NOON 12-2 | | | PM 4-6 | | | Off Peak Volumes | | | | |
| All Classes | | Volume | | | % | Volume | | | % | Volume | | | % | Volume | | |
| | | 600 | ↔ | | 19% | 286 | ↔ | | 9% | 356 | ↔ | | 11% | 1880 | ↔ 60% | |

| Classification Definitions | | | | |
|-------------------------------|-------------------------------|----------------------------|-----------------------------|----------------------------|
| 1 Motorcycles | 4 Buses | 7 >=4-Axle Single Units | 10 >=6-Axle Single Trailers | 13 >=7-Axle Multi-Trailers |
| 2 Passenger Cars | 5 2-Axle, 6-Tire Single Units | 8 <=4-Axle Single Trailers | 11 <=5-Axle Multi-Trailers | |
| 3 2-Axle, 4-Tire Single Units | 6 3-Axle Single Units | 9 5-Axle Single Trailers | 12 6-Axle Multi-Trailers | |

VOLUME

Keys Rd E/O Entrance to Bronco Winery

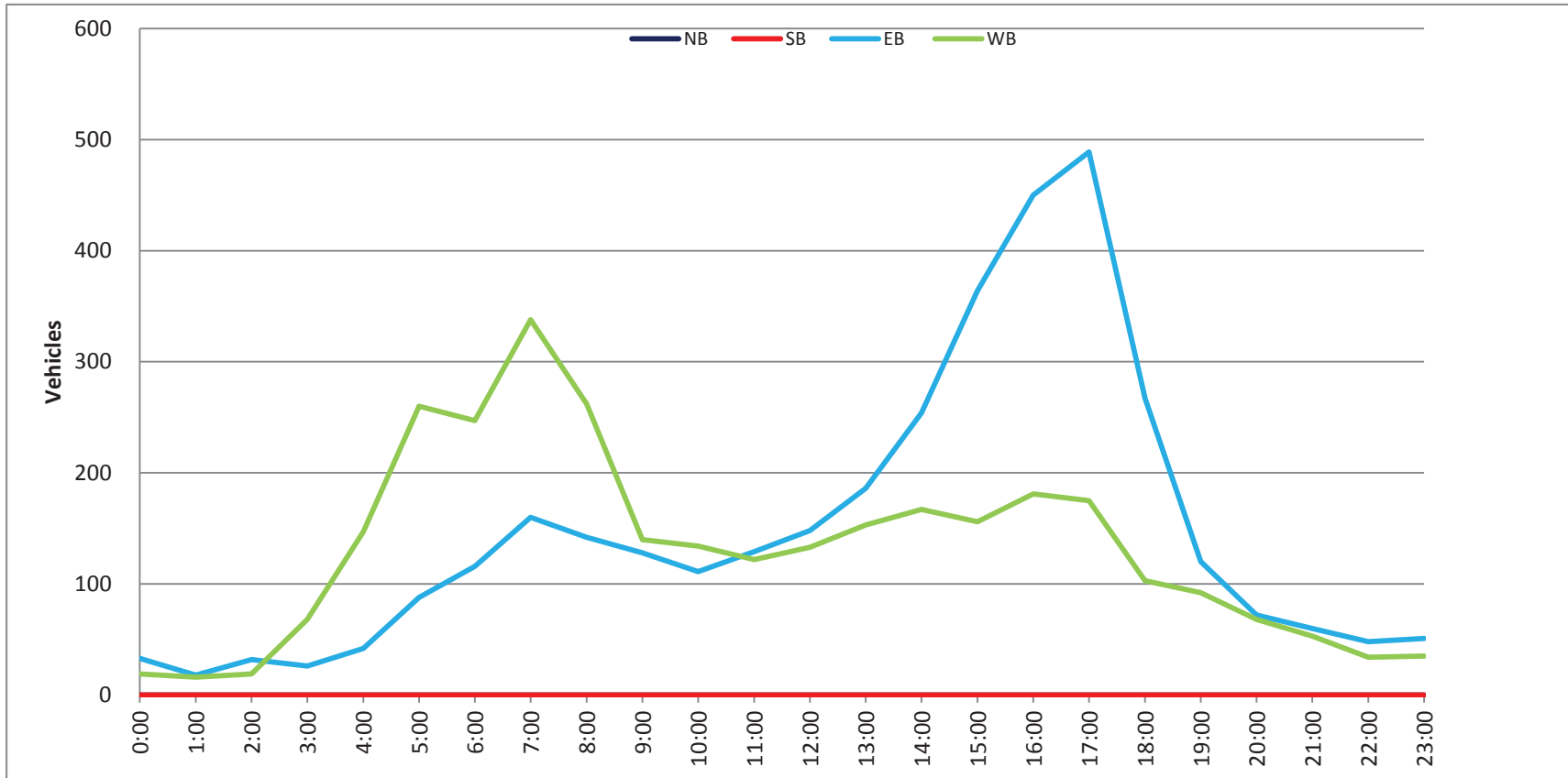
Day: Tuesday
 Date: 10/4/2016

County: Stanislaus
 Project #: CA16_7709_002

| DAILY TOTALS | | | | | NB | SB | EB | WB | Total | | | |
|----------------|----|----|-------|-------|-------|----------------|-------|-------|-------|-------|-------|-----|
| | | | | | 0 | 0 | 3,534 | 3,122 | 6,656 | | | |
| AM Period | NB | SB | EB | WB | TOTAL | PM Period | NB | SB | EB | WB | TOTAL | |
| 0:00 | 0 | 0 | 16 | 3 | 19 | 12:00 | 0 | 0 | 37 | 28 | 65 | |
| 0:15 | 0 | 0 | 4 | 9 | 13 | 12:15 | 0 | 0 | 44 | 33 | 77 | |
| 0:30 | 0 | 0 | 4 | 4 | 8 | 12:30 | 0 | 0 | 33 | 40 | 73 | |
| 0:45 | 0 | 0 | 9 | 33 | 3 | 12:45 | 0 | 0 | 34 | 148 | 32 | 133 |
| 1:00 | 0 | 0 | 8 | 2 | 10 | 13:00 | 0 | 0 | 38 | 31 | 69 | |
| 1:15 | 0 | 0 | 4 | 5 | 9 | 13:15 | 0 | 0 | 52 | 26 | 78 | |
| 1:30 | 0 | 0 | 3 | 5 | 8 | 13:30 | 0 | 0 | 49 | 53 | 102 | |
| 1:45 | 0 | 0 | 3 | 18 | 4 | 13:45 | 0 | 0 | 47 | 186 | 43 | 153 |
| 2:00 | 0 | 0 | 7 | 3 | 10 | 14:00 | 0 | 0 | 72 | 42 | 114 | |
| 2:15 | 0 | 0 | 4 | 6 | 10 | 14:15 | 0 | 0 | 58 | 39 | 97 | |
| 2:30 | 0 | 0 | 3 | 3 | 6 | 14:30 | 0 | 0 | 53 | 41 | 94 | |
| 2:45 | 0 | 0 | 18 | 32 | 7 | 14:45 | 0 | 0 | 71 | 254 | 45 | 167 |
| 3:00 | 0 | 0 | 9 | 12 | 21 | 15:00 | 0 | 0 | 78 | 45 | 123 | |
| 3:15 | 0 | 0 | 6 | 10 | 16 | 15:15 | 0 | 0 | 84 | 32 | 116 | |
| 3:30 | 0 | 0 | 5 | 19 | 24 | 15:30 | 0 | 0 | 96 | 51 | 147 | |
| 3:45 | 0 | 0 | 6 | 26 | 27 | 15:45 | 0 | 0 | 106 | 364 | 28 | 156 |
| 4:00 | 0 | 0 | 4 | 21 | 25 | 16:00 | 0 | 0 | 110 | 51 | 161 | |
| 4:15 | 0 | 0 | 8 | 28 | 36 | 16:15 | 0 | 0 | 98 | 49 | 147 | |
| 4:30 | 0 | 0 | 11 | 43 | 54 | 16:30 | 0 | 0 | 116 | 36 | 152 | |
| 4:45 | 0 | 0 | 19 | 42 | 55 | 16:45 | 0 | 0 | 126 | 450 | 45 | 181 |
| 5:00 | 0 | 0 | 26 | 39 | 65 | 17:00 | 0 | 0 | 127 | 48 | 175 | |
| 5:15 | 0 | 0 | 17 | 76 | 93 | 17:15 | 0 | 0 | 135 | 41 | 176 | |
| 5:30 | 0 | 0 | 26 | 72 | 98 | 17:30 | 0 | 0 | 115 | 41 | 156 | |
| 5:45 | 0 | 0 | 19 | 88 | 73 | 17:45 | 0 | 0 | 112 | 489 | 45 | 175 |
| 6:00 | 0 | 0 | 15 | 53 | 68 | 18:00 | 0 | 0 | 89 | 27 | 116 | |
| 6:15 | 0 | 0 | 31 | 43 | 74 | 18:15 | 0 | 0 | 81 | 26 | 107 | |
| 6:30 | 0 | 0 | 44 | 73 | 117 | 18:30 | 0 | 0 | 63 | 28 | 91 | |
| 6:45 | 0 | 0 | 26 | 116 | 78 | 18:45 | 0 | 0 | 34 | 267 | 22 | 103 |
| 7:00 | 0 | 0 | 30 | 54 | 84 | 19:00 | 0 | 0 | 39 | 25 | 64 | |
| 7:15 | 0 | 0 | 39 | 70 | 109 | 19:15 | 0 | 0 | 38 | 27 | 65 | |
| 7:30 | 0 | 0 | 53 | 104 | 157 | 19:30 | 0 | 0 | 22 | 18 | 40 | |
| 7:45 | 0 | 0 | 38 | 160 | 110 | 19:45 | 0 | 0 | 21 | 120 | 22 | 92 |
| 8:00 | 0 | 0 | 37 | 97 | 134 | 20:00 | 0 | 0 | 13 | 17 | 30 | |
| 8:15 | 0 | 0 | 28 | 73 | 101 | 20:15 | 0 | 0 | 17 | 15 | 32 | |
| 8:30 | 0 | 0 | 42 | 52 | 94 | 20:30 | 0 | 0 | 18 | 16 | 34 | |
| 8:45 | 0 | 0 | 35 | 142 | 40 | 20:45 | 0 | 0 | 24 | 72 | 20 | 68 |
| 9:00 | 0 | 0 | 38 | 43 | 81 | 21:00 | 0 | 0 | 19 | 16 | 35 | |
| 9:15 | 0 | 0 | 25 | 22 | 47 | 21:15 | 0 | 0 | 16 | 11 | 27 | |
| 9:30 | 0 | 0 | 30 | 39 | 69 | 21:30 | 0 | 0 | 12 | 11 | 23 | |
| 9:45 | 0 | 0 | 35 | 128 | 36 | 21:45 | 0 | 0 | 13 | 60 | 15 | 53 |
| 10:00 | 0 | 0 | 27 | 27 | 54 | 22:00 | 0 | 0 | 12 | 7 | 19 | |
| 10:15 | 0 | 0 | 27 | 39 | 66 | 22:15 | 0 | 0 | 9 | 4 | 13 | |
| 10:30 | 0 | 0 | 35 | 29 | 64 | 22:30 | 0 | 0 | 16 | 14 | 30 | |
| 10:45 | 0 | 0 | 22 | 111 | 39 | 22:45 | 0 | 0 | 11 | 48 | 9 | 34 |
| 11:00 | 0 | 0 | 37 | 33 | 70 | 23:00 | 0 | 0 | 9 | 10 | 19 | |
| 11:15 | 0 | 0 | 22 | 34 | 56 | 23:15 | 0 | 0 | 10 | 14 | 24 | |
| 11:30 | 0 | 0 | 47 | 20 | 67 | 23:30 | 0 | 0 | 20 | 7 | 27 | |
| 11:45 | 0 | 0 | 23 | 129 | 35 | 23:45 | 0 | 0 | 12 | 51 | 4 | 35 |
| TOTALS | | | 1025 | 1772 | 2797 | TOTALS | | | 2509 | 1350 | 3859 | |
| SPLIT % | | | 36.6% | 63.4% | 42.0% | SPLIT % | | | 65.0% | 35.0% | 58.0% | |

| DAILY TOTALS | | | | | NB | SB | EB | WB | Total | | |
|-----------------|-------|-------|-------|-------|-----------------|-----------------|-------|-------|-------|-------|-------|
| | | | | | 0 | 0 | 3,534 | 3,122 | 6,656 | | |
| AM Peak Hour | | 7:15 | 7:30 | 7:15 | PM Peak Hour | | 17:00 | 16:30 | 17:15 | | |
| AM Pk Volume | | 167 | 384 | 548 | PM Pk Volume | | 489 | 181 | 678 | | |
| Pk Hr Factor | | 0.788 | 0.873 | 0.873 | Pk Hr Factor | | 0.906 | 0.885 | 0.859 | | |
| 7 - 9 Volume | 0 | 0 | 302 | 600 | 902 | 4 - 6 Volume | 0 | 0 | 939 | 356 | 1295 |
| 7 - 9 Peak Hour | | 7:15 | 7:30 | 7:15 | 4 - 6 Peak Hour | | 16:30 | 16:00 | 16:45 | | |
| 7 - 9 Pk Volume | 0 | 0 | 167 | 384 | 548 | 4 - 6 Pk Volume | 0 | 0 | 504 | 181 | 678 |
| Pk Hr Factor | 0.000 | 0.000 | 0.788 | 0.873 | 0.873 | Pk Hr Factor | 0.000 | 0.000 | 0.933 | 0.887 | 0.963 |

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April 24, 2017

Kristin Doud
County of Stanislaus
Department of Planning and Community Development
1010 10th Street, Suite 3400
Modesto, CA 95354

Project: Rezone Application No. PLN2016-0066-Bronco Wine Company
District CEQA Reference No: 20170352

Dear Ms. Doud:

The San Joaquin Valley Unified Air Pollution Control District (District) has reviewed the project referenced above consisting of rezoning a 117.93 acre parcel from existing Planned Development (PD-6 and PD-321) zones to a new Planned Development (P-D) zone to allow for the expansion of an existing winery and bottling facility developed on 82.15 acres of the project site. The expansion includes 14 proposed buildings, totaling 1,462,186 square feet, the construction of two rail spurs, and the addition of a fleet of 53 foot long trucks and tanker trucks. The project is located at the southeast corner of Bystrum and E. Keyes Roads, east of Crows Landing Road, west of State Highway 99 and south of Ceres, CA. The District offers the following comments:

1. Based on information provided to the District, project specific emissions of criteria pollutants may exceed District significance thresholds of 10 tons/year NOX, 10 ton/year ROG, and 15 tons/year PM10.

It should be noted that compliance with District Rules and Regulations may not reduce project emissions to below the District's threshold of significance. The District's permitting process typically ensures that emissions of criteria pollutants from permitted equipment and activities at stationary sources are reduced or mitigated to below the District's thresholds of significance. However, the permitting process for projects subject to District Permits will not address construction and non-permitted source emissions. The District recommends that the County provide a more detailed assessment.

Seyed Sadredin
Executive Director/Air Pollution Control Officer

Northern Region
4800 Enterprise Way
Modesto, CA 95356-8718
Tel: (209) 557-6400 FAX: (209) 557-6475

Central Region (Main Office)
1990 E. Gettysburg Avenue
Fresno, CA 93726-0244
Tel: (559) 230-6000 FAX: (559) 230-6061
www.valleyair.org

Southern Region
34946 Flyover Court
Bakersfield, CA 93308-9725
Tel: 661-392-5500 FAX: 661-392-5585

- a) **Criteria Pollutants:** Project related criteria pollutant emissions should be identified and quantified. The discussion should include existing and post-project emissions.
 - i) **Construction Emissions:** Construction emissions are short-term emissions and should be evaluated separate from operational emissions. The District recommends preparation of an Environmental Impact Report (EIR) if annual construction emissions cannot be reduced or mitigated to below the following levels of significance: 10 tons per year of oxides of nitrogen (NOx), 10 tons per year of reactive organic gases (ROG), or 15 tons per year particulate matter of 10 microns or less in size (PM10).
 - ii) **Operational Emissions:** Operational Emissions: Permitted (stationary sources) and non-permitted (mobile sources) sources should be analyzed separately. The District recommends preparation of an Environmental Impact Report (EIR) if the sum of annual permitted and the sum of the annual non-permitted emissions each cannot be reduced or mitigated to below the following levels of significance: 10 tons per year of oxides of nitrogen (NOx), 10 tons per year of reactive organic gases (ROG), or 15 tons per year particulate matter of 10 microns or less in size (PM10).
2. **Health Impacts:** Project related health impacts should be evaluated to determine if emissions of toxic air contaminants (TAC) will pose a significant health risk to nearby sensitive receptors. TACs are defined as air pollutants that which may cause or contribute to an increase in mortality or serious illness, or which may pose a hazard to human health. The most common source of TACs can be attributed to diesel exhaust fumes that are emitted from both stationary and mobile sources. Health impacts may require a detailed health risk assessment (HRA).

Prior to conducting an HRA, an applicant may perform a prioritization on all sources of emissions to determine if it is necessary to conduct an HRA. A prioritization is a screening tool used to identify projects that may have significant health impacts. If the project has a prioritization score of 1.0 or more, the project has the potential to exceed the District's significance threshold for health impacts of 20 in a million and an HRA should be performed.

If an HRA is to be performed, it is recommended that the project proponent contact the District to review the proposed modeling approach. The project would be considered to have a significant health risk if the HRA demonstrates that project related health impacts would exceed the District's significance threshold of 20 in a million.

More information on TACs, prioritizations and HRAs can be obtained by:

- E-mailing inquiries to: hramodeler@valleyair.org; or
- Visiting the District's website at:
http://www.valleyair.org/busind/pto/Tox_Resources/AirQualityMonitoring.htm.

3. This project will be subject to District Rule 2010 (Permits Required) and Rule 2201 (New and Modified Stationary Source Review) and will require District permits. Prior to construction, the project proponent should submit to the District an application for an Authority to Construct (ATC). For further information or assistance, the project proponent may contact the District's Small Business Assistance (SBA) Office at (209) 557-6446 or visit <http://www.valleyair.org/busind/pto/ptoforms/1ptoformidx.htm>.
4. The proposed project may be subject to District rules and regulations, including: Regulation VIII (Fugitive PM10 Prohibitions), Rule 4102 (Nuisance), and Rule 4641 (Cutback, Slow Cure, and Emulsified Asphalt, Paving and Maintenance Operations). In the event an existing building will be renovated, partially demolished or removed, the project may be subject to District Rule 4002 (National Emission Standards for Hazardous Air Pollutants). Current District rules can be found online at: www.valleyair.org/rules/1ruleslist.htm.
5. The District recommends that a copy of the District's comments be provided to the project proponent.

If you have any questions or require further information, please call Michael Corder, at (559) 230-5818.

Sincerely,

Arnaud Marjollet
Director of Permit Services

Michael Corder

For: Brian Clements
Program Manager

DW: mc



DEPARTMENT OF PLANNING AND COMMUNITY DEVELOPMENT

1010 10th Street, Suite 3400, Modesto, CA 95354
Phone: 209.525.6330 Fax: 209.525.5911

CEQA INITIAL STUDY

Adapted from CEQA Guidelines APPENDIX G Environmental Checklist Form, Final Text, December 30, 2009

1. **Project title:** Rezone Application No. PLN2016-0066 – Bronco Wine Company. SCH No.2016082036
2. **Lead agency name and address:** Stanislaus County
1010 10th Street, Suite 3400
Modesto, CA 95354
3. **Contact person and phone number:** Kristin Doud, Senior Planner
(209) 525-6330
4. **Project location:** 6342 Bystrum Road, at the southeast corner of Bystrum and E. Keyes Roads, east of Crows Landing Road, west of State Highway 99, and south of Ceres. APN: 041-046-021
5. **Project sponsor's name and address:** John Franzia, Bronco Wine Company
6342 Bystrum Road
Ceres, CA 95307
6. **General Plan designation:** AG (Agriculture)
7. **Zoning:** Planned Development (6) & Planned Development (321)
8. **Description of project:**

This is a request to rezone a 117.93 acre parcel from existing Planned Development (PD-6 and PD-321) zones to a new Planned Development (P-D) zone to allow for expansion of an existing winery and bottling facility developed on 82.15 acres of the project site. The expansion includes construction of 14 proposed buildings, totaling 1,462,186 square feet (see Buildings labeled N-BB on the site plan included in Attachment A), which includes: four 120,000 square foot warehouses (Buildings N, O, P, and Q), two with 10 additional truck docks each; three 44,483 square foot warehouses (Buildings V, W, and X); one 13,000 square foot office (Building T); one 38,000 square foot office (Building U); one 10,300 square foot employee center (commercial kitchen, cafeteria, and conference area, Building R); one 2,264 square foot pavilion (roof only shade structure, Building S); one 20,000 square foot employee center (lockers and restrooms, Building Y); one 30,000 square foot administration building (Building Z); and, a 16,000 square foot filter storage building (Building AA). Phase one of development will occur within five years of project approval, which includes construction of a 120,000 square foot warehouse (Building Q) to be utilized for the storage of bottled wine stock. Future phases will be built as market demands. The hours of operation for the winery are Monday-Friday, 24 hours a day year round and additionally Sunday-Saturday 24 hours per day during seasonal months, which is from mid-July to mid-November. There are currently 396 employees year round with an additional 90 employees during seasonal months, for a total of 486 employees maximum. At full build-out there will be approximately 30 additional year round employees, for a total of 426 employees year round and 516 employees seasonally. The addition of the employee center and pavilion will be utilized for educational seminars and meetings, to be held up to two times per year for up to 68 people, for Bronco's National sales force, and for Bronco's Wholesale Division's monthly meetings (Northern California sales force), which proposes to utilize the Ceres site up to four times per year for up to 50 managers. All access to the project site will occur along Bystrum Road. All entrances to the operation are fenced and include security gates. The expansion also includes railroad access to Union Pacific Railroad by constructing two rail spurs, which will minimize traffic impacts in surrounding areas. As part of the rezone, a fleet of 53 foot long trucks and tanker trucks will be added to the operation and stored on-site to allow both bulk and bottled wines to be picked up and delivered to partner wineries. On-site truck maintenance will be limited to minor maintenance activities. Any required

major maintenance will be performed at off-site truck repair shops. The project proposes to maintain their current operational ratio of approximately 88% of product produced and owned by Bronco, 8% produced by other California wineries, and 4% imported from other countries. See attachment B for an expanded project description.

- 9. **Surrounding land uses and setting:** Scattered single family in all directions. To the north, orchards and row crops. To the east, a vineyard, orchards, and row crops. To the west orchards, row crops, and a dairy farm. To the south a chicken farm, orchard, row crops, and a dairy farm.
- 10. **Other public agencies whose approval is required (e.g., permits, financing approval, or participation agreement.):** Building Permits Division
Department of Environmental Resources
DER Hazardous Materials Division
Department of Public Works
Regional Water Quality Control Board
Turlock Irrigation District

ENVIRONMENTAL FACTORS POTENTIALLY AFFECTED:

The environmental factors checked below would be potentially affected by this project, involving at least one impact that is a “Potentially Significant Impact” as indicated by the checklist on the following pages.

- Aesthetics
- Agriculture & Forestry Resources
- Air Quality
- Biological Resources
- Cultural Resources
- Geology / Soils
- Greenhouse Gas Emissions
- Hazards & Hazardous Materials
- Hydrology / Water Quality
- Land Use / Planning
- Mineral Resources
- Noise
- Population / Housing
- Public Services
- Recreation
- Transportation / Traffic
- Utilities / Service Systems
- Mandatory Findings of Significance

DETERMINATION: (To be completed by the Lead Agency)
On the basis of this initial evaluation:

- I find that the proposed project **COULD NOT** have a significant effect on the environment, and a **NEGATIVE DECLARATION** will be prepared.
- I find that although the proposed project could have a significant effect on the environment, there will not be a significant effect in this case because revisions in the project have been made by or agreed to by the project proponent. A **MITIGATED NEGATIVE DECLARATION** will be prepared.
- I find that the proposed project **MAY** have a significant effect on the environment, and an **ENVIRONMENTAL IMPACT REPORT** is required.
- I find that the proposed project **MAY** have a “potentially significant impact” or “potentially significant unless mitigated” impact on the environment, but at least one effect 1) has been adequately analyzed in an earlier document pursuant to applicable legal standards, and 2) has been addressed by mitigation measures based on the earlier analysis as described on attached sheets. An **ENVIRONMENTAL IMPACT REPORT** is required, but it must analyze only the effects that remain to be addressed.
- I find that although the proposed project could have a significant effect on the environment, because all potentially significant effects (a) have been analyzed adequately in an earlier EIR or **NEGATIVE DECLARATION** pursuant to applicable standards, and (b) have been avoided or mitigated pursuant to that earlier EIR or **NEGATIVE DECLARATION**, including revisions or mitigation measures that are imposed upon the proposed project, nothing further is required.

Kristin Doud, Senior Planner

 Signature

March 22, 2017

 Date

EVALUATION OF ENVIRONMENTAL IMPACTS:

1) A brief explanation is required for all answers except “No Impact” answers that are adequately supported by the information sources a lead agency cites in the parentheses following each question. A “No Impact” answer is adequately supported if the referenced information sources show that the impact simply does not apply to projects like the one involved (e.g., the project falls outside a fault rupture zone). A “No Impact” answer should be explained where it is based on project-specific factors as well as general standards (e.g., the project will not expose sensitive receptors to pollutants, based on a project-specific screening analysis).

2) All answers must take account of the whole action involved, including off-site as well as on-site, cumulative as well as project-level, indirect as well as direct, and construction as well as operational impacts.

3) Once the lead agency has determined that a particular physical impact may occur, than the checklist answers must indicate whether the impact is potentially significant, less than significant with mitigation, or less than significant. “Potentially Significant Impact” is appropriate if there is substantial evidence that an effect may be significant. If there are one or more “Potentially Significant Impact” entries when the determination is made, an EIR is required.

4) “Negative Declaration: Less Than Significant With Mitigation Incorporated” applies where the incorporation of mitigation measures has reduced an effect from “Potentially Significant Impact” to a “Less Than Significant Impact.” The lead agency must describe the mitigation measures, and briefly explain how they reduce the effect to a less than significant level (mitigation measures from Section XVII, “Earlier Analyses,” may be cross-referenced).

5) Earlier analyses may be used where, pursuant to the tiering, program EIR, or other CEQA process, an effect has been adequately analyzed in an earlier EIR or negative declaration.

Section 15063(c)(3)(D). In this case, a brief discussion should identify the following:

a) **Earlier Analysis Used.** Identify and state where they are available for review.

b) **Impacts Adequately Addressed.** Identify which effects from the above checklist were within the scope of and adequately analyzed in an earlier document pursuant to applicable legal standards, and state whether such effects were addressed by mitigation measures based on the earlier analysis.

c) **Mitigation Measures.** For effects that are “Less than Significant with Mitigation Measures Incorporated,” describe the mitigation measures which were incorporated or refined from the earlier document and the extent to which they address site-specific conditions for the project.

6) Lead agencies are encouraged to incorporate into the checklist references to information sources for potential impacts (e.g., general plans, zoning ordinances). References to a previously prepared or outside document should, where appropriate, include a reference to the page or pages where the statement is substantiated.

7) **Supporting Information Sources:** A source list should be attached, and other sources used or individuals contacted should be cited in the discussion.

8) This is only a suggested form, and lead agencies are free to use different formats; however, lead agencies should normally address the questions from this checklist that are relevant to a project’s environmental effects in whatever format is selected.

9) The explanation of each issue should identify:

a) the significant criteria or threshold, if any, used to evaluate each question; and

b) the mitigation measure identified, if any, to reduce the impact to less than significant.

ISSUES

| I. AESTHETICS -- Would the project: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|--|--------------------------------|--|------------------------------|-----------|
| a) Have a substantial adverse effect on a scenic vista? | | | X | |
| b) Substantially damage scenic resources, including, but not limited to, trees, rock outcroppings, and historic buildings within a state scenic highway? | | | X | |
| c) Substantially degrade the existing visual character or quality of the site and its surroundings? | | | X | |
| d) Create a new source of substantial light or glare which would adversely affect day or nighttime views in the area? | | X | | |

Discussion: The site itself is not considered to be a scenic resource or a unique scenic vista. Community standards generally do not dictate the need or desire for architectural review of agricultural uses. The 117± acre project site is currently developed with structures to support the on-site wine manufacturing facility. The additional buildings proposed as part of this project will be consistent with existing construction and will include additional fencing and landscaping along the northern property boundary. The proposed buildings will not exceed 45 feet in height, with the warehouses totaling 26 feet in height.

A Mitigation Measure has been applied to the project to ensure that any additional lighting will be aimed down to prevent any glaring impacts onto adjacent properties or roadways. With this mitigation measure in place, aesthetic impacts are considered to be less than significant with mitigation included.

Mitigation:

No. 1 Mitigation Measure: All exterior lighting shall be designed (aimed down and toward the site) to provide adequate illumination without a glare effect. This shall include but not be limited to: the use of shielded light fixtures to prevent skyglow (light spilling into the night sky) and to prevent light trespass (glare and spill light that shines onto neighboring properties).

References: Application information; and the Stanislaus County General Plan and Support Documentation¹.

| II. AGRICULTURE AND FOREST RESOURCES: In determining whether impacts to agricultural resources are significant environmental effects, lead agencies may refer to the California Agricultural Land Evaluation and Site Assessment Model (1997) prepared by the California Department of Conservation as an optional model to use in assessing impacts on agriculture and farmland. In determining whether impacts to forest resources, including timberland, are significant environmental effects, lead agencies may refer to information compiled by the California Department of Forestry and Fire Protection regarding the state's inventory of forest land, including the Forest and Range Assessment Project and the Forest Legacy Assessment project; and forest carbon measurement methodology provided in Forest Protocols adopted by the California Air Resources Board. -- Would the project: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|---|--------------------------------|--|------------------------------|-----------|
| a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use? | | | X | |

| | | | | |
|--|--|--|---|---|
| b) Conflict with existing zoning for agricultural use, or a Williamson Act contract? | | | X | |
| c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g))? | | | X | |
| d) Result in the loss of forest land or conversion of forest land to non-forest use? | | | | X |
| e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use? | | | X | |

Discussion: The property is not currently restricted by a Williamson Act Contract. The project site is classified as Prime Farmland and Urban and Built-Up Land by the Farmland Mapping and Monitoring Program. The soils on site are listed as Grade 1 Hanford sandy loams (0-3% slopes, Index Rating of 95), Grade 2 Dinuba sandy loam (0-1% slopes, Index Rating between 60-72), and Grade 2 Tujunga loamy sand (0-3% slopes, Index Rating of 76).

The project will result in the paving over of prime farmland; however, the County recognizes that the proposed project is directly related to the production of commercial agricultural product on the subject parcel and adjacent southern parcel. Compatible uses include activities such as harvesting, processing and shipping. The rezoning of this parcel constitutes an expansion of the existing operation, which processes grapes and produces wine.

In December of 2007, Stanislaus County adopted an updated Agricultural Element, which incorporated guidelines for the implementation of agricultural buffers applicable to new and expanding non-agricultural uses within or adjacent to the A-2 Zoning District. The purpose of these guidelines is to protect the long-term health of agriculture by minimizing conflicts such as spray drift resulting from the interaction of agricultural and non-agricultural uses. Alternatives may be approved provided the Planning Commission finds that the alternative provides equal or greater protection than the existing buffer standards. Although the development proposed on the northern portion of the property does not meet the 300 foot buffer setback standard for people intensive uses on the northern and western property lines, the project does currently include six foot high security chain link fencing, and cypress trees along the eastern and western property borders and proposes to extend the fencing and cypress trees to act as an ag buffer along the northern property line.

Mitigation: None.

References: Stanislaus County General Plan and Support Documentation¹, Stanislaus County Agricultural Element¹, Stanislaus County Zoning Ordinance, California State Department of Conservation Farmland Mapping and Monitoring Program - Stanislaus County Farmland 2004, United States Department of Agriculture Soil Survey 1964 - Eastern Stanislaus Area, California.

| III. AIR QUALITY: Where available, the significance criteria established by the applicable air quality management or air pollution control district may be relied upon to make the following determinations. -- Would the project: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|--|--------------------------------|--|------------------------------|-----------|
| a) Conflict with or obstruct implementation of the applicable air quality plan? | | | X | |
| b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation? | | | X | |

| | | | | |
|--|--|--|---|--|
| c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)? | | | X | |
| d) Expose sensitive receptors to substantial pollutant concentrations? | | | X | |
| e) Create objectionable odors affecting a substantial number of people? | | | X | |

Discussion: The project site is within the San Joaquin Valley Air Basin, which has been classified as "severe non-attainment" for ozone and respirable particulate matter (PM-10) as defined by the Federal Clean Air Act. The San Joaquin Valley Air Pollution Control District (SJVAPCD) has been established by the State in an effort to control and minimize air pollution. As such, the District maintains permit authority over stationary sources of pollutants.

The expansion includes construction of 14 proposed buildings, totaling 1,462,186 square feet (see Buildings labeled N-BB on the site plan included in Attachment A), which includes: four 120,000 square foot warehouses (Buildings N, O, P, and Q), two with 10 additional truck docks each; three 44,483 square foot warehouses (Buildings V, W, and X); one 13,000 square foot office (Building T); one 38,000 square foot office (Building U); one 10,300 square foot employee center (commercial kitchen, cafeteria, and conference area, Building R); one 2,264 square foot pavilion (roof only shade structure, Building S); one 20,000 square foot employee center (lockers and restrooms, Building Y); one 30,000 square foot administration building (Building Z); and, a 16,000 square foot filter storage building (Building AA). Minimal emissions will occur during construction. Construction activities are considered to be less than significant as they are temporary in nature and are subject to meeting SJVAPCD standards for air quality control.

The primary source of air pollutants generated by this project would be classified as being generated from "mobile" sources created from increased truck trips generated from the expansion. Mobile sources would generally include dust from roads, farming, and vehicle exhausts. However, the addition of a fleet of trucks and the utilization of rail will allow the current truck trip to inventory ratio to be decreased. Trucks currently arrive to the site empty or leave the site empty. The addition of their own truck fleet will allow truck trips to be full both on the way to the site and on the way to a delivery/pick-up destination. The use of rail will also offset truck trips as the equivalent of four fully stocked trucks can fit into one rail car. At full build-out there will be approximately 30 additional year round employees, for a total of 426 employees year round and 516 employees seasonally. The addition of the employee center and pavilion will be utilized for educational seminars and meetings, to be held up to two times per year for up to 68 people, for Bronco's National sales force, and for Bronco's Wholesale Division's monthly meetings (Northern California sales force), which proposes to utilize the Ceres site up to four times per year for up to 50 managers. Mobile sources are generally regulated by the Air Resources Board of the California EPA which sets emissions for vehicles and acts on issues regarding cleaner burning fuels and alternative fuel technologies. As such, the District has addressed most criteria air pollutants through basin wide programs and policies to prevent cumulative deterioration of air quality within the Basin. Although no response was received from SJVAPCD, the applicant will be required to meet all Air District standards and to obtain any necessary Air District permits, including but not limited to an Air Impact Assessment (AIA). This requirement will be incorporated into the project's Conditions of Approval. With conditions of approval in place, no significant impacts to air quality are anticipated.

Mitigation: None.

References: Application information; Stanislaus County General Plan and Support Documentation¹

| IV. BIOLOGICAL RESOURCES -- Would the project: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|---|---------------------------------------|---|-------------------------------------|------------------|
| a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service? | | | X | |

| | | | | |
|--|--|--|---|--|
| b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service? | | | X | |
| c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means? | | | X | |
| d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites? | | | X | |
| e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance? | | | | |
| f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan? | | | X | |

Discussion: The project is located within the Ceres Quad of the California Natural Diversity Database. There are 14 plants and animals which are state or federally listed, threatened, or identified as species of special concern within the Waterford California Natural Diversity Database Quad. These species include the Swainson’s hawk, tricolored blackbird, burrowing owl, riffle sculpin, hardhead, steelhead, chinook salmon, obscure bumble bee, Crotch bumble bee, valley elderberry longhorn beetle, moestan blister beetle, Townsend’s big-eared bat, heartscale, and subtle orache. However, the project site is already developed and hardscaped or graded, making the likelihood for existence of these species on the project site very low.

An Early Consultation was sent to the California Department of Fish and Wildlife (formerly the Department of Fish and Game) and no response was received. The project will not conflict with a Habitat Conservation Plan, a Natural Community Conservation Plan, or other locally approved conservation plans. Impacts to endangered species or habitats, locally designated species, wildlife dispersal or mitigation corridors are considered to be less than significant.

Mitigation: None.

References: Stanislaus County General Plan and Support Documentation¹; California Department of Fish and Game California Natural Diversity Database.

| V. CULTURAL RESOURCES -- Would the project: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|--|--------------------------------|--|------------------------------|-----------|
| a) Cause a substantial adverse change in the significance of a historical resource as defined in § 15064.5? | | | X | |
| b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to § 15064.5? | | | X | |
| c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature? | | | X | |
| d) Disturb any human remains, including those interred outside of formal cemeteries? | | | X | |

Discussion: It does not appear this project will result in significant impacts to any archaeological or cultural resources. The applicant submitted a records search from the Central California Information Center (CCIC) which indicates that the project area has a low sensitivity for the possible discovery of prehistoric resources, due to the distance from a natural

water source, as well as a low sensitivity for historic archaeological resources. A Sacred Lands File Check, completed by the Native American Heritage Commission, indicated that no sacred sites were present within the project site. Conditions of Approval will be placed on the project, requiring that construction activities will be halted if any resources are found, until appropriate agencies are contacted and an archaeological survey is completed.

Mitigation: None.

References: Stanislaus County General Plan and Support Documentation¹; records search dated May 27, 2009, from the Central California Information Center; referral response from the Native American Heritage Commission dated November 17, 2009.

| VI. GEOLOGY AND SOILS -- Would the project: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|--|--------------------------------|--|------------------------------|-----------|
| a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving: | | | X | |
| i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? Refer to Division of Mines and Geology Special Publication 42. | | | X | |
| ii) Strong seismic ground shaking? | | | X | |
| iii) Seismic-related ground failure, including liquefaction? | | | X | |
| iv) Landslides? | | | X | |
| b) Result in substantial soil erosion or the loss of topsoil? | | | X | |
| c) Be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse? | | | X | |
| d) Be located on expansive soil creating substantial risks to life or property? | | | X | |
| e) Have soils incapable of adequately supporting the use of septic tanks or alternative waste water disposal systems where sewers are not available for the disposal of waste water? | | | X | |

Discussion: The soils on site are listed as Grade 1 Hanford sandy loams (0-3% slopes, Index Rating of 95), Grade 2 Dinuba sandy loam (0-1% slopes, Index Rating between 60-72), and Grade 2 Tujunga loamy sand (0-3% slopes, Index Rating of 76). As contained in Chapter 5 of the General Plan Support Documentation, the areas of the County subject to significant geologic hazard are located in the Diablo Range, west of Interstate 5. However, as per the 2007 California Building Code, all of Stanislaus County is located within a geologic hazard zone (Seismic Design Category D, E, or F) and a soils test may be required at building permit application. Results from the soils test will determine if unstable or expansive soils are present. If such soils are present, special engineering of the structure will be required to compensate for the soil deficiency. Any structures resulting from this project will be designed and built according to building standards appropriate to withstand shaking for the area in which they are constructed. Any earth moving is subject to Public Works Standards and Specifications which considers the potential for erosion and run-off prior to permit approval. Likewise, any addition of a septic tank or alternative waste water disposal system would require the approval of the Department of Environmental Resources (DER) through the building permit process, which also takes soil type into consideration within the specific design requirements.

Stanislaus County Department of Public Works has already reviewed and approved a grading and drainage plan for proposed Phase 1 of this project, which includes the 120,000 square foot warehouse (labeled on the site plan as Building Q) and a drainage basin, located on the northeast portion of the project site. Additional grading and drainage plans are required to be submitted to the Department of Public Works for review and approval for any additional grading activities, which will be reflected as a Condition of Approval for the project.

Mitigation: None.

References: California Building Code (2016); Stanislaus County General Plan and Support Documentation - Safety Element¹.

| VII. GREENHOUSE GAS EMISSIONS -- Would the project: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|--|--------------------------------|--|------------------------------|-----------|
| a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment? | | | X | |
| b) Conflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases? | | | X | |

Discussion: The principal Greenhouse Gasses (GHGs) are carbon dioxide (CO₂), methane (CH₄), nitrous oxide (N₂O), sulfur hexafluoride (SF₆), perfluorocarbons (PFCs), hydrofluorocarbons (HFCs), and tropospheric Ozone (O₃). CO₂ is the reference gas for climate change because it is the predominant greenhouse gas emitted. To account for the varying warming potential of different GHGs, GHG emissions are often quantified and reported as CO₂ equivalents (CO₂e). In 2006, California passed the California Global Warming Solutions Act of 2006 (Assembly Bill [AB] No. 32), which requires the California Air Resources Board (ARB) design and implement emission limits, regulations and other measures, such that feasible and cost-effective statewide GHG emissions are reduced to 1990 levels by 2020.

The expansion includes construction of 14 proposed buildings, totaling 1,462,186 square feet (see Buildings labeled N-BB on the site plan included in Attachment A), which includes: four 120,000 square foot warehouses (Buildings N, O, P, and Q), two with 10 additional truck docks each; three 44,483 square foot warehouses (Buildings V, W, and X); one 13,000 square foot office (Building T); one 38,000 square foot office (Building U); one 10,300 square foot employee center (commercial kitchen, cafeteria, and conference area, Building R); one 2,264 square foot pavilion (roof only shade structure, Building S); one 20,000 square foot employee center (lockers and restrooms, Building Y); one 30,000 square foot administration building (Building Z); and a 16,000 square foot filter storage building (Building AA).

The proposed structures are subject to the mandatory planning and design, energy efficiency, water efficiency and conservation, material conservation and resources efficiency, and environmental quality measures of the California Green Building Standards (CALGreen) Code (California Code of Regulations, Title 24, Part 11). Minimal greenhouse gas emissions will occur during construction. Construction activities are considered to be less than significant as they are temporary in nature and are subject to meeting SJVAPCD standards for air quality control.

Minimal greenhouse gas emissions will also be generated from additional vehicle and truck trips. However, the addition of a fleet of trucks and the utilization of rail will allow the current truck trip to inventory ratio to be decreased. Trucks currently arrive to the site empty or leave the site empty. The addition of their own truck fleet will allow truck trips to be full both on the way to the site and on the way to a delivery/pick-up destination. The use of rail will also offset truck trips as the equivalent of four fully stocked trucks can fit into one rail car. There are currently 396 employees year round with an additional 90 employees during seasonal months, for a total of 486 employees maximum. At full build-out there will be approximately 30 additional year round employees, for a total of 426 employees year round and 516 employees seasonally. The addition of the employee center and pavilion will be utilized for educational seminars and meetings, to be held up to two times per year for up to 68 people, for Bronco's National sales force, and for Bronco's Wholesale Division's

monthly meetings (Northern California sales force), which proposes to utilize the Ceres site up to four times per year for up to 50 managers. Although no response was received from SJVAPCD, the applicant will be required to meet all Air District standards and to obtain any necessary Air District permits, including but not limited to an Air Impact Assessment (AIA). This will be incorporated into the project's conditions of approval.

Mitigation: None.

References: Application information; Stanislaus County General Plan and Support Documentation¹

| VIII. HAZARDS AND HAZARDOUS MATERIALS -- Would the project: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|--|--------------------------------|--|------------------------------|-----------|
| a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials? | | | X | |
| b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment? | | | X | |
| c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school? | | | X | |
| d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment? | | | X | |
| e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area? | | | | X |
| f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area? | | | | X |
| g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan? | | | X | |
| h) Expose people or structures to a significant risk of loss, injury or death involving wildland fires, including where wildlands are adjacent to urbanized areas or where residences are intermixed with wildlands? | | | X | |

Discussion: DER is responsible for overseeing hazardous materials in this area. Pesticide exposure is a risk in areas located in the vicinity of agriculture. Sources of exposure include contaminated groundwater, which is consumed and drift from spray applications. Application of sprays is strictly controlled by the Agricultural Commissioner and can only be accomplished after first obtaining permits. Spraying activities on adjacent properties will be conditioned by the Agricultural Commissioner's Office. The project site is not located within an airport land use plan or a wildlands area. The project site is not located in a very high or high fire severity zone and is located within the Keyes Fire District. Standard conditions of approval regarding fire protection will be incorporated into the project.

An Early Consultation referral response from DER requested standard conditions regarding hazardous materials associated with the proposed project and site be incorporated into the project's conditions of approval.

Mitigation: None.

References: Application information; referral response dated from the Stanislaus County Department of Environmental Resources on August 16, 2016; Stanislaus County General Plan and Support Documentation¹

| IX. HYDROLOGY AND WATER QUALITY -- Would the project: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|---|--------------------------------|--|------------------------------|-----------|
| a) Violate any water quality standards or waste discharge requirements? | | | X | |
| b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)? | | | X | |
| c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner which would result in substantial erosion or siltation on- or off-site? | | | X | |
| d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site? | | | X | |
| e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff? | | | X | |
| f) Otherwise substantially degrade water quality? | | | | |
| g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other flood hazard delineation map? | | | X | |
| h) Place within a 100-year flood hazard area structures which would impede or redirect flood flows? | | | X | |
| i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam? | | | X | |
| j) Inundation by seiche, tsunami, or mudflow? | | | X | |

Discussion: Areas subject to flooding have been identified in accordance with the Federal Emergency Management Act (FEMA). The project site is located in FEMA Flood Zone X, which includes areas determined to be outside the 0.2% annual chance floodplains. All flood zone requirements will be addressed by the Building Permits Division during the building permit process. The Central Valley Regional Water Quality Control Board (RWQCB) provided an Early Consultation referral response requesting that the applicant coordinate with their agency to determine if any permits or Water Board requirements must be obtained/met prior to operation. Conditions of approval will be added to the project requiring the applicant comply with this request prior to issuance of a building permit.

Stanislaus County Department of Public Works has already reviewed and approved a grading and drainage plan for proposed Phase 1 of this project, which includes the 120,000 square foot warehouse (labeled on the site plan as Building Q) and a drainage basin, located on the northeast portion of the project site. Additional grading and drainage plans are required to be submitted to the Department of Public Works for review and approval for any additional grading activities. A Notice of Intention (NOI) may be required to be filed with the California Regional Water Quality Control Board and a Waste Discharge Identification Number obtained, in conjunction with future grading or building permits. These requirements will be reflected as Conditions of Approval for the project.

The California Safe Drinking Water Act (CA Health and Safety Code Section 116275(h)) defines a Public Water System as a system for the provision of water for human consumption through pipes or other constructed conveyances that has 15 or more service connections or regularly serves at least 25 individuals daily at least 60 days out of the year. A public water system includes the following:

- (1) Any collection, treatment, storage, and distribution facilities under control of the operator of the system that are used primarily in connection with the system.
- (2) Any collection or pretreatment storage facilities not under the control of the operator that are used primarily in connection with the system.
- (3) Any water system that treats water on behalf of one or more public water systems for the purpose of rendering it safe for human consumption.

This project is subject to the public water system permit and will be required to work with DER to ensure these permit requirements are met. This will be applied to the project as a Condition of Approval.

Mitigation: None.

References: Referral response from the Central Valley Regional Water Quality Control Board dated August 23, 2016; Application information; Stanislaus County General Plan and Support Documentation¹

| X. LAND USE AND PLANNING -- Would the project: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|---|--------------------------------|--|------------------------------|-----------|
| a) Physically divide an established community? | | | X | |
| b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect? | | | X | |
| c) Conflict with any applicable habitat conservation plan or natural community conservation plan? | | | X | |

Discussion: This is a request to expand the northern portion of Bronco Winery. The expansion includes construction of 14 proposed buildings, totaling 1,462,186 square feet (see Buildings labeled N-BB on the site plan included in Attachment A), which includes: four 120,000 square foot warehouses (Buildings N, O, P, and Q), two with 10 additional truck docks each; three 44,483 square foot warehouses (Buildings V, W, and X); one 13,000 square foot office (Building T); one 38,000 square foot office (Building U); one 10,300 square foot employee center (commercial kitchen, cafeteria, and conference area, Building R); one 2,264 square foot pavilion (roof only shade structure, Building S); one 20,000 square foot employee center (lockers and restrooms, Building Y); one 30,000 square foot administration building (Building Z); and a 16,000 square foot filter storage building (Building AA).

The project site is has a general plan designation of Agriculture. The southern portion of the site was re-zoned to Planned Development (6) in 1974, with Rezone 74-2, which allowed for the existing winery operations. The northern portion of the property was rezoned to Planned Development (321) in 2009, with Rezone 2009-04, which permitted conversion of an existing house to a shipping and receiving office, and to construct two 14,400 square foot office buildings, associated parking lot, and two driveways on E. Keyes Road to provide access to the proposed site and the existing Bronco Wine

Company processing and bottling plant. A Time Extension processed for PD-321 extended the Development Schedule to October 20, 2016. Although some grading occurred on the northern portion of the site prior to the date allowed by the Time Extension, the development schedule has not been met; and, as such, a new Rezone is required to develop the site. Additionally, the northern parcel (previously APN: 041-046-019) and the existing winery facility to the south (previously APN: 041-046-020) have been merged into one parcel, and a new and expanded project description is now being proposed, further requiring a new Rezone for the entire 117+ acre merged property (now APN: 041-046-021). If approved, the entire 117+ acre property would maintain a General Plan designation of Agriculture. The "Agriculture" General Plan designation is consistent with a Planned Development zoning designation when, "it is used for agriculturally-related uses or for uses of a demonstrably unique character, which due to specific agricultural needs or to their transportation needs or to needs that can only be satisfied in the agriculture designation, may be properly located within areas designated as "agricultural" on the General Plan. Such uses can include, facilities for packing fresh fruit, facilities for the processing of agricultural commodities utilized in the County's agriculture community, etc."

This request will not physically divide an existing community, nor does it conflict with any applicable land use plan, policy, or regulation, or any habitat or natural community conservation plan. The project must be consistent with the county's general plan, zoning ordinance, and noise ordinance in order to be approved. Through the application of mitigation measures, the project will be consistent with these policies.

Mitigation: None.

References: Application information; Rezone No. 74-02 – Bronco Winery; Rezone No. 2009-04 – Bronco Winery; Stanislaus County Zoning Ordinance (Title 21); Stanislaus County General Plan and Support Documentation¹

| XI. MINERAL RESOURCES -- Would the project: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|---|--------------------------------|--|------------------------------|-----------|
| a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state? | | | X | |
| b) Result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan? | | | X | |

Discussion: The location of all commercially viable mineral resources in Stanislaus County has been mapped by the State Division of Mines and Geology in Special Report 173. There are no known significant resources on the site.

Mitigation: None.

References: State Division of Mining & Geology - Special Report 173 (1993); Stanislaus County General Plan and Support Documentation¹

| XII. NOISE -- Would the project result in: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|---|--------------------------------|--|------------------------------|-----------|
| a) Exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies? | | | X | |
| b) Exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels? | | | X | |
| c) A substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project? | | | X | |

| | | | | |
|---|--|--|---|---|
| d) A substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project? | | | X | |
| e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels? | | | | X |
| f) For a project within the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels? | | | | X |

Discussion: A temporary noise increase will be associated with construction of the proposed buildings. Days and hours of operation are expected to remain the same, operating Monday thru Friday, 24 hours a day, and seasonally seven days a week, 24 hours a day. The project proposes an addition of a fleet of trucks and the utilization of rail, which will allow the current truck trip to inventory ratio to be decreased. There are currently 396 employees year round with an additional 90 employees during seasonal months, for a total of 486 employees maximum. At full build-out there will be approximately 30 additional year round employees, for a total of 426 employees year round and 516 employees seasonally. The addition of the employee center and pavilion will be utilized for educational seminars and meetings, to be held up to two times per year for up to 68 people, for Bronco’s National sales force, and for Bronco’s Wholesale Division’s monthly meetings (Northern California sales force), which proposes to utilize the Ceres site up to four times per year for up to 50 managers. These additional employee, truck, and rail trips will generate some additional noise. However, the activities associated with the project will take place mostly indoors. The operation is exempted from the County’s Noise Control Ordinance, as described in Stanislaus County Code Sections 10.46.080(H) and 9.32.10(B). Impacts associated with noise are considered to be less than significant.

Mitigation: None.

References: Application information; Stanislaus County Noise Control Ordinance (Title 10, Chapter 10.46); Stanislaus County General Plan and Support Documentation¹

| XIII. POPULATION AND HOUSING -- Would the project: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|---|--------------------------------|--|------------------------------|-----------|
| a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)? | | | X | |
| b) Displace substantial numbers of existing housing, necessitating the construction of replacement housing elsewhere? | | | X | |
| c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere? | | | X | |

Discussion: The proposed use of the site will not create significant service extensions or new infrastructure which could be considered as growth inducing, as services are already available to this property. No housing or persons will be displaced by this project. An increased ability to hire additional employees may result in the relocation of working families closer to the site. However, as the project site is surrounded by agricultural land it is unlikely that residential development will occur due to the fact that County voters passed the Measure E vote in February of 2008. Measure E, which was incorporated into Zoning Ordinance Chapter 21.118 (the 30-Year Land Use Restriction), requires that redesignation or rezoning of land from agricultural/open space to residential use shall require approval by a majority vote of the County voters at a general or special local election.

Mitigation: None.

References: Application information; Stanislaus County Zoning Ordinance (Title 21); Stanislaus County General Plan and Support Documentation¹

| XIV. PUBLIC SERVICES -- | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|--|---------------------------------------|---|-------------------------------------|------------------|
| a) Would the project result in the substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times or other performance objectives for any of the public services: | | | X | |
| Fire protection? | | | X | |
| Police protection? | | | X | |
| Schools? | | | X | |
| Parks? | | | X | |
| Other public facilities? | | | X | |

Discussion: The County has adopted Public Facilities Fees, as well as one for the Fire Facility Fees on behalf of the appropriate fire district, to address impacts to public services. Such fees are required to be paid at the time of building permit issuance. Conditions of Approval will be added to this project to ensure that the proposed development complies with all applicable fire department standards, with respect to access and water for fire protection. The applicant will construct all buildings in accordance with the current adopted building and fire codes. With conditions of approval and public facility fees in place, no impacts to public services are anticipated.

Mitigation: None.

References: Application information; Stanislaus County General Plan and Support Documentation¹

| XV. RECREATION -- | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|---|---------------------------------------|---|-------------------------------------|------------------|
| a) Would the project increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated? | | | X | |
| b) Does the project include recreational facilities or require the construction or expansion of recreational facilities which might have an adverse physical effect on the environment? | | | X | |

Discussion: The proposed project is not anticipated to significantly increase demand on recreational facilities or to have an adverse physical effect on the environment.

Mitigation: None.

References: Application information; Stanislaus County General Plan and Support Documentation¹

| XVI. TRANSPORTATION/TRAFFIC -- Would the project: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|---|--------------------------------|--|------------------------------|-----------|
| a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit? | | | X | |
| b) Conflict with an applicable congestion management program, including, but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways? | | | X | |
| c) Result in a change in air traffic patterns, including either an increase in traffic levels or a change in location that results in substantial safety risks? | | X | | |
| d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)? | | X | | |
| e) Result in inadequate emergency access? | | | X | |
| f) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities? | | | X | |

Discussion: The expansion includes construction of 14 proposed buildings, totaling 1,462,186 square feet (see Buildings labeled N-BB on the site plan included in Attachment A), which includes: four 120,000 square foot warehouses (Buildings N, O, P, and Q), two with 10 additional truck docks each; three 44,483 square foot warehouses (Buildings V, W, and X); one 13,000 square foot office (Building T); one 38,000 square foot office (Building U); one 10,300 square foot employee center (commercial kitchen, cafeteria, and conference area, Building R); one 2,264 square foot pavilion (roof only shade structure, Building S); one 20,000 square foot employee center (lockers and restrooms, Building Y); one 30,000 square foot administration building (Building Z); and, a 16,000 square foot filter storage building (Building AA).

A Traffic Impact Analysis for the proposed project was prepared by KD Anderson & Associates, Inc., dated November 23, 2016. The analysis evaluated traffic impacts from the project based on the proposed new structures and based on the addition of a fleet of trucks and the utilization of rail, which will allow the current truck trip to inventory ratio to be decreased. Trucks currently arrive to the site empty or leave the site empty. The addition of their own truck fleet will allow truck trips to be full both on the way to the site and on the way to a delivery/pick-up destination. The use of rail will also offset truck trips as the equivalent of four fully stocked trucks can fit into one rail car. There are currently 396 employees year round with an additional 90 employees during seasonal months, for a total of 486 employees maximum. At full build-out there will be approximately 30 additional year round employees, for a total of 426 employees year round and 516 employees seasonally. The addition of the employee center and pavilion will be utilized for educational seminars and meetings, to be held up to two times per year for up to 68 people, for Bronco's National sales force, and for Bronco's Wholesale Division's monthly meetings (Northern California sales force), which proposes to utilize the Ceres site up to four times per year for up to 50 managers.

Stanislaus County Public Works and the Stanislaus County Environmental Review Committee both provided referral responses requesting that the Traffic Impact Analysis be amended to address safety concerns. The Traffic Impact Analysis was revised on March 15, 2017, to include improvements to the intersection of Keyes Road and Bystrum Road, including dedicated turn lanes per the California Highway Design Manual, to address traffic safety concerns. This has been incorporated into the project as a Mitigation Measure. With mitigation applied, impacts to transportation and traffic are considered to be less than significant with mitigation included.

Mitigation:

No. 2 Mitigation Measure: Prior to issuance of a building permit, not including the building permit for Phase 1 which includes construction of the 120,000 square foot warehouse (Building Q), improvements to alleviate traffic congestion at the intersection of Keyes Road and Bystrum Road and to improve safety conditions along Keyes Road, to include dedicated turn lanes per the California Highway Design Manual, shall be completed. Improvement plans shall be reviewed and approved by the Stanislaus County Department of Public Works.

References: Traffic Impact Analysis prepared by KD Anderson & Associates, Inc., dated November 23, 2016, revised March 15, 2017; referral response from the Stanislaus County Environmental Review Committee dated August 30, 2016; Referral response from Stanislaus County Public Works dated January 6, 2017; Application information; Stanislaus County General Plan and Support Documentation¹

| XVII. UTILITIES AND SERVICE SYSTEMS -- Would the project: | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|---|--------------------------------|--|------------------------------|-----------|
| a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board? | | | X | |
| b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? | | | X | |
| c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects? | | | X | |
| d) Have sufficient water supplies available to serve the project from existing entitlements and resources, or are new or expanded entitlements needed? | | | X | |
| e) Result in a determination by the wastewater treatment provider which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments? | | | X | |
| f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs? | | | X | |
| g) Comply with federal, state, and local statutes and regulations related to solid waste? | | | X | |

Discussion: Limitations on providing services have not been identified. Conditions of Approval will be added to the project to address necessary permits from DER. On-site services will be provided by an approved septic system and water well as determined by DER. A public water system permit will be required to be obtained through DER.

A referral response was received from the Turlock Irrigation District, which included Conditions of Approval regarding existing irrigation infrastructure and electrical capacity for the project site. These comments will be applied to the project as Conditions of Approval.

With Conditions of Approval in place, no impacts to utilities and service systems are anticipated.

Mitigation: None.

References: Application information; Referral response from the Turlock Irrigation District dated August 29, 2016; Stanislaus County General Plan and Support Documentation¹

| XVIII. MANDATORY FINDINGS OF SIGNIFICANCE -- | Potentially Significant Impact | Less Than Significant With Mitigation Included | Less Than Significant Impact | No Impact |
|--|--------------------------------|--|------------------------------|-----------|
| a) Does the project have the potential to degrade the quality of the environment, substantially reduce the habitat of a fish or wildlife species, cause a fish or wildlife population to drop below self-sustaining levels, threaten to eliminate a plant or animal community, reduce the number or restrict the range of a rare or endangered plant or animal or eliminate important examples of the major periods of California history or prehistory? | | | X | |
| b) Does the project have impacts that are individually limited, but cumulatively considerable? (“Cumulatively considerable” means that the incremental effects of a project are considerable when viewed in connection with the effects of past projects, the effects of other current projects, and the effects of probable future projects.) | | | X | |
| c) Does the project have environmental effects which will cause substantial adverse effects on human beings, either directly or indirectly? | | X | | |

Discussion: Review of this project has not indicated any features which might significantly impact the environmental quality of the site and/or the surrounding area. Any potential impacts from this project have been mitigated to a level of less than significant.

¹Stanislaus County General Plan and Support Documentation adopted on August 23, 2016. *Housing Element* adopted on April 5, 2016.

Stanislaus County

Planning and Community Development

1010 10th Street, Suite 3400
Modesto, CA 95354

Phone: (209) 525-6330
Fax: (209) 525-5911

Mitigation Monitoring Plan

Adapted from CEQA Guidelines sec. 15097 Final Text, October 26, 1998

March 20, 2017

1. Project title and location: Rezone Application No. PLN2016-0066 – Bronco Wine Company

6342 Bystrum Road, at the southeast corner of Bystrum and E. Keyes roads, east of Crows Landing Road, west of State Highway 99, and south of Ceres. APN: 041-046-021
2. Project Applicant name and address: John Franzia, Bronco Wine Company
6342 Bystrum Road
Ceres, CA 95307
3. Contact person at County: Kristin Doud, Senior Planner (209) 525-6330

MITIGATION MEASURES AND MONITORING PROGRAM:

List all Mitigation Measures by topic as identified in the Mitigated Negative Declaration and complete the form for each measure.

I. AESTHETICS

No. 1 Mitigation Measure: All exterior lighting shall be designed (aimed down and toward the site) to provide adequate illumination without a glare effect. This shall include but not be limited to: the use of shielded light fixtures to prevent skyglow (light spilling into the night sky) and to prevent light trespass (glare and spill light that shines onto neighboring properties).

Who Implements the Measure: Operator/property owner.
When should the measure be implemented: Ongoing.
When should it be completed: Ongoing.
Who verifies compliance: Stanislaus County Planning and Community Development Department.
Other Responsible Agencies: None.

XVI. TRANSPORTATION/TRAFFIC

No. 2 Mitigation Measure: Prior to issuance of a building permit, not including the building permit for Phase 1, which includes construction of the 120,000 square foot warehouse (Building Q), improvements to alleviate traffic congestion at the intersection of Keyes Road and Bystrum Road and to improve safety conditions along Keyes Road, to include dedicated turn lanes per the

California Highway Design Manual, shall be completed. Improvement plans shall be reviewed and approved by the Stanislaus County Department of Public Works.

| | |
|---|---|
| Who Implements the Measure: | Operator/property owner. |
| When should the measure be implemented: | Prior to issuance of a building permit |
| When should it be completed: | Prior to issuance of a building permit |
| Who verifies compliance: | Stanislaus County Department of Public Works |
| Other Responsible Agencies: | Stanislaus County Planning and Community Development Department |

I, the undersigned, do hereby certify that I understand and agree to be responsible for implementing the Mitigation Program for the above listed project.

Signature on file

Person Responsible for Implementing Mitigation Program

March 2, 2017

Date

MITIGATED NEGATIVE DECLARATION

NAME OF PROJECT: Rezone Application No. PLN2016-0066 – Bronco Wine Company

LOCATION OF PROJECT: 6342 Bystrum Road, at the southeast corner of Bystrum and E. Keyes Roads, east of Crows Landing Road, west of State Highway 99, and south of Ceres. APN: 041-046-021

PROJECT DEVELOPER: John Franzia, Bronco Wine Company
6342 Bystrum Road
Ceres, CA 95307

DESCRIPTION OF PROJECT: Request to rezone a 117.93 acre parcel from existing Planned Development (PD-6 and PD-321) zones to a new Planned Development (P-D) zone to allow for the expansion of an existing winery and bottling facility developed on 82.15 acres of the project site. The expansion includes 14 proposed buildings, totaling 1,462,186 square feet, the construction of two rail spurs, and the addition of a fleet of 53 foot long trucks and tanker trucks.

Based upon the Initial Study, dated **March 22, 2017**, the Environmental Coordinator finds as follows:

1. This project does not have the potential to degrade the quality of the environment, nor to curtail the diversity of the environment.
2. This project will not have a detrimental effect upon either short-term or long-term environmental goals.
3. This project will not have impacts which are individually limited but cumulatively considerable.
4. This project will not have environmental impacts which will cause substantial adverse effects upon human beings, either directly or indirectly.

The aforementioned findings are contingent upon the following mitigation measures (if indicated) which shall be incorporated into this project:

1. All exterior lighting shall be designed (aimed down and toward the site) to provide adequate illumination without a glare effect. This shall include but not be limited to: the use of shielded light fixtures to prevent skyglow (light spilling into the night sky) and to prevent light trespass (glare and spill light that shines onto neighboring properties).
2. Prior to issuance of a building permit, not including the building permit for Phase 1 which includes construction of the 120,000 square foot warehouse (Building Q), improvements to alleviate traffic congestion at the intersection of Keyes Road and Bystrum Road and to improve safety conditions along Keyes Road, to include dedicated turn lanes per the California Highway Design Manual, shall be completed. Improvement plans shall be reviewed and approved by the Stanislaus County Department of Public Works.

The Initial Study and other environmental documents are available for public review at the Department of Planning and Community Development, 1010 10th Street, Suite 3400, Modesto, California.

Initial Study prepared by: Kristin Doud, Senior Planner

Submit comments to: Stanislaus County
Planning and Community Development Department
1010 10th Street, Suite 3400
Modesto, California 95354

(I:\PLANNING\STAFF REPORTS\REZ\2016\REZ PLN2016-0066 - BRONCO WINE COMPANY\CEQA-30-DAY-REFERRAL\MITIGATED NEGATIVE DECLARATION.DOC)

SUMMARY OF RESPONSES FOR ENVIRONMENTAL REVIEW REFERRALS

PROJECT: REZONE APPLICATION NO. PLN2016-0066 - BRONCO WINE COMPANY

| REFERRED TO: | RESPONDED | | | RESPONSE | | | MITIGATION MEASURES | | CONDITIONS | | | |
|---|-----------|--------|-----------------------|----------|----|----------------------------------|-----------------------------|---------------------|------------|----|-----|----|
| | 2 WK | 30 DAY | PUBLIC HEARING NOTICE | YES | NO | WILL NOT HAVE SIGNIFICANT IMPACT | MAY HAVE SIGNIFICANT IMPACT | NO COMMENT NON CEQA | YES | NO | YES | NO |
| CA DEPT OF CONSERVATION | X | X | X | | X | | | | | | | |
| CA DEPT OF FISH & WILDLIFE | X | X | X | | X | | | | | | | |
| CA DEPT OF TRANSPORTATION | X | X | X | | X | | | | | | | |
| CA NATIVE AMERICAN HERITAGE COMM | | | | X | | | | X | | X | | X |
| CA OPR STATE CLEARINGHOUSE | X | X | X | X | | | | X | | X | | X |
| CENTRAL VALLEY RWQCB | X | X | X | X | | | | X | | X | X | |
| COOPERATIVE EXTENSION | X | X | X | | X | | | | | | | |
| FIRE PROTECTION DIST: KEYES | X | X | X | | X | | | | | | | |
| IRRIGATION DISTRICT: TURLOCK | X | X | X | X | | | | X | | X | X | |
| MOSQUITO DISTRICT: TURLOCK | X | X | X | | X | | | | | | | |
| MT VALLEY EMERGENCY MEDICAL | X | X | X | | X | | | | | | | |
| PG&E | X | X | X | | X | | | | | | | |
| SAN JOAQUIN VALLEY APCD | X | X | X | X | | | X | | | X | X | |
| SCHOOL DISTRICT 1: CERES UNIFIED | X | X | X | | X | | | | | | | |
| STAN CO AG COMMISSIONER | X | X | X | | X | | | | | | | |
| STAN CO BUILDING PERMITS DIVISION | X | X | X | | X | | | | | | | |
| STAN CO CEO | X | X | X | | X | | | | | | | |
| STAN CO DER | X | X | X | X | | | | X | | X | X | |
| STAN CO ERC | X | X | X | X | | | X | | | X | | X |
| STAN CO FARM BUREAU | X | X | X | | X | | | | | | | |
| STAN CO HAZARDOUS MATERIALS | X | X | X | X | | | | X | | X | X | |
| STAN CO PUBLIC WORKS | X | X | X | X | | | X | | X | | X | |
| STAN CO SHERIFF | X | X | X | | X | | | | | | | |
| STAN CO SUPERVISOR DIST #2: CHIESA | X | X | X | | X | | | | | | | |
| STAN COUNTY COUNSEL | X | X | X | | X | | | | | | | |
| STANISLAUS FIRE PREVENTION BUREAU | X | X | X | | X | | | | | | | |
| STANISLAUS LAFCO | X | X | X | | X | | | | | | | |
| SURROUNDING LAND OWNERS & RESPONDING NEIGHBORS | | X | X | | X | | | | | | | |
| TELEPHONE COMPANY: AT&T | X | X | X | | X | | | | | | | |
| TRIBAL CONTACTS: TULE RIVER INDIAN TRIBE, NORTH VALLEY YOKUTS TRIBE, SOUTHERN SIERRA MIWUK NATION | X | X | X | | X | | | | | | | |
| US FISH AND WILDLIFE | X | X | X | | X | | | | | | | |
| US MILITARY | X | X | X | | X | | | | | | | |